
CITY OF SUSANVILLE
66 North Lassen Street ♦ Susanville CA
Kevin Stafford, Mayor
Joseph Franco, Mayor pro tem
Brian Moore * Mendy Schuster * Brian R. Wilson

SUSANVILLE COMMUNITY DEVELOPMENT AGENCY SUSANVILLE MUNICIPAL ENERGY CORPORATION
SUSANVILLE PUBLIC FINANCING AUTHORITY

Susanville City Council
Regular Meeting ♦ City Council Chambers
April 17, 2019 – 6:00 p.m.

Call meeting to order

Roll call of Councilmembers present

Next Resolution No. 19-5643

Next Ordinance No. 19-1015

- 1 APPROVAL OF AGENDA: (Additions and/or Deletions)**

- 2 PUBLIC COMMENT REGARDING CLOSED SESSION ITEMS (if any): Any person may address the Council at this time upon any subject for discussion during Closed Session.**

- 3 CLOSED SESSION:**
 - A PUBLIC EMPLOYMENT – Pursuant to Government Code Section §54957
 - 1 City Attorney Performance Evaluation
 - B CONFERENCE WITH REAL PROPERTY NEGOTIATOR – pursuant to Government Code Section §54956.8
 - 1 Property: APN: 105-164-01; 103-231-08 (?)
 Agency Negotiator: Mike Wilson, City Administrator
 Negotiating Parties: Various
 Under Negotiation: Price/Conditions/Terms
 - C CONFERENCE WITH LEGAL COUNSEL, INITIATION OF LITIGATION - Pursuant to Government Code Section 54956.9(c)

- 4 RETURN TO OPEN SESSION: (recess if necessary)**
 - *Reconvene in open session at 7:00 p.m.*
 - *Pledge of allegiance*
 - *Report any changes to agenda*
 - *Report any action out of Closed Session*
 - *Moment of Silence or Thought for the Day: Councilmember Schuster*
 - *Proclamations, awards or presentations by the City Council*

- 5 BUSINESS FROM THE FLOOR:**

Any person may address the Council at this time upon any subject on the agenda or not on the agenda within the jurisdiction of the City Council. However, comments on items on the agenda may be reserved until the item is discussed and any matter not on the agenda that requires action will be referred to staff for a report and action at a subsequent meeting. Presentations are subject to a five-minute limit

6 **CONSENT CALENDAR:**

All matters listed under the Consent Calendar are considered to be routine by the City Council. There will be no separate discussion on these items. Any member of the public or the City Council may request removal of an item from the Consent Calendar to be considered separately.

- A Approve minutes from the City Council's March 20, 2019 meeting
- B Approve **Resolution No. 19-5635** authorizing execution of agreement with Van Lant & Fankhanel, LLP for auditing services
- C Approve **Resolution No. 19-5640** authorizing the Finance Department to establish a restricted fund for the annual \$15,000 Federal Aviation Administration (FAA) entitlement grant match fund
- D Approve write-off of Accounts Receivable
- E Approve **Resolution No. 19-5637** approving acceptance of gift from Susanville Police Officers Association
- F Approve **Resolution No. 19-5639** approving legal services agreement with Prentice, Long & Epperson P.C.
- G Approve **Resolution No. 19-5641** approving acceptance of cash donation from WalMart

7 **PUBLIC HEARINGS:** No business.

8 **COUNCIL DISCUSSION/ANNOUNCEMENTS:**

Commission/Committee Reports:

9 **NEW BUSINESS:**

- A Consider approval of vendor warrants numbered 203616 through 203674 for a total of \$267,640.32 including \$122,747.01 in payroll warrants
- B Consider **Ordinance No. 19-1014** amending Chapter 8.28 Weed and Rubbish Abatement, Section 8.28.040 Abatement Procedure, of the Susanville Municipal Code; waive second reading and adopt
- C Consider **Resolution No. 19-5636** approving Operational Agreement with Lassen County
- D Consider approval of Water Main Replacement at the Johnstonville Road and Skyline Drive Intersection
- E Consider appointment of representatives to the Design Review Committee
- F Consider **Resolution No. 19-5626** accepting Paul Bunyan Connectivity Grant for funding infrastructure improvements
- G Consider discussion regarding establishing a City of Susanville Mission Statement; provide direction to staff
- H Consider **Resolution No. 19-5642** approving the School Resource Officer (SRO) agreement with Lassen Union High School

10 **SUSANVILLE COMMUNITY DEVELOPMENT AGENCY:** No business.

11 **SUSANVILLE MUNICIPAL ENERGY CORPORATION:** No business.

12 CONTINUING BUSINESS:

- A Consider approval of Final Design and Bid Documents for Fruit Growers Park Rehabilitation Project
- B Discussion and direction regarding the location of a Dog Park

13 CITY ADMINISTRATOR'S REPORTS:

- A Administrative Services Update
- B Food Truck Update

14 COUNCIL ITEMS:

- A AB1234 travel reports:

15 ADJOURNMENT:

- *The next regular meeting of the Susanville City Council will be held on May 1, 2019 at 6:00 p.m.*

Reports and documents relating to each agenda item are on file in the Office of the City Clerk and are available for public inspection during normal business hours and at the meeting. These reports and documents are also available at the City's website www.cityofsusanville.org, unless there were systems problems posting to the website.

Accessibility: An interpreter for the hearing-impaired may be made available upon request to the City Clerk seventy-two hours prior to a meeting. A reader for the vision-impaired for purposes of reviewing the agenda may be made available upon request to the City Clerk. The location of this meeting is wheelchair-accessible.

I, Gwenna MacDonald, certify that I caused to be posted notice of the regular meeting scheduled for April 17, 2019 in the areas designated on April 12, 2019.


Gwenna MacDonald, City Clerk

Reviewed by:  City Administrator

- Motion Only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted By: Gwenna MacDonald, City Clerk

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: Minutes of the City Council's March 20, 2019 meeting.

PRESENTED BY: Gwenna MacDonald, City Clerk

SUMMARY: Attached for the Council's review are the minutes of the City Council's March 20, 2019 meeting.

FISCAL IMPACT: None.

ACTION REQUESTED: Motion to waive oral reading and approve minutes of City Council's March 20, 2019 meeting.

ATTACHMENTS: Minutes: March 20, 2019

SUSANVILLE CITY COUNCIL
Regular Meeting Minutes
March 20, 2019– 6:00 p.m.

Meeting was called to order at 6:00 p.m. by Mayor Stafford.

Roll call of Councilmembers present: Brian Wilson, Joseph Franco, Brian Moore, Mendy Schuster and Mayor Kevin Stafford.

Staff present: Mike Wilson, City Administrator and Gwenna MacDonald, City Clerk.

1 APPROVAL OF AGENDA:

Mr. Wilson requested the removal of Item 9G, consider approving the purchase and install of a gate for the Pat Murphy Little League Ball Park.

Motion by Mayor pro tem Franco, second by Councilmember Moore, to approve the agenda with the change requested; motion carried unanimously. Ayes: Franco, Schuster, Wilson, Moore and Stafford.

2 PUBLIC COMMENT REGARDING CLOSED SESSION ITEMS: No comments.

3 CLOSED SESSION: At 6:01 p.m. the Council entered into Closed Session to discuss the following:

- A PUBLIC EMPLOYMENT – Pursuant to Government Code Section §54957
 - 1 Honey Lake Valley Recreation Authority (HLVRA) Executive Officer

4 RETURN TO OPEN SESSION:

At 7:00 p.m. the City Council reconvened in Open Session.

Staff present: Mike Wilson, City Administrator; Dan Newton, Public Works Director; Ryan Cochran, Police Captain; James Moore, Fire Chief; Quincy McCourt, Project Manager and Gwenna MacDonald, City Clerk.

Mr. Wilson reported that prior to Closed Session, the agenda was approved with the removal of Item 9G. The City Council met in Closed Session, provided direction to staff and there was no reportable action taken.

Councilmember Wilson provided the Thought of the Day.

Mayor Stafford read a proclamation observing April 2019 as Child Abuse and Assault Prevention Month.

5 BUSINESS FROM THE FLOOR:

Kerri Cobb, Lassen Family Services, accepted the proclamation and discussed events that were planned to raise awareness and prevention for child abuse and assault, including the Walk a Mile in Their Shoes event planned for April 13, 2019.

Dean Growden, Lassen County Sheriff, thanked Chief Moore for the cooperative effort that exists between the two agencies, specifically mentioning Captain Rob Brown, Dale Johnson, and Nick McBride of the Susanville Fire Department, who assisted with an emergency involving a swift water recovery effort. He provided an update to the City Council regarding activity at the Lassen County Sheriff's office.

John Ripley requested an update regarding the proposed dog park, and offered to volunteer his time towards the development of the park.

Councilmember Wilson requested the removal of Item 6C for separate discussion.

6 **CONSENT CALENDAR:**

- A Approve minutes from the City Council's February 6 and 20, 2019 meetings
- B Receive and file Finance Reports: February 2019
- C Receive and file Golf Course Update

Motion by Mayor pro tem Franco, second by Councilmember Schuster to approve Item 6A and 6B; motion carried unanimously. Ayes: Franco, Schuster, Moore, Wilson and Stafford.

Councilmember Wilson stated that in reviewing the golf course report, he was concerned that the rounds played numbers seem to be diminishing since the 2014/2015 year, particularly after all of the positive feedback received regarding the condition of the course last year. He asked if the negative cash balance was zeroed out from the 2017/2018 year.

There was a general discussion regarding a potential operator for the restaurant, and opportunities for improving the use of the course for the upcoming season.

Motion by Councilmember Wilson, second by Mayor pro tem Franco, to approve Item 6C; motion carried unanimously. Ayes: Wilson, Schuster, Moore, Franco and Stafford.

7 **PUBLIC HEARINGS:** No business.

8 **COUNCIL DISCUSSION/ANNOUNCEMENTS:** Commission/Committee reports: No business.

9 **NEW BUSINESS:**

9A **Consider approval of vendor warrants numbered 203386 through 203534 for a total of \$348,109.70 including \$183,011.52 in payroll warrants**

Motion by Councilmember Schuster, second by Councilmember Moore, to approve the vendor warrant report; motion carried unanimously. Ayes: Schuster, Moore, Wilson, Franco and Stafford.

9B **Consider approval of Fee Waiver Request for Lassen Family Services' 6th annual Walk-a-Mile Event to be held on April 13, 2019** Mr. Wilson reviewed the request submitted by Lassen Family Services to waive the use fees for Memorial Park and electrical panel deposit for the annual Walk-a-Mile event scheduled for April 13th at 10:00 a.m. The total fee waiver is \$226.

Motion by Mayor pro tem Franco, second by Councilmember Moore, to approve the fee waiver request; motion carried unanimously. Ayes: Franco, Moore, Wilson, Schuster and Stafford.

9C **Consider approval of Resolution No. 19-5623 authorizing staff to select up to six volunteer workers to participate in the Work for Golf Program for a period of April 1, 2019 through November 2019** Mr. Wilson explained that Work for Golf Program provides an opportunity for the City to increase the workforce needed to maintain the Golf Course, in exchange for providing an annual membership pass. Workers will be supervised by the Golf Course Superintendent, and the volunteers will perform the essential functions of the Grounds Maintenance Worker. A job description has been prepared, and staff proposes to select up to six volunteer workers to begin the program from April 1 through November 2019. There were no questions or comments.

Motion by Mayor pro tem Franco, second by Councilmember Schuster, to approve Resolution No. 19-5623; motion carried unanimously. Ayes: Franco, Schuster, Wilson, Moore and Stafford.

9D Consider approval of Resolution No. 19-5624 considering resolution to adopt HOME guidelines, combining the HOME First-Time Homebuyer program and the HOME Owner-Occupied Rehabilitation program with the CDBG Homebuyer assistance program Mr. McCourt explained that the City has established a homebuyer assistant program funded through the HOME and CDBG Programs. The intent of the program is to assist low-income residents in purchasing their own home. In addition, an Owner-Occupied Rehabilitation program is proposed as a component of the CDBG Homebuyer assistance program. Mr. McCourt reviewed criteria for the program as established by the proposed Program Guidelines, which are required by the State to provide a guideline to administer the programs. The State provides a guideline template to agencies in order to ensure that the money is spent in accordance with State and Federal requirements, and jurisdictions must review and adopt the Guidelines by resolution. He discussed the public outreach and next steps that are required prior to accepting applications from interested homebuyers. There is a current combined balance of \$469,231.40, and he requested feedback from the City Council regarding establish a maximum loan amount for each participant, with consideration to the preference of re-investing the money quickly into the community, versus helping the maximum number of applicants.

Councilmember Wilson asked if the City would be eligible to apply for more funding during the next Notice of Available Funding (NOFA) cycle, and if the Housing Element compliance would be a factor.

Mr. McCourt reviewed the NOFA timeline, and the effort towards updating the Housing Element.

There was an extensive general discussion regarding marketing efforts, public outreach, and the merits of providing assistance to more participants versus the time requirements for expending Program Income and getting the money back into the community. It was the consensus to establish a \$50,000 maximum loan amount.

Motion by Councilmember Wilson, second by Councilmember Schuster, to approve Resolution No. 19-5624; motion carried unanimously. Ayes: Wilson, Schuster, Franco, Moore and Stafford.

9E Consider authorizing the use of interns for CalRecycle Program and State Route 36 Safe Mobility Study Mr. McCourt reviewed the success of the City's grant task force, and the projects that staff has been working on utilizing the interns. The Council authorized the use of interns for projects that are completely reimbursable and the projects have been moving towards completion through this added staffing. The CalRecycle program and State Route 36 Safe Street Mobility Study are two additional programs that staff will be utilizing the intern workforce to implement. The City has a balance of \$15,000 through CalRecycle that has been utilized in the past for community clean up education and events. The Susanville Communities Study has been funded by Caltrans in the amount of \$8,000, and interns will be utilized for community outreach, data entry and other activities in support of conducting the study. This process is valuable to demonstrate success in utilizing funding, which in turn will put the City in a good position moving forward to receive additional funding.

Motion by Councilmember Schuster, second by Mayor pro Franco, to utilize intern staffing for the CalRecycle Grant and Safe Street Mobility Study; motion carried unanimously. Ayes: Schuster, Franco, Moore, Wilson and Stafford.

9F Discussion and direction to prepare budget for the 10 percent match of the upcoming larger airport projects Mr. McCourt explained that the Susanville Municipal Airport receives \$150,000 per year of entitlement funding through the Federal Aviation Administration (FAA) to pay for capital improvement projects. The funding, which can accumulate for four years, requires a ten percent match which the City budgets for annually. Mr. McCourt reviewed the upcoming projects that are approved as part of the Airport Capital Improvement Plan from 2020 through 2023, and based on those estimated

costs and required project match, the City will have a shortfall in the amount of project match needed to complete those projects. Mr. McCourt suggested that if the Council chooses to allocate \$15,000 additional funding in 2019 and 2020, and an additional \$10,000 in 2021, 2022 and 2023, then the City will have adequate project match for the entitlement funding received from the FAA.

The City Council discussed funding the projects at the Airport, the use of in-kind services as project match, the availability of matching funds through Caltrans, and the use of FAA funding to develop Capital Improvement Plans for the Airport. Mr. McCourt suggested that another option to consider would be to restrict the \$15,000 that is budgeted each year, so that it will accumulate and only be utilized for the City's required match. The Council discussed the proposed fencing project that was recommended to keep free-range cattle from the adjacent property.

It was the consensus of the City Council to bring back a resolution restricting use of \$15,000 which is budgeted annually.

9G Consider approving the purchase and install of a gate for the Pat Murphy Little League Ball Park
Removed.

10 SUSANVILLE COMMUNITY DEVELOPMENT AGENCY: No business.

11 SUSANVILLE MUNICIPAL ENERGY CORPORATION: No business.

12 CONTINUING BUSINESS: No business.

13 CITY ADMINISTRATOR'S REPORTS:

13A Main Street Construction Project Update Mr. Newton provided an update regarding the Main Street construction project. The conditions are poor, and generating a lot of negative feedback from the public. The Department started a Facebook page in order to keep the public informed regarding the status of projects. The Cap M project initiated by Caltrans and the Water Main Replacement project between Weatherlow and Park Street are under winter suspension. The weather conditions over the past few months have created additional problems with potholes and erosion. As soon as weather allows the crews to get back to work, the streets should be looking better by early to mid-summer.

14 COUNCIL ITEMS:

14A AB1234 travel reports:

Councilmember Moore requested bringing an item back to address allowing food trucks in the City.

Mayor pro tem Franco requested an update on the park projects, and a status update regarding the acquisition of the triangle piece of property near Fruitgrowers Park.

Councilmember Schuster commented that she would like to discuss the property maintenance ordinance, particularly in reference to the Hotel Mount Lassen building.

Councilmember Wilson added that it would be a good idea to look at the entire zoning section of the Susanville Municipal Code, and discussed the opportunity for an indoor shooting range in the community.

15 ADJOURNMENT:

Motion by Councilmember Schuster second by Councilmember Moore, to adjourn; motion carried unanimously. Ayes: Franco, Schuster, Moore, Wilson and Stafford.

Meeting adjourned at 8:49 p.m.

Kevin Stafford, Mayor

Respectfully submitted by

Approved on: _____

Gwenna MacDonald, City Council

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted By: Deborah Savage, Finance Manager

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: **Resolution No. 19-5635** Approving and authorizing the Mayor to execute three-year agreement for auditing services with Van Lant and Fankhanel, LLP for the fiscal years 2018/2019, 2019/2020 and 2020/2021.

PRESENTED BY: Deborah Savage, Finance Manager

SUMMARY: At the April 6th meeting, staff presented the City Council with the results of the RFP for auditing services and requested direction on their choice of auditing firms. The City Council chose the auditing firm of Van Lant and Fankhanel, LLP and staff is presenting the resolution and agreement to formalize the selection.

FISCAL IMPACT: Contract for auditing services for FY 2018/2019 not to exceed \$33,500 for the City Audit (\$27,000), Single Audit (\$3,500) and Lassen County Air Pollution Control District Audit (\$3,000)

ACTION REQUESTED: Motion to approve Resolution No. 19-5635, Approving and authorizing the City Administrator to execute agreement for auditing services with Van Lant and Fankhanel, LLP for the fiscal years 2018/2019, 2019/2020 and 2020/2021.

ATTACHMENTS: Resolution No. 19-5635
Agreement for auditing services with Van Lant and Fankhanel, LLP Certified Public Accountant's

RESOLUTION NO. 19-5635
A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SUSANVILLE
APPROVING AND AUTHORIZING THE MAYOR TO EXECUTE THREE-YEAR
AGREEMENT FOR PROFESSIONAL AUDITING SERVICES WITH VAN LANT &
FANKHANEL, LLP FOR FISCAL YEARS 2018/2019, 2019/2020 and 2020/2021

WHEREAS, the City of Susanville is required to have an annual audit of its financial statements;

WHEREAS, the City Council has determined that auditing services are best provided by Badawi & Associates, CPAs;

WHEREAS, Van Lant & Fankhanel, LLP, CPAs have agreed to perform auditing services for the City of Susanville.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Susanville hereby approves and authorizes the Mayor to execute a three-year agreement for professional auditing services for the fiscal years 2018/2019, 2019/2020 and 2020/2021.

APPROVED: _____
Kevin Stafford, Mayor

ATTEST: _____
Gwenna MacDonald, City Clerk

The foregoing Resolution No. 19-5635 was adopted at a regular meeting of the City Council of the City of Susanville, held on the 17thth day of April 2019 by the following vote:

AYES:
NOES:
ABSENT:
ABSTAINING:

Gwenna MacDonald, City Clerk

APPROVED AS TO FORM:

Jessica Ryan, City Attorney

**AGREEMENT FOR AUDITING SERVICES
BETWEEN THE CITY OF SUSANVILLE AND
VAN LANT & FANKHANEL, LLP**

THIS CONSULTANT SERVICES AGREEMENT is made and effective as of April 17, 2019 between the City of Susanville, a municipal corporation ("City") and Van Lant & Fankhanel, LLP Certified Public Accountants. ("Consultant"). In consideration of the mutual covenants and conditions set forth herein, the parties agree as follows:

1. **TERM**

This Agreement shall commence on April 17, 2019, and shall remain in effect for three years with a renewal option for two additional years, unless sooner terminated pursuant to the provisions of this Agreement.

2. **SERVICES**

Consultant shall perform the tasks described and set forth in the Request for Proposal, attached hereto and incorporated herein as though set forth in full as part of its services. Consultant may be asked to provide additional services if necessary during the term of this Agreement.

3. **PERFORMANCE**

Consultant shall at all times faithfully, competently and to the best of his/her ability, experience, and talent, perform all tasks described herein. Consultant shall employ, at a minimum, generally accepted standards and practices utilized by persons engaged in providing similar services as are required of Consultant hereunder in meeting its obligations under this Agreement.

4. **PAYMENT**

(a) The City agrees to pay Consultant in accordance with the payment rates and terms and the schedule of payment as set forth in Appendix B in the Request for Proposal, attached hereto and incorporated herein by this reference as though set forth in full, based upon actual time spent on the tasks outlined in the RFP, attached hereto and incorporated herein by this reference. This amount shall not exceed dollars \$33,500 (FY 2018-19), \$34,170 (FY 2019-20), \$34,853 (FY 2020-21), \$35,550 (Optional extension FY 2021-22) and \$36,261 (Optional extension FY 2022-23) for the services of this Agreement unless additional payment is approved as provided in this Agreement.

(b) Consultant shall not be compensated for any services rendered in connection with its performance of this Agreement which are in addition to those set forth herein, unless such additional services are authorized in advance and in writing by the City Administrator. Consultant shall be compensated for any additional services in the

amounts and in the manner as agreed to by the City Administrator and at the time the City's written authorization is given to Consultant for the performance of said services.

(c) Consultant will submit invoices for actual services performed. Payment shall be made no sooner than thirty (30) days of receipt of each invoice as to all non-disputed fees. If the City disputes any of Consultant's fees it shall give written notice to Consultant within thirty (30) days of receipt of an invoice of any disputed fees set forth on the invoice.

5. **KEY PERSONNEL CLAUSE** Consultant will provide the City with a list of the key personnel to be assigned to perform the audit. Key personnel will include the Engagement Partner, Audit Manager and Senior Auditor. Consultant agrees that any changes in key personnel must be approved by the City in writing prior to any changes. Any change in key personnel will allow the City to renegotiate the current contract.

6. **SUSPENSION OR TERMINATION OF AGREEMENT WITHOUT CAUSE**

(a) The City may at any time, for any reason, with or without cause, suspend or terminate this Agreement, or any portion hereof, by serving upon the consultant at least ten (10) days prior written notice. Upon receipt of said notice, the Consultant shall immediately cease all work under this Agreement, unless the notice provides otherwise. If the City suspends or terminates a portion of this Agreement such suspension or termination shall not make void or invalidate the remainder of this Agreement.

(b) In the event this Agreement is terminated pursuant to this Section, the City shall pay to Consultant the actual value of the work performed up to the time of termination, provided that the work performed is of value to the City. Upon termination of the Agreement pursuant to this Section, the Consultant will submit an invoice to the City.

7. **DEFAULT OF CONSULTANT**

(a) The Consultant's failure to comply with the provisions of this Agreement shall constitute a default. In the event that Consultant is in default for cause under the terms of this Agreement, City shall have no obligation or duty to continue compensating Consultant for any work performed after the date of default and can terminate this Agreement immediately by written notice to the Consultant. If such failure by the Consultant to make progress in the performance of work hereunder arises out of causes beyond the Consultant's control, and without fault or negligence of the Consultant, it shall not be considered a default.

(b) If the City Administrator determines that the Consultant is in default in the performance of any of the terms or conditions of this Agreement, he/she shall cause to be served upon the Consultant a written notice of the default. The Consultant shall have ten (10) days after service upon it of said notice in which to cure the default by rendering a satisfactory performance. In the event that the Consultant fails to cure its default within such period of time, the City shall have the right, notwithstanding any other provision of this Agreement, to terminate this Agreement without further notice and without prejudice to any other remedy to which it may be entitled at law, in equity or under this Agreement.

8. **OWNERSHIP OF DOCUMENTS**

(a) Consultant shall maintain complete and accurate records with respect to sales, costs, expenses, receipts, and other such information required by City that relate to the performance of services under this Agreement. Consultant shall maintain adequate records of services provided in sufficient detail to permit an evaluation of services. All such records shall be maintained in accordance with generally accepted accounting principles and shall be clearly identified and readily accessible. Consultant shall provide free access to the representatives of City or its designees at reasonable times to such books and records; shall give City the right to examine and audit said books and records; shall permit City to make transcripts there from as necessary; and shall allow inspection of all work, data, documents, proceedings, and activities related to this Agreement. Such records, together with supporting documents, shall be maintained for a period of three (3) years after receipt of final payment.

(b) Upon completion of, or in the event of termination or suspension of this Agreement, all original documents, designs, drawings, maps, models, computer files, surveys, notes, and other documents prepared in the course of providing the services to be performed pursuant to this Agreement shall become the sole property of the City and may be used, reused, or otherwise disposed of by the City without the permission of the Consultant.

9. **INDEMNIFICATION**

When the law establishes a professional standard of care for Consultant's Services, to the fullest extent permitted by law, Consultant shall indemnify, protect, defend and hold harmless City and any and all of its officials, employees and agents ("Indemnified Parties") from and against any and all losses, liabilities, damages, costs and expenses, including attorney's fees and costs to the extent same are caused in whole or in part by any negligent or wrongful act, error or omission of Consultant, its officers, agents, employees or subconsultants (or any entity or individual that Consultant shall bear the legal liability thereof) in the performance of professional services under this Agreement.

10. **INSURANCE**

- A) Consultant shall maintain in-force: Public Liability and Professional Liability Insurance. During the term of this Agreement, Consultant shall maintain in full force and effect a policy of public liability insurance with minimum coverage of \$1,000,000 in accordance with the requirements provided by CITY to Consultant. Consultant shall also maintain in-force; Professional Liability Insurance (and/or Errors & Omissions Insurance) with minimum limits of liability of \$1,000,000 combined single limit coverage against an injury, death, loss or damages because of wrongful or negligent acts or omissions by the named insured.
- B) Contractor shall maintain in-force Workers' Compensation and Employer's Liability Insurance as required by the California Labor Code. Evidence of coverage shall take the form of a Certificate of Insurance or a California Certificate to Self-Insure. Acceptable minimum limits for this

coverage are: Workers' Compensation; Statutory in California; Employer's Liability: \$1,000,000.

- C) Certificates of Insurance. Contractor shall furnish to Customer evidence of any insurance required by this Agreement. A Certificate of Insurance from an insurer admitted to do business in the State of California will be provided, indicating that the respective policy(s) meets the following requirements: (1) The City, its officers, employees, and agents shall be named as additional insured on the General Liability Insurance; (2) Insurance shall not be canceled or terminated without 30 days written notice to City; (3) General Liability shall be primary and any insurance held by City for its own protection shall be excess and shall be effective only upon exhaustion of Contractor's insurance; (4) Insurance shall be maintained for the duration of the Agreement, including any period extended beyond the expiration date of this Agreement required to complete performance as stipulated in this Agreement and all amendments thereto.

11. **INDEPENDENT CONSULTANT**

(a) Consultant is and shall at all times remain as to the City a wholly independent Consultant. The personnel performing the services under this Agreement on behalf of Consultant shall at all times be under Consultant's exclusive direction and control. Neither City nor any of its officers, employees, or agents shall have control over the conduct of Consultant or any of Consultant's officers, employees, or agents, except as set forth in this Agreement. Consultant shall not at any time or in any manner represent that it or any of its officers, employees, or agents are in any manner officers, employees, or agents of the City. Consultant shall not incur or have the power to incur any debt, obligation, or liability whatever against City, or bind City in any manner.

(b) No employee benefits shall be available to Consultant in connection with the performance of this Agreement. Except for the fees paid to Consultant as provided in the Agreement, City shall not pay salaries, wages, or other compensation to Consultant for performing services hereunder for City. City shall not be liable for compensation or indemnification to Consultant for injury or sickness arising out of performing services hereunder.

12. **LEGAL RESPONSIBILITIES**

The Consultant shall keep itself informed of State and Federal laws and regulations which in any manner affect those employed by it or in any way affect the performance of its service pursuant to this Agreement. The Consultant shall at all times observe and comply with all such laws and regulations. The City, and its officers and employees, shall not be liable at law or in equity occasioned by failure of the Consultant to comply with this Section.

13. **UNDUE INFLUENCE**

Consultant declares and warrants that no undue influence or pressure is used against or in concert with any officer or employee of the City of Susanville in connection with the award, terms or implementation of this Agreement, including any method of coercion, confidential financial arrangement, or financial incentive. No officer or employee of the City of Susanville will receive compensation, directly or indirectly, from Consultant, or from any officer, employee or agent of Consultant, in connection with the award of this Agreement or any work to be conducted as a result of this Agreement. Violation of this Section shall be a material breach of this Agreement entitling the City to any and all remedies at law or in equity.

14. **NO BENEFIT TO ARISE TO LOCAL EMPLOYEES**

No member, officer, or employee of City, or their designees or agents, and no public official who exercises authority over or responsibilities with respect to the Project during his/her tenure or for one year thereafter, shall have any interest, direct or indirect, in any agreement or sub-agreement, or the proceeds thereof, for work to be performed in connection with the Project performed under this Agreement.

15. **RELEASE OF INFORMATION/CONFLICTS OF INTEREST**

(a) All information gained by Consultant in performance of this Agreement shall be considered confidential and shall not be released by Consultant without City's prior written authorization. Consultant, its officers, employees, agents, or subconsultants, shall not without written authorization from the City Administrator or unless requested by the City Attorney, voluntarily provide declarations, letters of support, testimony at depositions, response to interrogatories, or other information concerning the work performed under this Agreement or relating to any project or property located within the City. Response to a subpoena or court order shall not be considered "voluntary" provided Consultant gives City notice of such court order or subpoena.

(b) Consultant shall promptly notify City should Consultant, its officers, employees, agents, or subconsultants be served with any summons, complaint, subpoena, notice of deposition, request for documents, interrogatories, request for admissions, or other discovery request, court order, or subpoena from any person or party regarding this Agreement and the work performed there under or with respect to any project or property located within the City. City retains the right, but has no obligation, to represent Consultant and/or be present at any deposition, hearing, or similar proceeding. Consultant agrees to cooperate fully with City and to provide the opportunity to review any response to discovery requests provided by Consultant. However, City's right to review any such response does not imply or mean the right by City to control, direct, or rewrite said response.

16. **ASSIGNMENT**

The Consultant shall not assign the performance of this Agreement, nor any part thereof, nor any monies due hereunder, without prior written consent of the City because of the personal nature of the services to be rendered pursuant to this Agreement.

17. **LICENSES**

At all times during the term of this Agreement, Consultant shall have in full force and effect, all licenses required of it by law for the performance of the services described in this Agreement including a city business license as required by City of Susanville Municipal Code.

18. **GOVERNING LAW**

The City and Consultant understand and agree that the laws of the State of California shall govern the rights, obligations, duties, and liabilities of the parties to this Agreement and also govern the interpretation of this Agreement. Any litigation concerning this Agreement shall take place in the municipal, superior, or federal district court with jurisdiction over the City of Susanville.

19. **ENTIRE AGREEMENT**

This Agreement contains the entire understanding between the parties relating to the obligations of the parties described in this Agreement. All prior or contemporaneous agreements, understandings, representations, and statements, oral or written, are merged into this Agreement and shall be of no further force or effect. Each party is entering into this Agreement based solely upon the representations set forth herein and upon each party's own independent investigation of any and all facts such party deems material.

20. **AUTHORITY TO EXECUTE THIS AGREEMENT**

The person or persons executing this Agreement on behalf of Consultant warrants and represents that he/she has the authority to execute this Agreement on behalf of the Consultant and has the authority to bind Consultant to the performance of its obligations hereunder.

21. **SEVERABILITY**

If any part of this agreement is deemed unenforceable or invalid under the rule of law, that part of this agreement will be regarded as stricken and the remainder of this agreement will remain in full force and effect.

IN WITNESS WHEREOF, the parties hereto have caused this Agreement to be executed the day and year first above written.

CITY OF SUSANVILLE:

CONSULTANT

Kevin Stafford
Mayor
City of Susanville

By: _____
Greg Fankhanel
Partner
Van Lant and Fankhanel, LLP
Certified Public Accountants

APPROVED AS TO FORM:

ATTEST:

Jessica Ryan
City Attorney
City of Susanville

Gwenna MacDonald
City Clerk
City of Susanville

All Inclusive Maximum Price

Proposing Firm:

Van Lant and Fankhanel, LLP
 25901 Kellogg Street
 Loma Linda, CA 92354

Contact: Greg Fankhanel, CPA, CFE, Audit Partner
 E-mail: gfankhanel@vlfcpa.com
 Phone: 909-856-6879

Total All-Inclusive Maximum Price for Each Fiscal Year

The schedule of professional fees and total all-inclusive maximum price for the audit services to be provided to the City for the fiscal years ending June 30, 2019 through June 30, 2021, and optional years ending June 30, 2022 and June 30, 2023 are provided below:

Schedule of Professional Fees and Expenses to Support the Total All-Inclusive Maximum Price				
	Hours	Standard Hourly Rates	Quoted Hourly Rates	Total
Partners	125	\$ 225	\$ 195	\$ 24,375
Managers	35	165	145	5,075
Supervisory Staff	100	135	120	12,000
Senior Accountant	90	105	95	8,550
Sub-Total				\$ 41,450
Out-of-Pocket Expenses				-
Discount				(7,950)
Total				\$ 33,500

All-Inclusive Maximum Price by Report	FY 2018-19	FY 2019-20	FY 2020-21	OPTIONAL	
				FY 2021-22	FY 2022-23
City	\$ 27,000	\$ 27,540	\$ 28,091	\$ 28,653	\$ 29,226
Lassen County Air Pollution Control District Audit	3,000	3,060	3,121	3,184	3,247
Single Audit	3,500	3,570	3,641	3,714	3,789
Total	\$ 33,500	\$ 34,170	\$ 34,853	\$ 35,550	\$ 36,261

Payment Terms

Invoices for our services will be submitted at two stages each fiscal year: one invoice subsequent to our preliminary audit work for approximately 1/3 of the total price (in August of each year), and the second invoice for the remainder after all reports have been issued.

Rates for Additional Professional Service

VLF agrees the City may broaden the scope of our engagement and we agree to hold ourselves available to perform such additional work as the City may desire. While we normally will negotiate additional fees for a scope change, the schedule at Appendix A above includes our standard hourly billing rates by staff level, should additional services be requested, along with the estimated hours by category for the normal audit services.

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: Quincy McCourt, Project Manager

Action Date: April 17, 2018

CITY COUNCIL AGENDA ITEM

SUBJECT: **Resolution No. 19-5640** authorizing the Finance Department to establish a restricted fund to be utilized for the annual FAA Entitlement Project Match

PRESENTED BY: Quincy McCourt, Project Manager

SUMMARY: Currently the City of Susanville budgets \$15,000 annually paid from the General Fund at a rate of \$1,250 each month. The money is intended to be used for the local project match when applying for Federal Aviation Administration (FAA) entitlement grants. Staff is requesting restricting the annual \$15,000 contribution be used to only be invested as the local match FAA entitlement grant match.

FISCAL IMPACT: \$15,000 budgeted annually

ACTION REQUESTED: Motion to approve Resolution No. 19-5640 authorizing the Finance Department to establish a restricted fund for the annual FAA Entitlement Project Match budgeted annually at \$15,000 and authorizing a budget transfer of \$32,869.69 to the Airport Restricted Fund.

ATTACHMENTS: Resolution No. 19-5640

RESOLUTION NO. 19-5640
A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SUSANVILLE
APPROVING THE ESTABLISHMENT OF A RESTRICTED FUND

WHEREAS, the City of Susanville receives entitlement funding each year from the Federal Airline Administration (FAA) in the amount of \$150,000 for Capital Improvement Projects at the Susanville Municipal Airport; and

WHEREAS, said entitlement funding requires that the City contribute 10 percent matching funding; and

WHEREAS, the City established a line item to set aside \$15,000 each year to budget for the project match; and

WHEREAS, the City Council has identified a need to restrict the annual amount budgeted to be utilized specifically for making the project match required by the FAA; and

WHEREAS, the Finance Manager is directed to establish a restricted fund and amend the FY 2018/2019 budget accordingly.

NOW THEREFORE, BE IT RESOLVED that the City Council of the City of Susanville authorizes:

1. The Finance Manager to create a restricted fund specifically for the Airport Project Match Fund; and
2. The transfer of the balance of \$32,869.69 from the Airport Fund to the Airport Restricted Fund

APPROVED: _____
Kevin Stafford, Mayor

ATTEST: _____
Gwenna MacDonald, City Clerk

The foregoing Resolution No. 19-5640 was adopted at a regular meeting of the City Council of the City of Susanville, held on the 17th day of April, 2019 by the following vote:

AYES:
NOES:
ABSENT:
ABSTAINING:

Gwenna MacDonald, City Clerk

APPROVED AS TO FORM: _____
Jessica Ryan, City Attorney

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted By: Deborah Savage, Finance Manager

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: Accounts receivable write-off

PRESENTED BY: Deborah Savage, Finance Manager

SUMMARY: The Administrative Services Department is responsible for the collection of revenues that are owed to the City. When accounts become past due, the City follows certain procedures for the collection of these accounts such as sending delinquent letters, searching for new accounts in the utility billing system or performing a search using our current credit reporting agency resources and calling phone numbers from the closed accounts. After all attempts have failed, staff brings a report to the City Council requesting approval to remove the uncollectible balances from our books and send them on to a collection agency.

Our collections agency has access to additional records and has been successful in recovering additional funds. They have also informed us that accounts received in the first 6-12 months have the highest potential for recovery. As delinquent accounts age, there are limitations placed on collection options. The City receives 66% of delinquencies collected and there is no additional fee charged for this service.

At this time we are requesting that the Council approve writing off \$2,554.94 in uncollected utility revenue thru March 18, 2019 and \$8,268.79 in accounts receivable billing for uncollected commercial operator fee.

FISCAL IMPACT: Write off \$10,823.73 in potential revenue with a portion to be later recovered through collections.

ACTION REQUESTED: Motion to approve the write-off of accounts receivable and send \$10,823.73 to collections.

ATTACHMENTS: Report of uncollectible amounts

3/18/2019

ACCOUNT #	TOTAL AFTER DEP. APPLIED	WATER	GAS	Deposits	ACTION TAKEN
104.2235.00.08	\$157.43	\$42.25	\$115.22	Applied	No new address found in UE, mailed returned. L/M for call back
104.1600.00.04	\$126.79	\$102.74	\$24.05	Applied	New address found in UE, no response. Phone number on file does not belong to customer
103.2335.00.28	\$100.20	\$89.38	\$10.82	Applied	New address found in UE, mail returned. No working number on file.
101.1160.00.13	\$298.97	\$16.70	\$282.27	Applied	Called customer, No voice mail box set up. No new address found in UE
102.9013.00.08	\$197.34	\$119.43	\$77.91	None	No new address found in UE, moved out of state, no working number on file
104.1335.08.04	\$130.14		\$130.14	Applied	No new address found in UE. L/M for customer to call back.
102.8107.00.04	\$202.44	\$153.77	\$48.67	Applied	No new address found in UE
104.3835.00.27	\$298.69	\$53.90	\$244.78	None	No new address found in UE. No working number on file.
103.0560.00.33	\$315.41	\$90.65	\$224.38	Applied	Spoke with customer, customer is refusing to pay
101.0295.01.42	\$206.63		\$206.63	Applied	New address found out of state no response. No working number on file
101.0620.00.04	\$262.81	\$145.80	\$117.01	Applied	No new address found in UE. L/M no call back from customer
102.1230.00.10	\$141.61	\$70.80	\$70.81	Applied	No new address found in UE. No working number on file.
103.2225.00.18	\$116.48	\$116.48		None	Spoke with customer, customer was to call back to make payments, customer has not followed though.
4414	\$8,268.79			None	Commercial Operator

Running Total \$10,823.73

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: Kevin Jones, Police Chief

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: Resolution No. 19-5637 approving acceptance of gift from Susanville Police Officers Reserve Association

PRESENTED BY: Kevin Jones, Police Chief

SUMMARY: The now defunct Susanville Police Reserves Association possessed a banking account at the Lassen Co. Federal Credit Union. The Account has been abandoned since 1995 but still maintained a balance of \$2,313.01. The Credit Union considered the account abandoned and allowed its use by the Chief of Police.

Based on such, I appropriated \$1,661.52 from the account to purchase thirty (30) badges for members of the Police Department. The remainder of the funds (\$651.62) has been given to Crimestoppers, a local organization that provides rewards for information on outstanding criminals.

These badges were a gift from the account and request they be accepted by the City Council as City of Susanville property.

FISCAL IMPACT: N/A

ACTION REQUESTED: Motion to approve Resolution No. 19-5637 approving acceptance of gift from Susanville Police Officers Reserve Association.

ATTACHMENTS: Resolution No. 19-5637
Account register.

RESOLUTION NO. 19-5637
A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SUSANVILLE
APPROVING ACCEPTANCE OF GIFT FROM THE SUSANVILLE POLICE
RESERVE OFFICERS ASSOCIATION

WHEREAS, the Susanville Police Department has designed and purchased new badges for all SPD Employees; and

WHEREAS, the gifts of cash or real property should be accepted by the City Council; and

WHEREAS, the Susanville Police Reserves Association had a banking account at the Lassen County Federal Credit Union and has been abandoned, with no action since 1995; and

WHEREAS, funds in the amount of \$1,661.52 were applied to the purchase of thirty (30) police badges; and

WHEREAS, the City needs to authorize the acceptance of these gifts and declare them City of Susanville property.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Susanville approves to accept the gift of thirty (30) badges from the abandoned checking account of the Susanville Police Reserve Officer Association.

APPROVED: _____
Kevin Stafford, Mayor

ATTEST: _____
Gwenna MacDonald, City Clerk

The foregoing Resolution was adopted at a regular meeting of the City Council of the City of Susanville, held on the 17th day of April, 2019 by the following vote:

AYES:
NOES:
ABSENT:
ABSTAINING:

Gwenna MacDonald, City Clerk

APPROVED AS TO FORM: _____
Jessica Ryan, City Attorney

LASSEN COUNTY FCU
 2605 RIVERSIDE DRIVE
 SUSANVILLE, CA 96130-4776
 (530) 257-7736

Notice: See insert for important information regarding
 your right to dispute errors on your statement.

ACCOUNT NUMBER: 51399
 YTD DIV RECEIVED: .69
 PAGE NUMBER: 1 of 1

142
 172

RV LOAN SPECIAL!!
 1% OFF QUALIFYING RATE, NO
 PAYMENTS FOR 60 DAYS!!

SUSANVILLE POLICE RESERVES
 1801 MAIN ST
 SUSANVILLE CA 96130-4518

172

SUMMARY OF YOUR ACCOUNTS

SUFFIX 001 ORG SHARES		SUFFIX 006 FREEDOM		SUFFIX 008 DRAFT	
STATEMENT PERIOD 03/01/19 - 03/31/19		STATEMENT PERIOD 03/01/19 - 03/31/19		STATEMENT PERIOD 03/01/19 - 03/31/19	
BEGINNING BALANCE	25.10	BEGINNING BALANCE	2,313.01	BEGINNING BALANCE	.00
DEPOSITS	0 .00	DEPOSITS	1 .13	DEPOSITS	0 .00
WITHDRAWALS	0 .00	WITHDRAWALS	1 1,661.52	WITHDRAWALS	0 .00
ENDING BALANCE	25.10	ENDING BALANCE	651.62	DRAFTS CLEARED	0 .00
DIVIDEND YEAR-TO-DATE	.00	DIVIDEND YEAR-TO-DATE	.69	ENDING BALANCE	.00
DIVIDEND THIS PERIOD	.00	DIVIDEND THIS PERIOD	.13	DIVIDEND YEAR-TO-DATE	.00
AVERAGE DAILY BALANCE	25.10	AVERAGE DAILY BALANCE	1,026.67	DIVIDEND THIS PERIOD	.00
DAYS DIVIDEND EARNED	31	DAYS DIVIDEND EARNED	31		
		ANNUAL PERCENTAGE			
		YIELD EARNED	0.15%		

SUFFIX 001 ORG SHARES
 (NO ACTIVITY)

SUFFIX 006 FREEDOM

HISTORY	DESCRIPTION	TRANSACTION AMOUNT	ACCOUNT BALANCE
DATE			
3/08/19	Badges	1,661.52-	651.49
3/31/19	DIVIDEND	.13	651.62

SUFFIX 008 DRAFT
 (NO ACTIVITY)

LASSEN COUNTY FCU
 2605 RIVERSIDE DRIVE
 SUSANVILLE, CA 96130-4776
 (530) 257-7736

Notice: See insert for important information regarding
 your right to dispute errors on your statement.

ACCOUNT NUMBER: 51399
 YTD DIV RECEIVED: .69
 PAGE NUMBER: 1 of 1

343
 172

RV LOAN SPECIAL!!
 1% OFF QUALIFYING RATE, NO
 PAYMENTS FOR 60 DAYS!!

SUSANVILLE POLICE RESERVES
 1801 MAIN ST
 SUSANVILLE CA 96130-4518

172

SUMMARY OF YOUR ACCOUNTS

SUFFIX 001 ORG SHARES		SUFFIX 006 FREEDOM		SUFFIX 008 DRAFT	
STATEMENT PERIOD 03/01/19 - 03/31/19		STATEMENT PERIOD 03/01/19 - 03/31/19		STATEMENT PERIOD 03/01/19 - 03/31/19	
BEGINNING BALANCE	25.10	BEGINNING BALANCE	2,313.01	BEGINNING BALANCE	.00
DEPOSITS	0 .00	DEPOSITS	1 .13	DEPOSITS	0 .00
WITHDRAWALS	0 .00	WITHDRAWALS	1 1,661.52	WITHDRAWALS	0 .00
ENDING BALANCE	25.10	ENDING BALANCE	651.62	DRAFTS CLEARED	0 .00
DIVIDEND YEAR-TO-DATE	.00	DIVIDEND YEAR-TO-DATE	.69	ENDING BALANCE	.00
DIVIDEND THIS PERIOD	.00	DIVIDEND THIS PERIOD	.13	DIVIDEND YEAR-TO-DATE	.00
AVERAGE DAILY BALANCE	25.10	AVERAGE DAILY BALANCE	1,026.67	DIVIDEND THIS PERIOD	.00
DAYS DIVIDEND EARNED	31	DAYS DIVIDEND EARNED	31		
		ANNUAL PERCENTAGE			
		YIELD EARNED	0.15%		

SUFFIX 001 ORG SHARES
 (NO ACTIVITY)

SUFFIX 006 FREEDOM

HISTORY			
DATE	DESCRIPTION	TRANSACTION AMOUNT	ACCOUNT BALANCE
3/08/19	Badges	1,661.52-	651.49
3/31/19	DIVIDEND	.13	651.62

SUFFIX 008 DRAFT
 (NO ACTIVITY)

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted By: Deborah Savage, Finance Manager

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: **Resolution No. 19-5639** Approving legal services agreement with Prentice, Long & Epperson P.C.

PRESENTED BY: Mike Wilson, City Administrator

SUMMARY: From time to time the City requires professional legal services to assist with Human Resource and Personnel matters. The Law Firm of Prentice, Long & Epperson, P.C. assist numerous Northern California cities and counties with their HR and Personnel needs such as contracts, CalPERS, etc. Staff is proposing that the City enter into an agreement for legal services with Prentice, Long & Epperson for a term of three years.

FISCAL IMPACT: Billing rate of \$165.00 per hour.

ACTION REQUESTED: Resolution No. 19-5639 Approving legal services agreement with Prentice, Long & Epperson P.C.

ATTACHMENTS: Resolution No. 19-5639
Agreement for Legal Services

RESOLUTION NO. 19-5639
A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SUSANVILLE
APPROVING AND AUTHORIZING THE MAYOR TO EXECUTE AN AGREEMENT
FOR LEGAL SERVICES WITH PRENTICE, LONG & EPPERSON, P.C.

WHEREAS, the City of Susanville requires the services of legal counsel from time to time:

WHEREAS, the City Council has determined that professional legal counsel can be provided by Prentice, Long & Epperson, P.C.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Susanville hereby approves and authorizes the Mayor to execute a three-year agreement for legal services with Prentice, Long & Epperson, P.C.

APPROVED: _____
Kevin Stafford, Mayor

ATTEST: _____
Gwenna MacDonald, City Clerk

The foregoing Resolution No. 19-5639 was adopted at a regular meeting of the City Council of the City of Susanville, held on the 17th day of April 2019 by the following vote:

AYES:
NOES:
ABSENT:
ABSTAINING:

Gwenna MacDonald, City Clerk

APPROVED AS TO FORM:

Jessica Ryan, City Attorney

**LEGAL SERVICES AGREEMENT BETWEEN THE CITY OF SUSANVILLE AND
PRENTICE, LONG & EPPERSON FOR LEGAL SERVICES**

THIS AGREEMENT for legal services is entered into by and between the City of Susanville (hereinafter referred to as "City") and PRENTICE, LONG & EPPERSON P.C. (hereinafter referred to as "Law Firm"), as of April, 2019.

SECTION 1.

SERVICES. Subject to the terms and conditions set forth in this Agreement, Law Firm shall provide to CITY the services described in the Scope of Work attached hereto and incorporated herein as Exhibit A/B at the time and place and in the manner specified therein. In the event of a conflict in or inconsistency between the terms of this Agreement and Exhibit A/B, the Agreement shall prevail.

1.1 Term of Services. The term of this Agreement shall begin on April, 2019 and end on April, 2022.

1.2 Standard of Performance. Law Firm shall perform all services required pursuant to this Agreement in the manner and according to the standards observed by a competent practitioner of the profession in which Law Firm is engaged in the geographical area in which Law Firm practices its profession. Law Firm shall prepare all work products required by this Agreement in a substantial, first-class manner and shall conform to the standards of quality normally observed by a person practicing in Law Firm's profession.

1.3 Assignment of Personnel. Law Firm shall assign only competent personnel to perform services pursuant to this Agreement. Margaret Long will be assigned as Counsel.

1.4 Time. Law Firm shall devote such time to the performance of services pursuant to this Agreement as may be reasonably necessary to meet the standard of performance provided in Section 1.2 above and to satisfy Law Firm's obligations hereunder.

1.5 Maximum. The maximum amount payable of this Agreement is \$ _____.

SECTION 2.

COMPENSATION. Notwithstanding any contrary indications that may be contained in Law Firm's proposal, CITY agrees to pay Law Firm in accordance with the Compensation Schedule provided in Exhibit B for services to be performed and reimbursable costs incurred under this Agreement. In the event of a conflict between this Agreement and Law Firm's proposal regarding the amount of compensation, the Agreement shall prevail. CITY shall pay Law Firm for services rendered pursuant to this Agreement at the time and in the manner set forth herein. The payments specified below shall be the only payments from CITY to Law Firm for services rendered pursuant to this Agreement. Law Firm shall submit all invoices to CITY in the manner specified herein.

2.1 Invoices. Law Firm shall submit invoices not more often than once a month during the term of this Agreement (unless requested otherwise by CITY), based on the cost for services performed and reimbursable costs incurred prior to the invoice date. Invoices shall contain the following information:

SECTION 5.

STATUS OF LAW FIRM.

5.1 Independent Contractor. At all times during the term of this Agreement, Law Firm shall be an independent contractor and shall not be an employee of CITY. CITY shall have the right to control Law Firm only insofar as the results of Law Firm's services rendered pursuant to this Agreement and assignment of personnel pursuant to Subparagraph 1.3; however, CITY shall otherwise not have the right to control the means by which Law Firm accomplishes services rendered pursuant to this Agreement. Notwithstanding any other CITY, state, or federal policy, rule, regulation, law, or ordinance to the contrary, Law Firm – and any of its employees, agents, and subcontractors providing services under this Agreement – shall not qualify for or become entitled to, and hereby agree to waive any and all claims to, any compensation, benefit, or any incident of employment by CITY, including but not limited to eligibility to enroll in the California Public Employees Retirement System (PERS) as an employee of CITY, and entitlement to any contribution to be paid by CITY for employer contributions and/or employee contributions for PERS benefits.

5.2 Law Firm Not Agent. Except as CITY may specify in writing or as provided by law, Law Firm shall have no authority, express or implied, to act on behalf of CITY in any capacity whatsoever as an agent. Law Firm shall have no authority, express or implied, pursuant to this Agreement to bind CITY to any obligation whatsoever.

SECTION 6.

LEGAL REQUIREMENTS.

6.1 Governing Law. The laws of the State of California shall govern this Agreement.

6.2 Compliance with Applicable Laws. Law Firm and any subcontractors shall comply with all laws applicable to the performance of the work hereunder.

6.3 Other Governmental Regulations. To the extent that this Agreement may be funded by fiscal assistance from another governmental entity, Law Firm and any subcontractors shall comply with all applicable rules and regulations to which CITY is bound by the terms of such fiscal assistance program.

6.4 Licenses and Permits. Law Firm represents and warrants to CITY that Law Firm and its employees, agents, and any subcontractors have all licenses, permits, qualifications, and approvals of whatsoever nature legally required to practice their respective professions. Law Firm represents and warrants to CITY that Law Firm and its employees, agents, any subcontractors shall, at their sole cost and expense, keep in effect at all times during the term of this Agreement any licenses, permits, and approvals legally required to practice their respective professions. In addition to the foregoing, Law Firm and any subcontractors shall obtain and maintain during the term of this Agreement any required business licenses from CITY.

6.5 Nondiscrimination and Equal Opportunity. Law Firm shall not discriminate, on the basis of a person's race, religion, color, national origin, age, physical or mental handicap or disability, medical condition, marital status, sex, or sexual orientation, against any employee, applicant for employment, subcontractor, bidder for a subcontract, or participant in, recipient of,

8.1 Records Created as Part of Law Firm's Performance. Law Firm hereby agrees to deliver copies of all reports, data, maps, models, charts, studies, surveys, photographs, memoranda, plans, studies, specifications, records, files, or any other documents or materials, in electronic or any other form, that Law Firm prepares or obtains pursuant to this Agreement to CITY upon termination of the Agreement. It is understood and agreed that the documents and other materials, including but not limited to those described above, prepared pursuant to this Agreement are prepared specifically for CITY and are not necessarily suitable for any future or other use. CITY and Law Firm agree that, until final approval by CITY, all data, plans, specifications, reports and other documents are confidential and will not be released to third parties without prior written consent of both parties.

8.2 Law Firm's Books and Records. Law Firm shall maintain any and all ledgers, books of account, invoices, vouchers, canceled checks, and other records or documents evidencing or relating to charges for services or expenditures and disbursements charged to CITY under this Agreement for a minimum of three (3) years, or for any longer period required by law, from the date of final payment to the Law Firm pursuant to this Agreement.

8.3 Inspection and Audit of Records. Any records or documents that Section 9.2 of this Agreement requires Law Firm to maintain shall be made available for inspection, audit, and/or copying at any time during regular business hours, upon oral or written request of CITY.

SECTION 9.

MISCELLANEOUS PROVISIONS.

9.1 Attorneys' Fees. If a party to this Agreement brings any action, including an action for declaratory relief, to enforce or interpret the provision of this Agreement, the prevailing party shall be entitled to reasonable attorneys' fees in addition to any other relief to which that party may be entitled. The court may set such fees in the same action or in a separate action brought for that purpose.

9.2 Venue. In the event that either party brings any action against the other under this Agreement, the parties agree that trial of such action shall be vested exclusively in the Superior Court for the County of Lassen.

9.3 Severability. If a court of competent jurisdiction finds or rules that any provision of this Agreement is invalid, void, or unenforceable, the provisions of this Agreement not so adjudged shall remain in full force and effect. The invalidity in whole or in part of any provision of this Agreement shall not void or affect the validity of any other provision of this Agreement.

9.4 No Implied Waiver of Breach. The waiver of any breach of a specific provision of this Agreement does not constitute a waiver of any other breach of that term or any other term of this Agreement.

9.5 Successors and Assigns. The provisions of this Agreement shall inure to the benefit of and shall apply to and bind the successors and assigns of the parties.

9.6 Conflict of Interest. Law Firm may serve other clients, but none whose activities within the corporate limits of CITY or whose business, regardless of location, would place Law Firm in a "conflict of interest," as that term is defined in the rules of professional

CITY

City of Susanville

LAW FIRM

Prentice Long & Epperson P.C.

By: _____

By _____
Margaret Long

EXHIBIT B

COMPENSATION SCHEDULE AND REIMBURSABLE EXPENSES

ATTORNEY SERVICES COMPENSATION SCHEDULE:

Law Firm is available to provide services, upon approval of the Board, at the following rate:

Attorney Rate: \$165/hour

Paralegal Rate: \$90/hour

REIMBURSABLE EXPENSES:

Reasonable travel expenses (mileage) for trips on behalf of City.	Applicable IRS rate per mile x number of miles
Duplication/reproduction fees	Actual cost if performed by outside service; no charge if in-house
Any other expense not listed above that becomes necessary for the successful resolution of a client matter	Actual Cost
2.0% administrative fee in lieu of separate charges for phone, fax and copies	Based on the amount of fees billed during the month

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: James M. Moore, Fire Chief

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: Resolution No. 19-5641 Wal-Mart Foundation Grant

PRESENTED BY: James M. Moore, Fire Chief

SUMMARY: The Fire Department has recently been contacted by our local Wal-Mart store manager with his intent to donate \$1000.00 to the Fire Department during their grand re-opening event on Friday, April 19, 2019. To become eligible for the donation, a grant application must be prepared and submitted to the Wal-Mart Foundation. Part of the grant package requires a letter of approval to receive grant monies signed by the Mayor. Upon the receipt of this document we can complete and submit the grant application.

FISCAL IMPACT: \$1,000 in revenue to the Fire Department Budget

ACTION REQUESTED: Motion to approve Resolution No. 19-5641 authorizing acceptance of grant from the WalMart Foundation

ATTACHMENTS: Resolution No. 19-5641

RESOLUTION NO. 19-5641
A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SUSANVILLE
APPROVING AND ACCEPTING A DONATION FROM WAL-MART FOUNDATION
THROUGH THE COMMUNITY GRANT PROGRAM

WHEREAS, the City of Susanville has been notified that Wal-Mart Foundation would like to make a donation of a \$1,000 through the Community Grant Program to the City of Susanville Fire Department; and

WHEREAS, the City of Susanville Fire Department has a need to purchase public education supplies, prevention supplies, and safety hand-out brochures; and

WHEREAS, the City being authorized to accept gifts and donations; and

WHEREAS, the City Council acknowledges that the prevention and education supplies, and safety hand-out brochures are valuable resources to the community; and

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Susanville accept the donation from the Community Grant Program through the Wal-Mart Foundation.

BE IT FURTHER RESOLVED that the Mayor is authorized to approve this donation on behalf of the City of Susanville.

APPROVED: _____
Kevin Stafford, Mayor

ATTEST: _____
Gwenna MacDonald, City Clerk

The foregoing **Resolution No. 19-5641** was adopted at a regular meeting of the City Council of the City of Susanville held on the 17th day of April, 2019, by the following vote:

AYES:
NOES:
ABSENT:
ABSTAINING.

Gwenna MacDonald, City Clerk

APPROVED AS TO FORM: _____
Jessica Ryan, City Attorney

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: Deborah Savage, Finance Manager

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: Vendor and Payroll Warrants

PRESENTED BY: Deborah Savage, Finance Manager

SUMMARY: Warrants dated March 23, 2019 through April 5, 2019 numbered 203616 through 203674.

FISCAL IMPACT: Accounts Payable vendor warrants totaling \$ 144,893.31 plus \$122,747.01 in payroll warrants, for a total of \$ 267,640.32.

ACTION

REQUESTED: Motion to receive and file.

ATTACHMENTS: Payments by vendor and transmittal check registers.

Report Criteria:
 Report type: GL detail
 Check Voided = False

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
03/19	03/28/2019	203624	7952		TR EX NAPA FTO TRAINING 033	03282019	1	1000-421-10-45	TRAINING	363.00	363.00
Total 03282019:										363.00	363.00
Grand Totals:										363.00	363.00

Report Criteria:

Report type: GL detail
 Check.Voided = False

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
03/19	03/29/2019	203625	728	U S POSTMASTER	UB BILLING GAS	032919	1	7401-430-62-46	POSTAGE	273.66	273.66
03/19	03/29/2019	203625	728	U S POSTMASTER	UB BILLING WATER	032919	2	7110-430-42-46	POSTAGE	531.22	531.22
Total 032919:										804.88	804.88
Grand Totals:										804.88	804.88

Report Criteria:
 Report type: GL detail
 Check.Voided = False

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
04/19	04/04/2019	203626	9598	INTERNAL REVENUE SE	EMPLOYEE CLASSIFICATION S	040419	1	7630-411-40-48	SETTLEMENTS (LEGAL)	2,056.00	2,056.00
Total 040419:										2,056.00	2,056.00
Grand Totals:										2,056.00	2,056.00

Report Criteria:

Report type: GL detail
 Check.Voided = False

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
04/19	04/05/2019	203627	1256	AMA GOLF CO	TEES- GC	146184	1	7530-451-55-46	SUPPLIES - GENERAL	214.98	214.98
Total 146184:										214.98	214.98
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	CUSTODIAL SUPPLIES-PW	636648114	1	7620-430-10-44	LINEN SERVICE	29.53	29.53
Total 636648114:										29.53	29.53
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	UNIFORM SERVICE-GAS	636648115	1	7401-430-62-44	LINEN SERVICES	49.44	49.44
Total 636648115:										49.44	49.44
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	UNIFORM SERVICE-STREETS	636648116	1	2007-431-20-44	LINEN SERVICE	50.44	50.44
Total 636648116:										50.44	50.44
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	UNIFORM SERVICE-WATER	636648117	1	7110-430-42-44	LINEN SERVICE	46.53	46.53
Total 636648117:										46.53	46.53
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	CUSTODIAL SUPPLIES-PW	636661891	1	7620-430-10-44	LINEN SERVICE	30.06	30.06
Total 636661891:										30.06	30.06
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	UNIFORM SERVICE-STREETS	636661892	1	2007-431-20-44	LINEN SERVICE	124.72	124.72
Total 636661892:										124.72	124.72
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	UNIFORM SERVICE-GAS	636661893	1	7401-430-62-44	LINEN SERVICES	51.77	51.77
Total 636661893:										51.77	51.77
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	UNIFORM SERVICE-WATER	636661894	1	7110-430-42-44	LINEN SERVICE	52.14	52.14

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 636661894:										52.14	52.14
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	CUSTODIAL SUPPLIES-PW	636675819	1	7620-430-10-44	LINEN SERVICE	33.53	33.53
Total 636675819:										33.53	33.53
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	UNIFORM SERVICE-STREETS	636675820	1	2007-431-20-44	LINEN SERVICE	189.98	189.98
Total 636675820:										189.98	189.98
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	UNIFORM SERVICE-GAS	636675821	1	7401-430-62-44	LINEN SERVICES	49.54	49.54
Total 636675821:										49.54	49.54
04/19	04/05/2019	203628	44	ARAMARK UNIFORM SE	UNIFORM SERVICE-WATER	636675822	1	7110-430-42-44	LINEN SERVICE	41.92	41.92
Total 636675822:										41.92	41.92
04/19	04/05/2019	203629	1231	ASBURY ENVIRONMENT	REPAIR & MAINT- STREETS	1500-00421208	1	2007-431-20-44	REPAIR AND MAINTENANCE-V	25.00	25.00
04/19	04/05/2019	203629	1231	ASBURY ENVIRONMENT	REPAIR & MAINT- WATER	1500-00421208	2	7110-430-42-44	REPAIR AND MAINTENANCE-V	25.00	25.00
04/19	04/05/2019	203629	1231	ASBURY ENVIRONMENT	REPAIR & MAINT- GAS	1500-00421208	3	7401-430-62-44	REPAIR AND MAINT-VEHICLE	25.00	25.00
Total 1500-00421208:										75.00	75.00
04/19	04/05/2019	203630	927	BAXTER AUTO PARTS IN	SUPPLIES-GAS	32-0201016	1	7401-430-62-44	REPAIR AND MAINT-VEHICLE	207.55	207.55
Total 32-0201016:										207.55	207.55
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	CREDIT-FD	386070	1	1000-422-10-44	FACILITY - REPAIR & MAINTEN	15.43-	15.43-
Total 386070:										15.43-	15.43-
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES- GC	435500	1	7530-451-52-44	REPAIR & MAINTENANCE - MIS	3.17	3.17
Total 435500:										3.17	3.17
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES- GC	435842	1	7530-451-52-44	REPAIR & MAINTENANCE - MIS	34.74	34.74

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 435842:										34.74	34.74
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES- WATER	435875	1	7110-430-42-46	SUPPLIES-GENERAL	61.65	61.65
Total 435875:										61.65	61.65
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES- WATER	436188	1	7110-430-42-46	SUPPLIES-GENERAL	2.08	2.08
Total 436188:										2.08	2.08
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES-PARKS	436243	1	1000-452-20-46	SUPPLIES-GENERAL	.66	.66
Total 436243:										.66	.66
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES-PARKS	436259	1	1000-452-20-46	SUPPLIES-GENERAL	21.36	21.36
Total 436259:										21.36	21.36
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES-PARKS	436298	1	1000-452-20-46	SUPPLIES-GENERAL	3.86	3.86
Total 436298:										3.86	3.86
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES- PARKS	436341	1	1000-452-20-46	SUPPLIES-GENERAL	13.50	13.50
Total 436341:										13.50	13.50
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES-PARKS	436358	1	1000-452-20-46	SUPPLIES-GENERAL	.66	.66
Total 436358:										.66	.66
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES-PARKS	436401	1	1000-452-20-46	SUPPLIES-GENERAL	9.64	9.64
Total 436401:										9.64	9.64
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES- PARKS	436406	1	1000-452-20-46	SUPPLIES-GENERAL	11.75	11.75
Total 436406:										11.75	11.75

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES- PARKS	436442	1	1000-452-20-46	SUPPLIES-GENERAL	23.16	23.16
Total 436442:										23.16	23.16
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES-STREETS	436667	1	2007-431-20-46	SUPPLIES-GENERAL	3.26	3.26
Total 436667:										3.26	3.26
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES-GAS	436693	1	7401-430-62-46	SUPPLIES-GENERAL	16.76	16.76
Total 436693:										16.76	16.76
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES- GAS	436805	1	7401-430-62-46	SUPPLIES-GENERAL	16.08	16.08
Total 436805:										16.08	16.08
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES-WATER	437277	1	7110-430-42-46	SUPPLIES-SMALL TOOLS	61.13	61.13
Total 437277:										61.13	61.13
04/19	04/05/2019	203631	76	BILLINGTON ACE HARD	SUPPLIES-PW	437278	1	7620-430-10-46	SUPPLIES-GENERAL	15.52	15.52
Total 437278:										15.52	15.52
04/19	04/05/2019	203632	815	CALIFORNIA RURAL WAT	MEMBESHIP RENEWAL	040319	1	7110-430-42-48	DUES AND MEMBERSHIPS	1,302.00	1,302.00
Total 040319:										1,302.00	1,302.00
04/19	04/05/2019	203633	116	CASHMAN EQUIPMENT	SUPPLIES-STREETS	INPS2920692	1	2007-431-20-44	REPAIR AND MAINTENANCE-V	707.88	707.88
Total INPS2920692:										707.88	707.88
04/19	04/05/2019	203634	5608		REFUND COMMUNITY CENTER	032619	1	1000-2228-009	DEPOSITS-COMM CENTER RE	50.00	50.00
Total 032619:										50.00	50.00
04/19	04/05/2019	203635	139	CITY CLERKS ASSOC OF	2019 DUES	032219	1	1000-411-40-48	DUES AND MEMBERSHIPS	90.00	90.00

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 032219:										90.00	90.00
04/19	04/05/2019	203636	148	COMPUTER LOGISTICS	TECHNICAL SERVICES-PD	72227	1	1000-421-10-43	PROFESSIONAL SVCS	2,297.74	2,297.74
Total 72227:										2,297.74	2,297.74
04/19	04/05/2019	203636	148	COMPUTER LOGISTICS	19-5613	72247	1	7620-430-10-43	TECHNICAL SVCS	9,067.28	9,067.28
Total 72247:										9,067.28	9,067.28
04/19	04/05/2019	203636	148	COMPUTER LOGISTICS	19-5613	72249	1	7620-430-10-43	TECHNICAL SVCS	5,494.48	5,494.48
Total 72249:										5,494.48	5,494.48
04/19	04/05/2019	203636	148	COMPUTER LOGISTICS	MONTHLY SERVICES MAINTEN	80311	1	1000-417-10-43	TECHNICAL SVCS	220.00	220.00
Total 80311:										220.00	220.00
04/19	04/05/2019	203636	148	COMPUTER LOGISTICS	ANTI-VIRUS BARRACUDA 400	80324	1	1000-417-10-43	TECHNICAL SVCS	100.00	100.00
Total 80324:										100.00	100.00
04/19	04/05/2019	203637	152	COUSO TECHNOLOGY &	WEBSITE MAINT 3/19-4/19	1565	1	1000-417-10-43	TECHNICAL SVCS	790.00	790.00
Total 1565:										790.00	790.00
04/19	04/05/2019	203638	194	DIAMOND SAW SHOP IN	SUPPLIES -PARKS	16860	1	1000-452-20-46	SUPPLIES-GENERAL	66.66	66.66
Total 16860:										66.66	66.66
04/19	04/05/2019	203638	194	DIAMOND SAW SHOP IN	SUPPLIES -PARKS	16861	1	1000-452-20-44	MISC - REPAIR & MAINTENANC	13.89	13.89
Total 16861:										13.89	13.89
04/19	04/05/2019	203639	1260	DIRECTV INC	CABLE- GC	36048073673	1	7530-451-52-45	COMMUNICATIONS	392.03	392.03
Total 36048073673:										392.03	392.03

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
04/19	04/05/2019	203640	208		REIM HEALTH INSURANCE	040119	1	7610-2239-006	RETIREE SICK LEAVE BANK PA	416.88	416.88
Total 040119:										416.88	416.88
04/19	04/05/2019	203641	219	ED STAUB & SONS PETR	262.69 GAL PROPANE- AIRPOR	04000192	1	7201-430-81-46	PROPANE	451.32	451.32
Total 04000192:										451.32	451.32
04/19	04/05/2019	203641	219	ED STAUB & SONS PETR	SUPPLIES- WATER	1480875	1	7301-430-52-46	SUPPLIES-GENERAL	246.25	246.25
Total 1480875:										246.25	246.25
04/19	04/05/2019	203642	241	FEATHER PUBLISHING C	EMPLOYMENT AD PARK MAINT	1525992	1	1000-416-10-45	ADVERTISING	79.00	79.00
Total 1525992:										79.00	79.00
04/19	04/05/2019	203642	241	FEATHER PUBLISHING C	EMPLOYMENT AD PARK MAINT	1526716	1	1000-416-10-45	ADVERTISING	79.00	79.00
Total 1526716:										79.00	79.00
04/19	04/05/2019	203642	241	FEATHER PUBLISHING C	EMPLOYMENT AD PARK MAINT	1527439	1	1000-416-10-45	ADVERTISING	79.00	79.00
Total 1527439:										79.00	79.00
04/19	04/05/2019	203643	1033	FGL ENVIRONMENTAL	WEEKLY WATER SAMPLING-D	971481A	1	7110-430-42-43	TECHNICAL SVCS	147.00	147.00
Total 971481A:										147.00	147.00
04/19	04/05/2019	203643	1033	FGL ENVIRONMENTAL	WEEKLY WATER SAMPLING-D	971820A	1	7110-430-42-43	TECHNICAL SVCS	95.00	95.00
Total 971820A:										95.00	95.00
04/19	04/05/2019	203644	265	FRONTIER	257-0315 AWOS AIRPORT	0315 031519	1	7201-430-81-45	COMMUNICATIONS	48.84	48.84
Total 0315 031519:										48.84	48.84
04/19	04/05/2019	203644	265	FRONTIER	257-1057-FAX PW	1057 032019	1	7620-430-10-45	COMMUNICATIONS	174.26	174.26

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 1057 032019:										174.26	174.26
04/19	04/05/2019	203644	265	FRONTIER	257-1182 NAT GAS TELEMETRY	1182 031019	1	7401-430-62-45	COMMUNICATIONS	41.31	41.31
Total 1182 031019:										41.31	41.31
04/19	04/05/2019	203644	265	FRONTIER	252-1182 SCADA - WATER	2-1182 031019	1	7110-430-42-45	COMMUNICATIONS	304.84	304.84
Total 2-1182 031019:										304.84	304.84
04/19	04/05/2019	203644	265	FRONTIER	257-2845 U/B ROLL OVER	2845 031519	1	7620-430-10-45	COMMUNICATIONS	70.11	70.11
Total 2845 031519:										70.11	70.11
04/19	04/05/2019	203644	265	FRONTIER	257-4725-FAX	4725 031519	1	1000-419-10-45	COMMUNICATIONS	60.52	60.52
04/19	04/05/2019	203644	265	FRONTIER	257-4725 CITY HALL FAX	4725 031519	2	1000-417-10-45	COMMUNICATIONS	60.52	60.52
Total 4725 031519:										121.04	121.04
04/19	04/05/2019	203645	8569		REIMBURSEMENT TRAVEL EXP	032719	1	7620-430-10-46	GASOLINE	59.81	59.81
Total 032719:										59.81	59.81
04/19	04/05/2019	203646	288		REIMBURSE HEALTH INS	040219	1	7610-2239-006	RETIREE SICK LEAVE BANK PA	86.61	86.61
Total 040219:										86.61	86.61
04/19	04/05/2019	203647	335	J.W. WOOD CO INC	SUPPLIES-WATER	S109824	1	7110-430-42-46	SUPPLIES-GENERAL	34.36	34.36
Total S109824:										34.36	34.36
04/19	04/05/2019	203648	338	JACKSON'S SERVICE CE	FUEL-WATER	040119	1	7110-430-42-46	GASOLINE	887.59	887.59
04/19	04/05/2019	203648	338	JACKSON'S SERVICE CE	FUEL- GAS	040119	2	7401-430-62-46	GASOLINE	317.65	317.65
04/19	04/05/2019	203648	338	JACKSON'S SERVICE CE	FUEL- STREETS	040119	3	2007-431-20-46	GASOLINE	496.68	496.68
04/19	04/05/2019	203648	338	JACKSON'S SERVICE CE	FUEL- PW	040119	4	7620-430-10-46	GASOLINE	81.68	81.68
Total 040119:										1,783.60	1,783.60

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
04/19	04/05/2019	203649	911	JOHNSTONE SUPPLY	SUPPLIES-GAS	415-S2366421.001	1	7401-430-62-46	SUPPLIES-GENERAL	151.43	151.43
Total 415-S2366421.001:										151.43	151.43
04/19	04/05/2019	203650	1350	JONES & MAYER	PROFESSIONAL SERVICES	91052	1	1000-412-10-43	PROFESSIONAL SVCS	510.00	510.00
Total 91052:										510.00	510.00
04/19	04/05/2019	203651	362	KAUFFMAN, BILL	CUSTODIAL SVCS-PW	753445	1	7620-430-10-44	CUSTODIAL	250.00	250.00
Total 753445:										250.00	250.00
04/19	04/05/2019	203651	362	KAUFFMAN, BILL	CUSTODIAL SVCS	753446	1	1000-417-10-44	CUSTODIAL	650.00	650.00
Total 753446:										650.00	650.00
04/19	04/05/2019	203652	1346	KENNETH KORVER M.D	PRE-EMPLOYMENT SCREEN	030819	1	1000-416-10-43	PROFESSIONAL SVCS	320.00	320.00
Total 030819:										320.00	320.00
04/19	04/05/2019	203853	1335	LASSEN COUNTY OFFIC	TECHNICAL SERVICES	18/19-262	1	1000-421-10-43	TECHNICAL SVCS	1,312.50	1,312.50
Total 18/19-262:										1,312.50	1,312.50
04/19	04/05/2019	203654	411	LASSEN MOTOR PARTS	SUPPLIES-PARKS	318609	1	1000-452-20-44	VEHICLE - REPAIR & MAINTEN	18.32	18.32
Total 318609:										18.32	18.32
04/19	04/05/2019	203654	411	LASSEN MOTOR PARTS	SUPPLIES-PARKS	318926	1	1000-452-20-44	MISC - REPAIR & MAINTENANC	62.10	62.10
Total 318926:										62.10	62.10
04/19	04/05/2019	203654	411	LASSEN MOTOR PARTS	SUPPLIES-STREETS	319471	1	2007-431-20-44	REPAIR AND MAINTENANCE-V	47.75	47.75
Total 319471:										47.75	47.75
04/19	04/05/2019	203654	411	LASSEN MOTOR PARTS	SUPPLIES- STREETS	319611	1	2007-431-20-44	REPAIR AND MAINTENANCE-V	18.91	18.91

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 319611:										18.91	18.91
04/19	04/05/2019	203654	411	LASSEN MOTOR PARTS	SUPPLIES-STREETS	319658	1	2007-431-20-46	SUPPLIES-SMALL TOOLS	160.25	160.25
Total 319658:										160.25	160.25
04/19	04/05/2019	203655	9595		REFUND WATER DEPOSIT	10314000011	1	7110-2228-000	DEPOSITS-CUSTOMER	52.22	52.22
Total 10314000011:										52.22	52.22
04/19	04/05/2019	203656	437	LMUD	AIRPORT VASI LIGHTS	10108 032219	1	7201-430-81-46	ELECTRICITY	97.11	97.11
Total 10108 032219:										97.11	97.11
04/19	04/05/2019	203656	437	LMUD	JOHNSTONVILLE RD SPRINKLE	10262 031819	1	1000-452-20-46	ELECTRICITY	34.88	34.88
Total 10262 031819:										34.88	34.88
04/19	04/05/2019	203656	437	LMUD	GOLF COURSE IRR WELL30 HP	122907 032219	1	7530-451-52-46	ELECTRICITY	34.34	34.34
Total 122907 032219:										34.34	34.34
04/19	04/05/2019	203656	437	LMUD	GOLF COURSE PUMP STATION	122910 032219	1	7530-451-52-46	ELECTRICITY	20.00	20.00
Total 122910 032219:										20.00	20.00
04/19	04/05/2019	203656	437	LMUD	GOLF COURSE IRR PUMP/8TH	122929 032219	1	7530-451-52-46	ELECTRICITY	20.00	20.00
Total 122929 032219:										20.00	20.00
04/19	04/05/2019	203656	437	LMUD	GOLF COURSE PUMP HOUSE	132052 032219	1	7530-451-52-46	ELECTRICITY	278.22	278.22
Total 132052 032219:										278.22	278.22
04/19	04/05/2019	203656	437	LMUD	470-895 CIRCLE DR-CLUB HOU	144281 032219	1	7530-451-52-46	ELECTRICITY	161.28	161.28
Total 144281 032219:										161.28	161.28

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
04/19	04/05/2019	203656	437	LMUD	SOUTH ST - PW OFFICE	14590 032819	1	7620-430-10-46	ELECTRICITY	506.22	506.22
Total 14590 032819:										506.22	506.22
04/19	04/05/2019	203656	437	LMUD	SOUTH ST ROOSEVELT AREA	1744 032819	1	1000-452-20-46	ELECTRICITY	8.64	8.64
Total 1744 032819:										8.64	8.64
04/19	04/05/2019	203656	437	LMUD	RIVERSIDE PARK LIGHTS	1999 032819	1	1000-452-20-46	ELECTRICITY	33.15	33.15
Total 1999 032819:										33.15	33.15
04/19	04/05/2019	203656	437	LMUD	CADY SPRINGS	26784 032819	1	7110-430-42-46	ELECTRICITY	41.64	41.64
Total 26784 032819:										41.64	41.64
04/19	04/05/2019	203656	437	LMUD	SKYLINE DR WELL 4-WATER	29931 031219	1	7110-430-42-46	ELECTRICITY	32.78	32.78
Total 29931 031219:										32.78	32.78
04/19	04/05/2019	203656	437	LMUD	472-105 JOHNSTONVILLE WAT	350161 031819	1	7112-430-42-46	ELECTRICITY	182.17	182.17
Total 350161 031819:										182.17	182.17
04/19	04/05/2019	203656	437	LMUD	RICHMOND RD BRIDGE	35094 032819	1	2007-431-60-46	ELECTRICITY	249.35	249.35
Total 35094 032819:										249.35	249.35
04/19	04/05/2019	203656	437	LMUD	LITTLE LEAGUE PARK AREA LI	3522 032219	1	1000-452-20-46	ELECTRICITY	57.37	57.37
Total 3522 032219:										57.37	57.37
04/19	04/05/2019	203656	437	LMUD	N WEATHERLOW ST SIGNALS-	3651 032219	1	2007-431-60-46	ELECTRICITY	123.68	123.68
Total 3651 032219:										123.68	123.68
04/19	04/05/2019	203656	437	LMUD	720 SOUTH EMULSION TANK-P	38646 032819	1	7620-430-10-46	ELECTRICITY	113.79	113.79

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 38646 032819:										113.79	113.79
04/19	04/05/2019	203656	437	LMUD	105 S ASH STREET	412864 032219	1	1000-452-20-46	ELECTRICITY	20.00	20.00
Total 412864 032219:										20.00	20.00
04/19	04/05/2019	203656	437	LMUD	SAN FRANCISCO ST- STREETS	416835 031819	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 416835 031819:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	FIRST STREET & ALLEY STREE	416848 031819	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 416848 031819:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	LITTLE LEAGUE PARK DRVEW	416851 032219	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 416851 032219:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	LAUREL SR MID POINT OF LAU	416902 032219	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 416902 032219:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	SOUTH ST & WEST END- STRE	416924 032819	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 416924 032819:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	CAMPBELL ST- STREETS	416940 031219	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 416940 031219:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	WASHO LN -STREET LIGHTS	416959 031219	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 416959 031219:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	RICHMOND RD & PEARL CR- S	416984 032819	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 416984 032819:										15.01	15.01

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
04/19	04/05/2019	203656	437	LMUD	1801 MAIN ST STREET WEST SI	417512 032219	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 417512 032219:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	ORCHARD STREET LIGHTS	418802 032819	1	2007-431-60-46	ELECTRICITY	9.16	9.16
Total 418802 032819:										9.16	9.16
04/19	04/05/2019	203656	437	LMUD	RIVERSIDE DR. & RIVER ST. LI	418824 032819	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 418824 032819:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	RIVERSIDE DR. & LAUREL STR	418833 032819	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 418833 032819:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	MARTHA & ARNOLD STREET LI	421476 031219	1	2007-431-60-46	ELECTRICITY	15.01	15.01
Total 421476 031219:										15.01	15.01
04/19	04/05/2019	203656	437	LMUD	GLENN & CHERRY TR - SCADA-	44298 031219	1	7110-430-42-46	ELECTRICITY	26.76	26.76
Total 44298 031219:										26.76	26.76
04/19	04/05/2019	203656	437	LMUD	PAIUTE LN SCADA-WATER	44316 031219	1	7110-430-42-46	ELECTRICITY	24.96	24.96
Total 44316 031219:										24.96	24.96
04/19	04/05/2019	203656	437	LMUD	BAGWELL SPRINGS - SCADA-W	45542 031219	1	7110-430-42-46	ELECTRICITY	51.41	51.41
Total 45542 031219:										51.41	51.41
04/19	04/05/2019	203656	437	LMUD	WELL #3-WATER	4559 031819	1	7110-430-42-46	ELECTRICITY	62.41	62.41
Total 4559 031819:										62.41	62.41
04/19	04/05/2019	203656	437	LMUD	MAIN & ALEXANDER SIGNAL-S	49496 032219	1	2007-431-60-46	ELECTRICITY	144.12	144.12

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 49496 032219:										144.12	144.12
04/19	04/05/2019	203656	437	LMUD	MAIN & FAIRFIELD-STREETS	49497 032219	1	2007-431-60-46	ELECTRICITY	112.82	112.82
Total 49497 032219:										112.82	112.82
04/19	04/05/2019	203656	437	LMUD	MAIN & JOHNSTNVLE SIGNAL-	49498 032219	1	2007-431-60-46	ELECTRICITY	142.24	142.24
Total 49498 032219:										142.24	142.24
04/19	04/05/2019	203656	437	LMUD	RIVERSIDE & MAIN LIGHTS-ST	49499 032219	1	2007-431-60-46	ELECTRICITY	228.30	228.30
Total 49499 032219:										228.30	228.30
04/19	04/05/2019	203656	437	LMUD	AIRPORT HANGER 6	54333 032219	1	7201-430-81-46	ELECTRICITY	23.76	23.76
Total 54333 032219:										23.76	23.76
04/19	04/05/2019	203656	437	LMUD	SPRING RIDGE BOOSTER-WAT	55754 032819	1	7110-430-42-46	ELECTRICITY	176.91	176.91
Total 55754 032819:										176.91	176.91
04/19	04/05/2019	203656	437	LMUD	925 SIERRA RD SPORTS CTR	60453 032219	1	1000-452-20-46	ELECTRICITY	22.10	22.10
Total 60453 032219:										22.10	22.10
04/19	04/05/2019	203656	437	LMUD	AIRPORT OFFICE	7146 032219	1	7201-430-81-46	ELECTRICITY	562.88	562.88
Total 7146 032219:										562.88	562.88
04/19	04/05/2019	203656	437	LMUD	AIRPORT GAS PUMP	7154 032219	1	7201-430-81-46	ELECTRICITY	28.79	28.79
Total 7154 032219:										28.79	28.79
04/19	04/05/2019	203656	437	LMUD	GOLF COURSE CLUB HOUSE	7394 032219	1	7530-451-52-46	ELECTRICITY	65.62	65.62
Total 7394 032219:										65.62	65.62

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
04/19	04/05/2019	203656	437	LMUD	GOLF COURSE CART BARN 2	7400 032219	1	7530-451-52-46	ELECTRICITY	23.21	23.21
Total 7400 032219:										23.21	23.21
04/19	04/05/2019	203656	437	LMUD	WELL #1-WATER	7714 032219	1	7110-430-42-46	ELECTRICITY	147.60	147.60
Total 7714 032219:										147.60	147.60
04/19	04/05/2019	203656	437	LMUD	1801 MAIN ST	8314 032219	1	1000-421-10-46	ELECTRICITY	867.69	867.69
Total 8314 032219:										867.69	867.69
04/19	04/05/2019	203656	437	LMUD	GOLF COURSE BARN 1 & 3	9312 032219	1	7530-451-52-46	ELECTRICITY	20.15	20.15
Total 9312 032219:										20.15	20.15
04/19	04/05/2019	203656	437	LMUD	RIVERSIDE PARK LIGHTS	9501 032819	1	1000-452-20-46	ELECTRICITY	93.05	93.05
Total 9501 032819:										93.05	93.05
04/19	04/05/2019	203656	437	LMUD	GEO PUMP #2	9503 032619	1	7301-430-52-46	ELECTRICITY	36.08	36.08
Total 9503 032619:										36.08	36.08
04/19	04/05/2019	203656	437	LMUD	HOSPITAL LN-GEO	9963 032819	1	7301-430-52-46	ELECTRICITY	20.00	20.00
Total 9963 032819:										20.00	20.00
04/19	04/05/2019	203657	9593		REFUND GAS DEPOSIT	10417600018	1	7401-2228-000	DEPOSITS-CUSTOMER	41.63	41.63
Total 10417600018:										41.63	41.63
04/19	04/05/2019	203658	1508	MAIN STREET LUBE	OIL & FILTER #80-PD	18689	1	1000-421-10-44	VEHICLE - REPAIR & MAINTEN	52.21	52.21
Total 18689:										52.21	52.21
04/19	04/05/2019	203659	452	MARTIN SECURITY SYST	470-895 CIRCLE DR PROSHOP	033028	1	7530-451-50-43	TECHNICAL SVCS	40.00	40.00

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 033028:										40.00	40.00
04/19	04/05/2019	203659	452	MARTIN SECURITY SYST	60 N LASSEN SECURITY	33196	1	1000-417-10-43	TECHNICAL SVCS	48.00	48.00
04/19	04/05/2019	203659	452	MARTIN SECURITY SYST	75 WEATHERLOW SECURITY	33196	2	1000-452-20-43	TECHNICAL SVCS	40.00	40.00
Total 33196:										88.00	88.00
04/19	04/05/2019	203660	467	METER VALVE & CONTR	SUPPLIES- GAS	15356	1	7401-430-62-46	SUPPLIES-GENERAL	1,703.02	1,703.02
Total 15356:										1,703.02	1,703.02
04/19	04/05/2019	203660	467	METER VALVE & CONTR	SUPPLIES- GAS	15391	1	7401-430-62-46	SUPPLIES-GENERAL	80.96	80.96
Total 15391:										80.96	80.96
04/19	04/05/2019	203661	481	MISSION LINEN & UNIFO	SUPPLIES	509574178	1	1000-417-10-46	SUPPLIES-GENERAL	181.68	181.68
Total 509574178:										181.68	181.68
04/19	04/05/2019	203662	9493	NATIONAL AUTO PARTS	SUPPLIES- STREETS	52-00103731	1	2007-431-20-44	REPAIR AND MAINTENANCE-V	279.60	279.60
Total 52-00103731:										279.60	279.60
04/19	04/05/2019	203662	9493	NATIONAL AUTO PARTS	SUPPLIES- STREETS	52-00103846	1	2007-431-20-44	REPAIR AND MAINTENANCE-V	267.63	267.63
Total 52-00103846:										267.63	267.63
04/19	04/05/2019	203662	9493	NATIONAL AUTO PARTS	SUPPLIES-GAS	52-103516	1	7401-430-62-44	REPAIR AND MAINT-VEHICLE	104.03	104.03
Total 52-103516:										104.03	104.03
04/19	04/05/2019	203662	9493	NATIONAL AUTO PARTS	SUPPLIES-STREETS	52-103538	1	2007-431-20-44	REPAIR AND MAINTENANCE-MI	75.18	75.18
Total 52-103538:										75.18	75.18
04/19	04/05/2019	203662	9493	NATIONAL AUTO PARTS	SUPPLIES-GAS	52-103544	1	7401-430-62-44	REPAIR AND MAINT-VEHICLE	3.91	3.91

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 52-103544:										3.91	3.91
04/19	04/05/2019	203662	9493	NATIONAL AUTO PARTS	SUPPLIES- STREETS	52-13539	1	2007-431-20-44	REPAIR AND MAINTENANCE-V	15.63	15.63
04/19	04/05/2019	203662	9493	NATIONAL AUTO PARTS	SUPPLIES- WATER	52-13539	2	7110-430-42-44	REPAIR AND MAINTENANCE-V	15.62	15.62
04/19	04/05/2019	203662	9493	NATIONAL AUTO PARTS	SUPPLIES- GAS	52-13539	3	7401-430-62-44	REPAIR AND MAINT-VEHICLE	15.63	15.63
Total 52-13539:										46.88	46.88
04/19	04/05/2019	203663	9594		REFUND GAS DEPOSIT	10311050229	1	7401-2228-000	DEPOSITS-CUSTOMER	38.34	38.34
Total 10311050229:										38.34	38.34
04/19	04/05/2019	203664	546	PAYLESS BUILDING SUP	SUPPLIES-PARKS	2504001	1	1000-452-20-44	FACILITY - REPAIR & MAINTEN	23.39	23.39
Total 2504001:										23.39	23.39
04/19	04/05/2019	203665	558	PLUMAS-SIERRA	INTERNET ACCESS CIRCUIT 4/	58020 040119	1	1000-417-10-45	COMMUNICATIONS	840.00	840.00
Total 58020 040119:										840.00	840.00
04/19	04/05/2019	203666	1296	RENTAL GUYS	EQUIPMENT RENTAL-PARKS	700355-5	1	1000-452-20-44	RENT & LEASES EQUIP & VEHI	1,237.78	1,237.78
Total 700355-5:										1,237.78	1,237.78
04/19	04/05/2019	203666	1296	RENTAL GUYS	SUPPLIES- WATER	701849-5	1	7110-430-42-46	SUPPLIES-GENERAL	29.96	29.96
Total 701849-5:										29.96	29.96
04/19	04/05/2019	203667	628	SCORE	WORKERS COMP 7/18-6/19	Q4 WC SUSAN	1	7630-411-40-42	WORKERS' COMPENSATION	99,317.17	99,317.17
Total Q4 WC SUSAN:										99,317.17	99,317.17
04/19	04/05/2019	203668	9596		REFUND GAS DEPOSIT	101420650238	1	7401-2228-000	DEPOSITS-CUSTOMER	145.92	145.92
Total 101420650238:										145.92	145.92
04/19	04/05/2019	203669	1076	SIERRA COFFEE AND BE	BOTTLED WATER	51240	1	1000-417-10-46	SUPPLIES-GENERAL	21.75	21.75

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 51240:										21.75	21.75
04/19	04/05/2019	203669	1076	SIERRA COFFEE AND BE	BOTTLED WATER-PW	51321	1	7620-430-10-46	SUPPLIES-GENERAL	21.75	21.75
Total 51321:										21.75	21.75
04/19	04/05/2019	203670	640	SIERRA ELECTRONICS	REPAIRS RADIO-PD	AR15643	1	1000-421-10-44	RADIO - REPAIR & MAINTENAN	662.99	662.99
Total AR15643:										662.99	662.99
04/19	04/05/2019	203671	872	SLAKEY BROTHERS INC.	SUPPLIES-GAS	11277160-00	1	7401-430-62-46	SUPPLIES-GENERAL	222.50	222.50
Total 11277160-00:										222.50	222.50
04/19	04/05/2019	203671	872	SLAKEY BROTHERS INC.	SUPPLIES-GAS	11278327-00	1	7401-430-62-46	SUPPLIES-GENERAL	258.24	258.24
Total 11278327-00:										258.24	258.24
04/19	04/05/2019	203672	1382	SONSRAY MACHINERY L	REPAIR & MAINT- STREETS	P17457-07	1	2007-431-20-44	REPAIR AND MAINTENANCE-V	127.26	127.26
04/19	04/05/2019	203672	1382	SONSRAY MACHINERY L	REPAIR & MAINT- WATER	P17457-07	2	7110-430-42-44	REPAIR AND MAINTENANCE-V	127.26	127.26
04/19	04/05/2019	203672	1382	SONSRAY MACHINERY L	REPAIR & MANIT-GAS	P17457-07	3	7401-430-62-44	REPAIR AND MAINT-VEHICLE	127.26	127.26
Total P17457-07:										381.78	381.78
04/19	04/05/2019	203673	712	TNS TRUCKING CO	SUPPLIES-WATER	3601	1	7110-430-42-46	SUPPLIES-GENERAL	308.88	308.88
Total 3601:										308.88	308.88
04/19	04/05/2019	203674	770	WESTERN NEVADA SUP	SUPPLIES- GAS	67780954	1	7401-430-62-46	SUPPLIES-GENERAL	40.12	40.12
Total 67780954:										40.12	40.12
04/19	04/05/2019	203674	770	WESTERN NEVADA SUP	SUPPLIES- WATER	67783472	1	7110-430-42-46	SUPPLIES-GENERAL	35.39	35.39
Total 67783472:										35.39	35.39
04/19	04/05/2019	203674	770	WESTERN NEVADA SUP	SUPPLIES- WATER	67797312	1	7110-430-42-46	SUPPLIES-GENERAL	39.63	39.63

GL Period	Check Issue Date	Check Number	Vendor Number	Payee	Description	Invoice Number	Inv Seq	GL Account No	GL Account Title	Seq Amount	Check Amount
Total 67797312:										39.63	39.63
Grand Totals:										141,669.43	141,669.43

Report Criteria:

Report type: GL detail
Check.Voided = False

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: Gwenna MacDonald, City Clerk

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: **Ordinance No. 19-1014** amending Chapter 8.28 Weed and Rubbish Abatement, Section 8.28.040 Abatement Procedure, of the Susanville Municipal Code

PRESENTED BY: James M. Moore, Fire Chief

SUMMARY: The City Council voted to introduce Ordinance No. 19-1014 at its April 3, 2019 meeting. The modification of Susanville Municipal Code Section 8.28 (Weed and Rubbish Abatement) will reflect current practices and bring consistency to the enforcement process for weed and rubbish abatements. The ordinance will take effect 31 days after its adoption.

FISCAL IMPACT: None at this time

ACTION REQUESTED: Motion to waive second reading and adopt Ordinance No. 19-1014 amending Chapter 8.28 Weed and Rubbish Abatement Section 8.28.040 Abatement Procedure, of the Susanville Municipal Code

ATTACHMENTS: Ordinance No. 19-1014
Susanville Municipal Code Section 8.28.040 Abatement procedure

**ORDINANCE NO. 19-1014
AN ORDINANCE REPEALING AND REPLACING SECTION 8.28.040 ABATEMENT
PROCEDURE OF THE SUSANVILLE MUNICIPAL CODE**

WHEREAS, Susanville City Council has the authority, under its police power, to enact regulation for the public peace, safety, morals and welfare of the City of Susanville (hereinafter City), Cal. Const. art. XI, § 7; and

WHEREAS, the Susanville City Council finds that certain conditions constitute a threat to the public peace, safety and welfare of the City; and

WHEREAS, the Susanville City Council is committed to protecting the public peace, safety, morals and welfare by providing security and protection to the community from harms and wrongdoings that arise from the presence of weeds and rubbish; and

WHEREAS, the Susanville City Council has an interest in maintaining the City in an orderly and safe condition; and

WHEREAS, Pursuant to the authority of Government Code Section 39501 et seq., the Susanville City Council previously established Chapter 8.28 of the Municipal Code of the City of Susanville setting forth a procedure for the requiring of and providing for the removal of weeds and rubbish from sidewalks, parking areas or streets, and from private property; and

WHEREAS, The existing Chapter 8.28.040 of the Municipal Code of the City of Susanville needs some modifications to make it consistent to the service requirements of other sections of the Municipal Code of the City of Susanville; and

WHEREAS, The existing Chapter 8.28.040 of the Municipal Code of the City of Susanville needs some modifications to make it consistent with current practices of the City of Susanville; and

WHEREAS, the City of Susanville proposes amending Municipal Code section 8.28.040 as set forth below.

THE CITY COUNCIL OF THE CITY OF SUSANVILLE DOES ORDAIN AS FOLLOWS:

Section 1

Section 8.28.040 of the Susanville Municipal Code is hereby repealed in its entirety and amended to provide as follows:

8.28.040 Abatement procedure.

In the event of a violation of Section 8.28.030, the fire chief of the city or his or her designated representative shall proceed as follows:

A. Notice to Remove. The Fire Chief is authorized to give notice to the owner, agent, lessee or other person having charge or control of private property, and order the removal or destruction of weeds or rubbish from the property or abutting public property within ten (10) days. Hereinafter this notice shall be referred to as "Notice to Remove"

and the removal or destruction of weeds or rubbish shall be referred to as "weed or rubbish abatement". The Notice to Remove shall be served by posting in a conspicuous place on or in front of the property or personal service on such owner, agent, lessee or other person having charge or control of the premises as well as any of the following methods: i) by certified mail, return receipt requested, or ii) regular first class mail addressed to the owner, agent, lessee or other person having charge or control of the property, at his or her last known address. In the event the Notice to Remove is directed to one other than the owner of the property, a courtesy copy of the Notice to Remove shall be sent by regular first class mail to the owner at his or her last known address on file with the Lassen County Assessor's office.

B. Action Upon Noncompliance. Upon the failure, neglect or refusal of any owner, agent, lessee, or other person having charge or control, to properly perform weed or rubbish abatement on the property within ten (10) days of the Notice to Remove being served as required by this chapter, the Fire Chief is authorized to pay for the weed and rubbish abatement from the City code enforcement and abatement funds.

C. If the owner, agent, lessee or other person having control of the private real property in the City fails to perform weed or rubbish abatement in accordance with the Notice to Remove, within ten (10) days after service of the Notice to Remove it shall be the duty of the Fire Chief to abate the weeds and rubbish by destroying or causing to be destroyed, or removing or causing to be removed, the weeds and/or rubbish. Fire Chief and/or contracting agents of the City are expressly authorized to enter upon private property for the purpose of weed and rubbish abatement without prior consent of any owner, agent, lessee, or other person having charge or control or court permission and without a warrant, so long as the service requirements of this chapter are followed. It is unlawful for any person to interfere, hinder, or refuse to allow the Fire Chief and/or contracting agents of the City to enter upon private property for the purpose of weed and rubbish abatement in accordance with the provisions of this chapter. Any person owning, occupying, renting, managing, leasing, or controlling real property in the City shall have the right to destroy or remove weeds or rubbish or have the same destroyed or removed at his or her own expense at any time before the arrival of the Fire Chief or authorized representatives for such purpose.

D. The City Finance Director shall keep an account of the cost of weed and rubbish abatement to the City for each separate lot or parcel of land and the portions of adjoining public property to the same. The City Finance Director shall serve by first class mail, a bill for the weed and rubbish abatement on a lot or parcel of land or adjoining public property to each owner, agent, lessee or other person having control of the lot or parcel of land or adjoining public property. Each owner, agent, lessee or other person having control the lot or parcel of land or adjoining public property shall have 30 days from the date the bill was mailed, to pay the bill in full. The City Finance Director shall keep an account of the unpaid cost to the City for the weed and rubbish abatement for each separate lot or parcel of land and the portions of public property adjoining the same, and shall embody the account in a report and assessment list to the City Administrator. The City Administrator will present the report and assessment list to the City Council. The report shall refer to each separate lot or parcel of land and adjoining public property by description sufficiently reasonable to identify the same, together with the expense proposed to be assessed against it. The cost of the weed and rubbish abatement may include the actual cost to remove or destroy weeds and/or rubbish as well as incidental expenses, including, but not limited to, the expenses and costs to the City in the

inspection of lots, preparations of notices, specifications and contracts, and in inspecting work, the cost of printing, servicing and mailing required under this Chapter in an amount to be set by resolution of the City Council, per each occurrence of abatement.

E. The City Administrator shall post a copy of the report and assessment list on a bulletin board near the entrance to the City Hall, together with the notice of the filing thereof, and the time and place where and when it will be submitted to the City Council for hearing and confirmation. The Finance Director shall mail to the persons in the manner prescribed in Section 8.28.040 a notice in the format substantially as follows:

**ASSESSMENT FOR DESTRUCTION OR
REMOVAL OF WEEDS AND RUBBISH AND NOTICE OF HEARING THEREON.**

NOTICE IS HEREBY GIVEN that pursuant to the provisions of Susanville Municipal Code, Title 8, Chapter 8.28, the Fire Chief has performed weed or rubbish abatement by destroying or removing weeds or rubbish from the real property owned, occupied, rented, managed or controlled by you and from the portions of streets, alleys and sidewalks adjoining the same, which real property is described as follows:

(here insert description of real property sufficient for reasonable and ready identification)

The cost of said abatement proposed to be assessed against the said real property is \$ _____.

FURTHER NOTICE IS HERBY GIVEN that on _____, the ____ day of _____, 20__, at the hour of ____ p.m. in the Council Chambers of City Hall, of the City of Susanville, located at 66 North Lassen Street, Susanville, Lassen County, California, the report of the Finance Director on the cost of the weed or rubbish abatement and the assessment list thereof will be presented to the City Council for consideration, correction and confirmation and that at said time and place any and all persons interested in or having any objections to said report or list of proposed assessments, or to any matter or thing contained therein may appear and be heard. The failure to make any objection to said report and list shall be deemed a waiver of the same.

Upon confirmation of said assessment by the City Council, the amount thereof will be payable. In the event the same is not paid said assessment will be added to the tax bill for said property and thereafter shall become a lien on said property.

Dated: _____

City Administrator of City of Susanville

Section 2. The City Clerk shall, within fifteen days after passage of this Ordinance, cause this Ordinance to be published at least once in the Lassen County Times, an adjudicated newspaper of general circulation, published and circulated within the City.

Section 3. If any section, subsection, sentence, clause or phrase of this Ordinance is, for any reason, held to be invalid or unconstitutional, such invalidity or unconstitutionality shall not affect the validity or constitutionality of the remaining portions of this Ordinance, it being expressly declared that this Ordinance and each section, subsection, clause and phrase hereof would have been prepared, proposed, adopted, approved and ratified irrespective of the fact that any one or more other sections, subsections, sentences, clause or phrases be declared invalid or unconstitutional.

Section 4. This Ordinance shall take effect upon the thirty-first day after its final passage.

APPROVED: _____
Kevin Stafford, Mayor

ATTEST: _____
Gwenna MacDonald, City Clerk

The foregoing Ordinance No. 19-1014 was adopted at a regular meeting of the City Council of the City of Susanville, held on the 17th day of April, 2019 by the following vote:

AYES:
NOES:
ABSENT:
ABSTAINING:

Gwenna MacDonald, City Clerk

APPROVED AS TO FORM: _____
Jessica Ryan, City Attorney

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: Kevin Jones, Police Chief

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: **Resolution No. 19-5636** approving Operational Agreement with Lassen County Sheriff's Office

PRESENTED BY: Kevin Jones, Police Chief

SUMMARY: The Lassen County Sheriff's Office is applying for a federal grant (Byrne-JAG), to fund the Lassen County Prevention and Education Program. This program will fund a part-time Youth Services Officer for SPD who will work with LCSO's Youth Officers in the implementation of Life Skills, Safe Date and Green Dot curriculum in High Schools and Middle schools throughout Lassen County.

If funded, this is a three-year grant program that will begin in October of 2019 and will operate for three (3) years. Funding received will cover 100% of a 30 hour Youth Services Officer and benefits.

Lassen Co. Sheriff's Office is the receiver and administrator of this grant, funds would be received through reimbursement. If awarded, the grant will return to the City Council for formal agreement and budget.

FISCAL IMPACT: N/A

ACTION

REQUESTED: Motion to approve Resolution No. 19-5636 approving Operational Agreement with Lassen County Sheriff's Office

ATTACHMENTS: Resolution No. 19-5636
Operational Agreement

RESOLUTION NO. 19-5636
A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SUSANVILLE
AUTHORIZING CHIEF OF POLICE TO SIGN OPERATIONAL AGREEMENT WITH
LASSEN COUNTY SHERIFF'S OFFICE

WHEREAS, City of Susanville Police Department has a need to facilitate more programs for youth in our community; and

WHEREAS, Lassen County Sheriff's Office is currently applying for a grant to fund the Lassen County Prevention and Education Program; and

WHEREAS, Susanville Police Department would like to collaborate with the Sheriff's Office on this grant opportunity; and

WHEREAS, authorization is needed to sign the operational agreement with the LCSD for the purpose of their application.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Susanville authorizes the Chief of Police to sign the Operational Agreement with the Lassen Co. Sheriff's Office for the purpose of a grant application.

APPROVED: _____
Kevin Stafford, Mayor

ATTEST: _____
Gwenna MacDonald, City Clerk

The foregoing Resolution was adopted at a regular meeting of the City Council of the City of Susanville, held on the 17th day of April, 2019 by the following vote:

AYES:
NOES:
ABSENT:
ABSTAINING:

Gwenna MacDonald, City Clerk

APPROVED AS TO FORM: _____
Jessica Ryan, City Attorney

**Operational Agreement
Between the Lassen County Sheriff's Office and Susanville Police Department**

This Operational Agreement stands as evidence that the **Lassen County Sheriff's Office** and the **Susanville Police Department** intend to work together toward the goals outlined in the Three-Year JAG Strategy. Both agencies believe that implementation of the **Lassen County Prevention & Education Program** as described within this proposal, will further these goals. Each agency agrees to participate in the JAG Program, if selected for funding, as outlined herein.

The **Lassen County Sheriff's Office** project will closely coordinate JAG services and activities with the **Susanville Police Department** through:

- Project staff including, but not limited to the *Susanville Police Department's* Youth Services Officer, being readily available to assist all grant partners, on a full time basis of 30 hours per week in the implementation of Life Skills Training, Safe Date, and Green Dot curriculum in High Schools and Middle Schools throughout Lassen County as well as Lassen College; and
- Attend regularly scheduled meetings occurring at least monthly between Lassen County Sheriff's Office Youth Services Officer, Behavioral Health's Prevention Specialists, Susanville Indian Rancheria's Indian Education Coordinator, Community Church's Youth Leader, to discuss strategies, timetables, and implementation of mandated services.

Specifically:

- Implementation of Safe Date and Life Skills Training Curriculum through the school-based program in middle schools and high schools throughout Lassen County.
- Implementation of the Green Dot Program through community engagement and outreach.
- Reinstatement of the Life Skills Program through the Diversionary Program for delinquent youth;
- The Lassen County Sheriff, Susanville Chief of Police, Lassen County Chief of Probation, and Lassen County Health and Human Services' Director, Susanville Indian Rancheria Education Coordinator, and Community Church Youth Leader will meet at least quarterly to discuss the progress of the program, budget and administrative policies.
- This Agreement will be effective from October, 2019 to December 31, 2022.
- Funding for the salary and associated benefits of a full-time (up to 30 hours per week) Susanville Police Department Youth Services Officer from JAG state funds will be designated to the City of Susanville for their services (contingent on annual funding through JAG program).
- Amount of JAG state funds designated to the Susanville Police Department are estimated at \$33,440 year 1, \$34,443 year 2 and \$35,476 year 3.

We the undersigned, as authorized representatives of **Lassen County Sheriff's Office** and **Susanville Police Department** do hereby approve this document.

*Kevin Jones, Chief of Police
Susanville Police Department*

Date

*Dean Growdon, Lassen County Sheriff-Coroner
Lassen County Sheriff's Office*

Date

Reviewed by:  City Administrator

- Motion Only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: Daniel Gibbs, City Engineer

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: Water Main Replacement at the Johnstonville Road and Skyline Drive Intersection

PRESENTED BY: Daniel Gibbs, City Engineer

SUMMARY: Lassen County has started the Skyline Drive road extension project from Johnstonville Road to State Route 36. The intersection at Johnstonville Road will be significantly impacted for the installation of a new traffic signal, intersection widening and transition paving. In order to avoid disturbing the new construction associated with the work for Skyline, staff is proposing the placement of new 12" PVC water main now in conjunction with the County's Skyline Road project

There is an opportunity for a City/County partnership as the City has included replacement of a segment of the City's existing 14" steel water main line in the Integrated Regional Water Management Implementation Grant. Once the Cady Springs project was bid and costs determined, a project was scheduled for later this year for replacing the existing 14" steel water main, mainly due to excessive leaks and depth combined with it's proximity to the City's high pressure steel gas main.

Future connections or extension of the water main north and south would be driven by local development and may necessitate disturbing of the new pavement or replacement once their needs and limits of work are known. City staff is proposing sufficient lengths of installation along Johnstonville Road to minimize impacts, within reason, on the signal, loop detection and striping. Coordination efforts with the County and their contractor, Hat Creek Construction is required in order to meet their delivery schedule and minimize disruption of the progress of the Skyline project and any inconvenience to the community.

Dig It Construction, Inc. of Chester CA, the contractor for the Main Street Water main project recently completed, has been approached and is willing to install approximately 500 linear feet (LF) of City provided water main in the intersection and extending each direction a sufficient distance to minimize damaging new improvements installed with the County project.

A contract change order (CCO) with Dig It can be negotiated and executed to accommodate the work desired. The cost of the installation is estimated at approximately \$93,000 less contingencies and construction engineering. The work would include trenching, installation of the pipe, and with temporary repair of asphalt, The City would provide the pipe and fittings. There is presently over \$500,000 in the 7114 Water Fund based on Dig It Construction being fully compensation for the Main Street Water Main project and recent payment for work performed for Caltrans on the CAP-M.

Staff is requesting direction from Council to proceed in preparing a contract change order with Dig-It Construction of Chester, CA to allow for the installation of 12" of water main through the Johnstonville Road/Skyline Road intersection. Installation of the pipe will require revisions to the City Water Capital Improvement program as well.

FISCAL IMPACT: The cost of materials to be supplied by the City from local vendors is estimated at approximately \$20,000. Costs associated with the CCO, 10% contingencies and construction engineering of 5% will bring the total cost estimated to be no more than \$127,000.

ACTION REQUESTED: Provide direction to staff authorizing execution of a contract change order with Dig it Construction for the installation of water main in Johnstonville Road and authorizing the revisions to the City Water Capital Improvement program.

ATTACHMENTS: Johnstonville Road Water Main Plan Sheet
Skyline Road Extension Plan Sheet
Dig It Construction Cost Proposal
Proposed Water Capital Improvement Program



PROPOSAL AND CONTRACT

Office: 530.258.3306
 Fax: 530.258.4246
 Cell: 530.200.0010
 PO Box 494
 803 Lorraine Drive
 Chester, CA 96020
 Email: nikki@digitconst.com

City of Susanville
 C/O Bill Hoffman, Daniel Gibbs
 720 South Street
 Susanville, Ca 96130

Work to be performed at: JOHNSTONVILLE RD

ITEM	DESCRIPTION	QTY/UNIT	PRICE	TOTAL
Site Work Proposal				
JOHNSTONVILLE RD 12" MAIN 500 LNFT QUOTE				
1.	MOB	LPSM	\$ 3500.00	\$ 3,500.00
2.	Traffic Control 3 days	LPSM	\$ 11000.00	\$ 11,000.00
3.	Trench Paving	500 LNFT	\$ 49.00	\$ 24,500.00
4.	Valve installation 12"	EACH x4	\$ 500.00	\$ 2,000.00
5.	12" C -900	500 LNFT	\$ 104.00	\$ 52,000.00

Total for job **\$93,000.00**

JOHNSTONVILLE RD 12" MAIN 1,100 LNFT QUOTE

1.	MOB	LPSM	\$ 3500.00	\$ 3,500.00
2.	Traffic control 6 days	LPSM	\$ 16000.00	\$ 16,000.00
3.	Trench Paving	1000 LNFT	\$ 35.00	\$ 38,500.00
4.	Valve installation 12"	EACH x4	\$ 500.00	\$ 2,000.00
5.	12" C-900	LNFT	\$ 101.00	\$ 111,100.00

Total for job **\$ 171, 100.00**

PROPOSAL AND CONTRACT

City of Susanville
720 South Street
Susanville Ca 96130

PROPOSAL/ CONTRACT

Dig It Construction A Lic. #747715
Bond #9004307
Insured through Traveler's \$ 2,000,000 - \$1,000,000 Each Occurrence
Workman's Comp through Traveler's
Tax id # 16-1695023

NOTES: Anything not specifically included in above quote shall be considered excluded.

This Proposal/Contract does not include any permits. **Owner to supply all permits.** Anything not specifically included shall be considered excluded. Alteration or deviations from the above specifications involving extra cost of material or labor will only be executed upon a written change order and will become an extra charge over the sum mentioned in this contract; all agreements must be made in writing. This Proposal does not include engineering design, soils engineering, compaction tests, or construction staking. This proposal/contract is based on information provided to us at this time. Our company assumes no liability for changes on quantities or prices due to unforeseen conditions or changes required by governing agencies. All grades + or - .10'. Any material that cannot be excavated to grades (rocks, hardpan, etc.) will become an extra charge. **Amount of contract to be paid as work progresses and final payment due 15 days after completion.**

Contractor's State License Board
3132 Bradshaw Rd.
Sacramento, CA 95826

Respectfully Submitted
By *Caleb Holland*
Caleb Holland -President

ACCEPTANCE

You are authorized to furnish all materials and labor required to complete the work mentioned in the above proposal, for which *(print name)* _____ agrees to pay the amount mentioned in said proposal and according to the terms thereof.

Signed: _____ Date: _____

Johnstonville Rd 12" Main 500 LNFT QUOTE

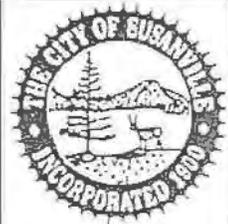
Item #	Item Description	Unit Meas	Est. Qty	Bid Unit \$	Bid Total \$
1	Mob	LPSM	1	\$ 3,500.00	\$ 3,500.00
2	Traffic Control 500 LNFT 3days	LPSM	1	\$ 11,000.00	\$ 11,000.00
3	Trench Paving 500LNFT	LNFT	500	\$ 49.00	\$ 24,500.00
4	Valve installation 4-12" valves	EACH	4	\$ 500.00	\$ 2,000.00
5	12" C-900	LNFT	500	\$ 104.00	\$ 52,000.00
				Total	\$ 93,000.00

Johnstonville Rd 12" Main 1,100 LNFT QUOTE

Item #	Item Description	Unit Meas	Est. Qty	Bid Unit \$	Bid Total \$
1	Mob	LPSM	1	\$ 3,500.00	\$ 3,500.00
2	Traffic Control 500 LNFT 6days	LPSM	1	\$ 16,000.00	\$ 16,000.00
3	Trench Paving 500LNFT	LNFT	1100	\$ 35.00	\$ 38,500.00
4	Valve installation 4-12" valves	EACH	4	\$ 500.00	\$ 2,000.00
5	12" C-900	LNFT	1,100	\$ 101.00	\$ 111,100.00
				Total	\$ 171,100.00

City Of Susanville to supply all PIPE,VALVES AND CROSS PARTS AS NEEDED

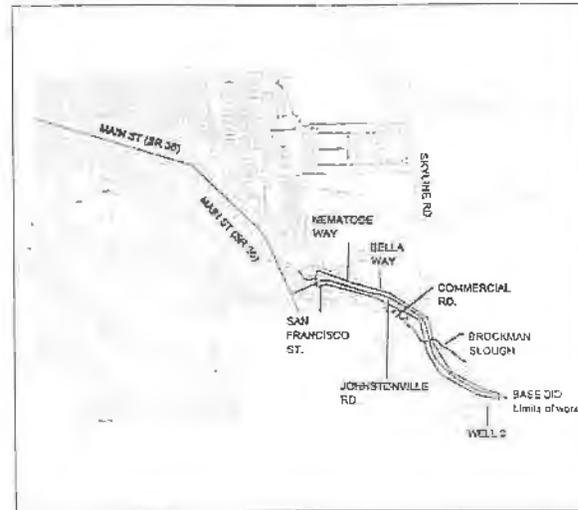
CITY OF SUSANVILLE 2018 JOHNSTONVILLE RD. WATER LINE REPLACEMENT - SAN FRANCISCO ST. TO WELL 3 (800 FT. EAST OF COMMERICAL RD.) PROP 84 USDA GRANT AGREEMENT NO. 4600011520



GENERAL NOTES

1. ALL WORK AND MATERIALS SHALL COMPLY WITH THE CITY OF SUSANVILLE STANDARD DETAILS AND THESE PLANS AND PROJECT SPECIFICATIONS. ANY DISCREPANCIES FOUND BY CONTRACTOR SHALL BE BROUGHT TO THE ATTENTION OF THE CITY ENGINEER FOR REVIEW. ANY SUBSTITUTIONS AND/OR CHANGES IN WORK SHALL BE APPROVED IN WRITING BY THE CITY ENGINEER BEFORE BEING IMPLEMENTED.
2. CONTRACTOR SHALL CALL UNDERGROUND SERVICE ALEKT AT LEAST 48 HOURS PRIOR TO BEGINNING WORK. NO WORK SHALL COMMENCE PRIOR TO ESTABLISHMENT OF USA MARKINGS AND APPROPRIATE INVESTIGATION OF SUBTERREAN FACILITIES SUCH AS POTHOLES.
3. ALL DEBRIS SHALL BE REMOVED FROM SITE AND THE AREA COMPLETELY CLEANED UPON COMPLETION OF WORK EACH DAY THE PROJECT IS ACTIVE.
4. CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE AND FEDERAL SAFETY REGULATIONS, BOTH FOR WATER DISTRIBUTION SYSTEMS AND OVERALL PROJECT SAFETY.
5. CONTRACTOR SHALL APPLY FOR AND OBTAIN APPROVAL FOR TRAFFIC CONTROL PLANS AND ANY PERMITS FROM CALTRANS PRIOR TO COMMENCING ANY WORK.
6. ALL WATER MAIN SHUT DOWNS SHALL BE CO-ORDINATED WITH THE CITY AND ALL AFFECTED PROPERTY OWNERS AT LEAST 72 HOURS PRIOR TO IMPLEMENTATION.
7. CONTRACTOR SHALL COMPLY WITH ALL ASPECTS OF TITLE 28 FOR MAINTAINING WATER SYSTEM POTABILITY AND SEPARATION FROM OTHER UTILITIES SUCH AS SANITARY AND STORM SEWER SYSTEMS, THEIR PIPELINES, ACCESS ETC.
8. STORM DRAIN SYSTEM ON MAIN ST. (NOT PAUTE CREEK) IS OWNED BY THE STATE. SANITARY SEWER SYSTEM IS OWNED BY SANITARY DISTRICT # (530) .255-5666.
9. EXISTING FACILITIES TO BE ABANDONED SHALL BE PERFORMED IN SUCH A MANNER THAT PROVIDES A 12" MINIMUM PHYSICAL SEPARATION FROM NEW MAINS. ABANDONED MAINS SHALL REMAIN IN PLACE UNLESS IN CONFLICT WITH DESIRED ALIGNMENTS FOR NEW MAINS. MAINS AND SERVICES SHALL BE PERMANENTLY CAPPED AND FILLED WITH 1 SACK CEMENT SLURRY or CRUSHED IN PLACE AS DIRECTED.

VICINITY MAP



SCALE: 1" = 100'

ABBREVIATIONS

- (N) NEW
- (E) EXISTING
- R/W RIGHT OF WAY
- CL CLASS
- BTW BETWEEN
- SD STORM DRAIN
- CSD CITY STD. DETAIL
- IPS IRON PIPE SIZE
- E/O EAST OF
- W/O WEST OF

LEGEND

- INDICATES EXISTING WATER METER BOX
- ◻ INDICATES NEW WATER METER BOX N-16 WITH LID
- INDICATES EXISTING WATER SERVICE TO BE ABANDONED
- INDICATES NEW 1" PE WATER SERVICE I.P.S.
- INDICATES EXISTING WATER VALVE. EXISTING BOX TO BE REMOVED
- ⊙ INDICATES NEW WATER VALVE WITH C18 BOX
- INDICATES NEW 8" C-800 CL255 WATER MAIN
- ⊙ INDICATES NEW 6" FIRE HYDRANT
- ⊙ INDICATES EXISTING 6" FIRE HYDRANT TO BE REMOVED ABANDON LINE.
- ⊙ MANHOLE, UTILITY
- ⊙ THRUST BLOCK
- ⊙ 'F' TYPE FITTING
- ⊙ DETAIL & SHEET NO.
- ⊙ GRATE, STORM DRAIN INLET
- ⊙ 'FJ' TEE FITTING

SHEET INDEX

SHEET 2	STATION 0+00 to STATION 3+50
SHEET 3	STATION 3+50 to STATION 9+00
SHEET 4	STATION 9+00 to STATION 13+50
SHEET 5	STATION 13+50 to STATION 17+00
SHEET 6	STATION 17+00 to STATION 20+50
SHEET 7	STATION 20+50 to STATION 24+00
SHEET 8	STATION 24+00 to STATION 30+00
SHEET 9	STATION 30+00 to STATION 38+00
SHEET 10	STATION 38+00 to STATION 48+50
SHEET 11	DETAIL SHEET NO. W-13 & W-4
SHEET 12	DETAIL SHEET NO. S-20 & W-
SHEET 13	DETAIL SHEET NO. W-5 & W-8
SHEET 14	DETAIL SHEET NO. W-10 & CALTRANS TRENCH
SHEET 15	DETAIL SHEET NO. W-1 & W-2
SHEET 16	DETAIL SHEET NO. W-3 & W-4
SHEET 17	DETAIL SHEET NO. S-1 & S-4
SHEET 18	DETAIL SHEET NO. S-8 & S-10

ATTACHED: FRONTIER AERIAL/BURIED MAP

4 PAGES OF BURIED CABLE AND FIBER ALONG JOHNSTONVILLE RD



SIGNATURE _____ DATE _____

DESIGN OFFICE: PUBLIC WORKS

DESIGNED BY: DMH/ee

DRAWN BY: BWH

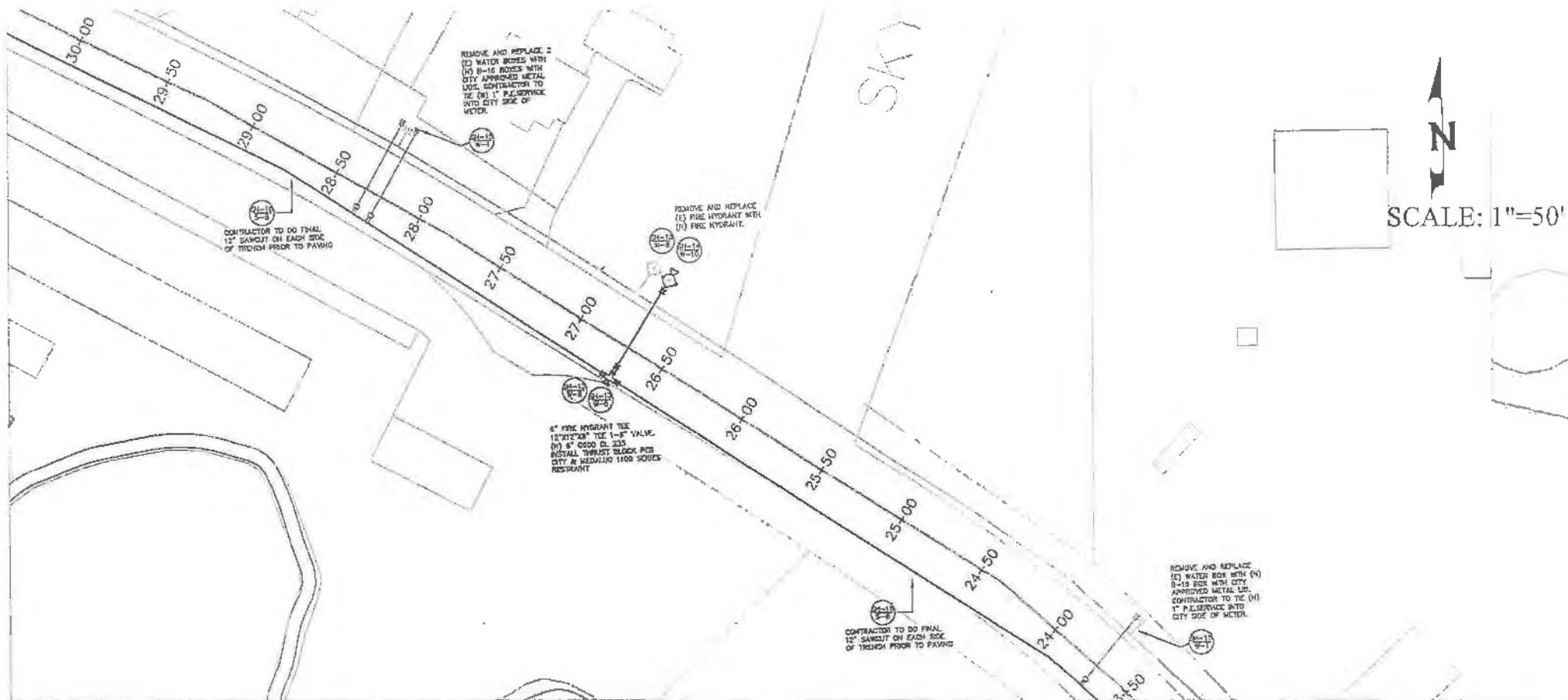
CHECKED BY: dca

APPROVED BY:

DATE: JUNE 2018

DATE REVISED:

SHEET 1 OF 14



N
SCALE: 1"=50'

* QUANTITIES PROVIDED FOR CONVENIENCE ONLY. CONTRACTOR SHALL USE BID QUANTITIES FROM THE BID BOOK ONLY. SOME WORK MAY BE BID LUMP SUM. LUMP SUM PRICES SHALL INCLUDE ALL WORK NECESSARY FOR COMPLETE INSTALLATION AND OPERATION OF WATER FACILITIES WHETHER SHOWN OR NOT. CONTRACTOR SHALL PERFORM ADEQUATE DUE DILIGENCE WITH ALL OWNERS TO ENSURE THEIR FACILITIES ARE OPERATIONAL AND SAFE FOR PUBLIC CONSUMPTION AT COMPLETION OF WORK.

NO	JOHNSTONVILLE RD	12" C900 PVC WATER MAIN	6" C900 PVC WATER MAIN	12" DUCTILE IRON	VALVE & FITTINGS	THRUST BLOCK	1/2" HMA TONS	FIRE HYDRANTS
1.	FROM STA 30+00 TO 24+00	600 LF	80 LF	0 LF	6"-1 12"-0	2	60 TONS	1

CITY OF
SUSANVILLE

JOHNSTONVILLE

WATER LINE
REPLACEMENT

DESIGN OFFICE: PUBLIC WORKS
HYDRANT RELOCATION

DESIGNED BY: BWH

DRAWN BY: BWH

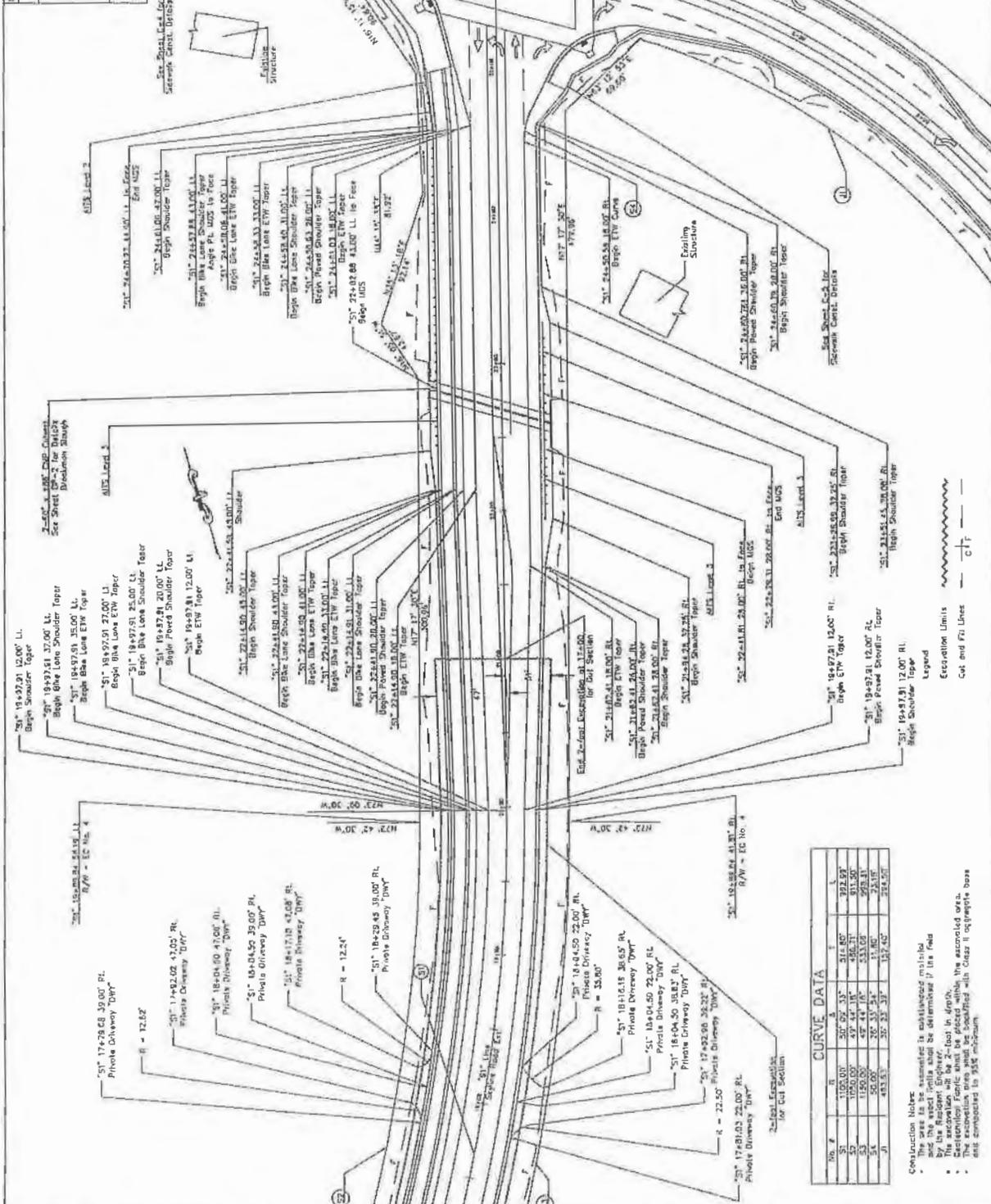
CHK'D BY: Jlg

DATE: AUGUST 8, 2013

DATE REVISED:

SHEET 8 of 23

COUNTY: LASSEN
 PROJECT NAME: LASSSEN COUNTY PUBLIC WORKS
 SHEET NO.: 55
 DATE: 01/27/19
 DRAWN BY: [Signature]
 CHECKED BY: [Signature]
 PROJECT LOCATION: 207 Nevada Street, Sacramento, CA 95830



No. #	R	L	E
1	17428.63	39.00'	RL
2	17428.63	39.00'	RL
3	17428.63	39.00'	RL
4	17428.63	39.00'	RL
5	17428.63	39.00'	RL
6	17428.63	39.00'	RL
7	17428.63	39.00'	RL
8	17428.63	39.00'	RL
9	17428.63	39.00'	RL
10	17428.63	39.00'	RL

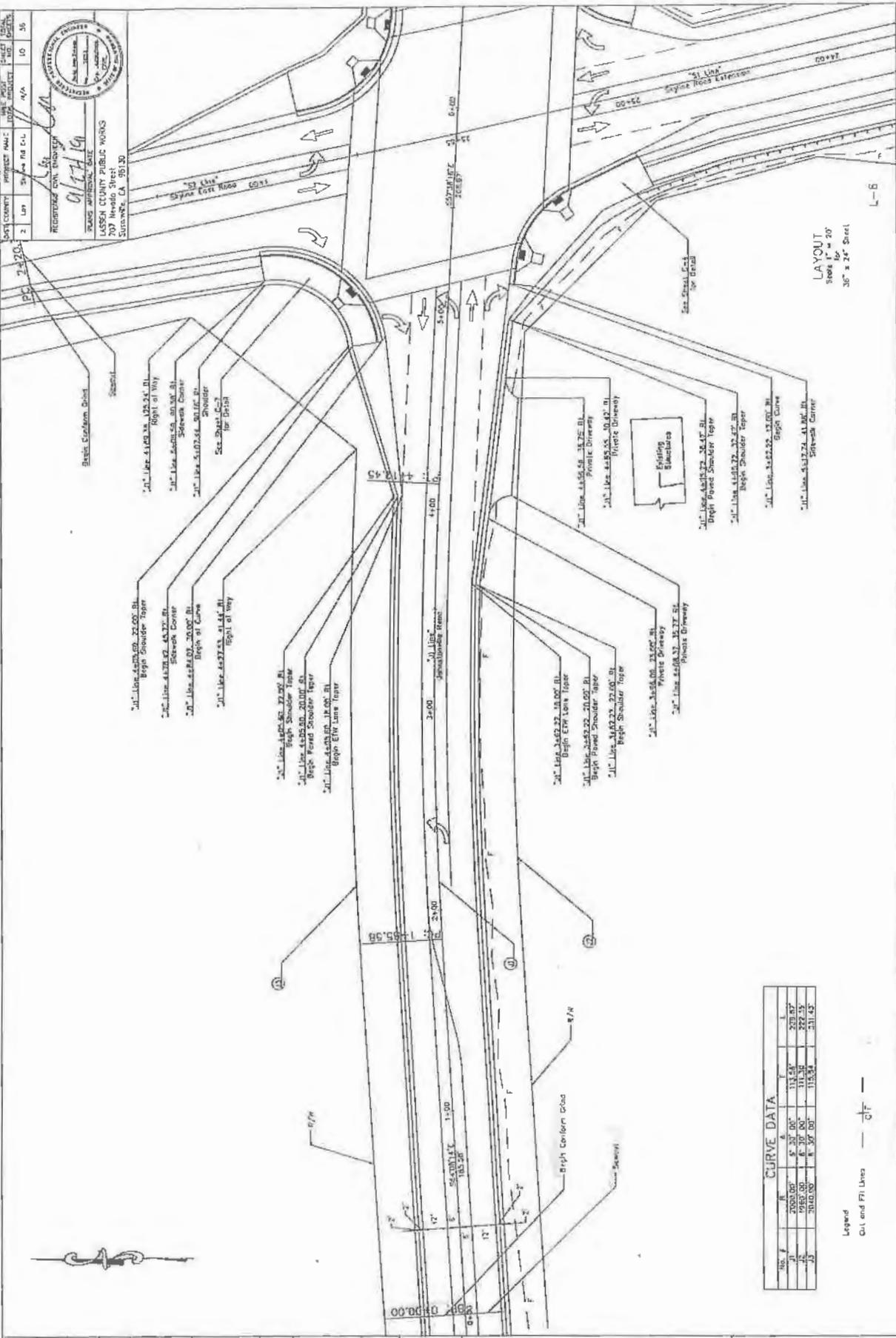
CURVE DATA
 No. # R L E
 1 17428.63 39.00' RL
 2 17428.63 39.00' RL
 3 17428.63 39.00' RL
 4 17428.63 39.00' RL
 5 17428.63 39.00' RL
 6 17428.63 39.00' RL
 7 17428.63 39.00' RL
 8 17428.63 39.00' RL
 9 17428.63 39.00' RL
 10 17428.63 39.00' RL

Construction Notes:
 - The area to be excavated is subgrade only.
 - Excavation shall be 2'-0" in depth.
 - The excavation area shall be backfilled with Class II compacted base.
 - The backfill shall be compacted to 95% minimum.

LAYOUT
 Scale: 1" = 30'
 30' x 7.5" Sheet

COUNTY OF LASSEN
 PROJECT ENGINEER - David John Emoga

DATE: 01/27/19
 REASON: [Blank]
 DATE: [Blank]



LAYOUT
Scale 1" = 20'
36" x 24" Sheet

L-6

CURVE DATA			
Sta. #	R	Δ	L
117	2000.00'	5° 37' 00"	115.54'
118	1980.00'	6° 30' 00"	114.30'
119	2010.00'	6° 30' 00"	115.34'

Legend
Cut and Fill Lines — C/F —

DATE	REVISION	DATE	DESCRIPTION
9/18			
PROJECT ENGINEER — David John Emago			
COUNTY OF LASSEN			

PROJECT NAME: 1074 PROJECT 10 36
 SHEET NO. 4/A
 DATE: 9/18
 COUNTY OF LASSEN
 PUBLIC WORKS
 707 Nevada St., Suite 200
 Sacramento, CA 95811



Local Streets and Roads - Projected FY2018-19 Revenues

Based on State Dept of Finance statewide revenue projections as of January 2019

Estimated 15 January 2019

	Highway Users Tax Acct (HUTA) ⁽¹⁾ Streets & Highways Code					TOTAL HUTA	SB1		TOTAL
							TCRF Loan Repayment ⁽⁶⁾	Road Maintnc Rehab Acct ⁽⁷⁾	
	Sec2103 ⁽⁵⁾	Sec2105 ⁽³⁾	Sec2106 ⁽³⁾	Sec2107 ⁽³⁾	Sec2107.5 ⁽⁴⁾				
HUMBOLDT COUNTY									
ARCATA	65,262	102,968	75,976	135,234	4,000	383,439	20,748	291,737	695,924
BLUE LAKE	4,540	7,164	9,752	9,409	1,000	31,865	1,443	20,297	53,605
EUREKA	96,453	152,179	109,993	199,866	6,000	564,492	30,663	431,168	1,026,323
FERNDALE	4,863	7,673	10,104	10,077	1,000	33,718	1,548	21,740	57,004
FORTUNA	42,716	67,395	51,387	88,514	3,000	253,012	13,880	190,950	457,542
RIO DELL	11,947	18,850	17,830	24,756	1,000	74,383	3,798	53,406	131,587
TRINIDAD	1,302	2,054	6,220	2,698	1,000	13,273	414	5,820	19,507
IMPERIAL COUNTY									
BRAWLEY	97,255	153,444	86,731	201,527	6,000	544,958	30,918	434,752	1,010,628
CALEXICO	146,143	230,578	127,916	302,831	6,000	813,468	46,460	653,293	1,513,222
CALIPATRIA	27,332	43,122	27,825	56,635	2,000	156,914	8,689	122,178	287,782
EL CENTRO	164,291	259,210	143,205	340,436	6,000	913,142	52,230	734,418	1,699,789
HOLTVILLE	23,061	36,384	24,227	47,785	2,000	133,457	7,331	103,086	243,875
IMPERIAL	68,717	108,419	62,690	142,393	4,000	366,219	21,846	307,182	715,247
WESTMORLAND	8,247	13,012	11,748	17,090	1,000	51,097	2,622	36,868	90,587
INYO COUNTY									
BISHOP	13,912	21,950	18,810	28,828	1,000	84,501	4,423	62,191	151,115
KERN COUNTY									
ARVIN	76,961	121,426	52,276	159,475	5,000	415,138	24,457	344,034	783,638
BAKERSFIELD	1,372,214	2,165,013	851,295	2,843,441	10,000	7,241,963	496,240	6,134,112	13,812,314
CALIFORNIA CITY	52,765	83,251	37,350	109,338	3,000	285,704	16,775	235,873	538,352
DELANO	188,983	298,169	121,380	391,603	7,500	1,007,635	60,080	844,798	1,912,513
MARICOPA	4,101	6,470	7,330	8,497	1,000	27,397	1,304	18,331	47,031
MCFARLAND	53,581	84,538	37,853	111,029	4,000	291,001	17,034	239,520	547,555
RIDGECREST	102,239	161,307	67,869	211,855	6,000	549,270	32,503	457,031	1,038,804
SHAFTER	68,359	107,854	46,969	141,651	4,000	368,833	21,732	305,581	696,145
TAFT	33,635	53,068	25,549	69,597	2,000	183,949	10,693	150,356	344,998
TEHACHAPI	51,130	80,671	36,341	105,949	3,000	277,091	16,255	228,563	521,909
WASCO	98,227	154,978	65,394	203,541	6,000	528,140	31,227	439,097	998,464
KINGS COUNTY									
AVENAL	55,000	86,776	40,723	113,969	4,000	300,468	17,485	245,863	563,816
CORCORAN	88,018	138,870	62,288	182,387	5,000	476,563	27,982	393,460	898,004
HANFORD	206,365	325,592	139,584	427,620	7,500	1,106,661	65,605	922,498	2,094,764
LEMOORE	91,845	144,909	64,788	190,318	6,000	497,860	29,199	410,570	937,628
LAKE COUNTY									
CLEARLAKE	56,462	89,082	54,663	116,997	4,000	321,204	17,950	252,396	591,550
LAKEPORT	18,212	28,733	20,883	37,737	2,000	107,565	5,790	81,410	194,765
LASSEN COUNTY									
SUSANVILLE	63,662	100,444	45,704	214,506	4,000	428,315	20,239	284,586	733,140

Local Streets and Roads - Projected FY2019-20 Revenues

Based on State Dept of Finance statewide revenue projections as of January 2019

Estimated 15 January 2019

	Highway Users Tax Acct (HUTA) ⁽¹⁾ Streets & Highways Code					TOTAL HUTA	SB1		TOTAL
	Sec2103 ⁽⁶⁾	Sec2105 ⁽³⁾	Sec2106 ⁽³⁾	Sec2107 ⁽³⁾	Sec2107.5 ⁽⁴⁾		TCRF Loan Repayment ⁽⁵⁾	Road Maintncl Rehab Acct ⁽⁷⁾	
HUMBOLDT COUNTY									
ARCATA	156,911	102,352	75,542	134,403	4,000	473,208	20,748	304,517	798,473
BLUE LAKE	10,917	7,121	9,722	9,351	1,000	38,110	1,443	21,186	60,740
EUREKA	231,904	151,270	109,352	198,639	6,000	697,164	30,663	450,056	1,177,883
FERNDALE	11,693	7,627	10,072	10,016	1,000	40,407	1,546	22,692	64,646
FORTUNA	102,703	66,992	51,102	87,971	3,000	311,768	13,580	199,315	524,663
RIO DELL	28,725	18,737	17,750	24,604	1,000	90,816	3,798	55,746	150,360
TRINIDAD	3,130	2,042	6,211	2,681	1,000	15,064	414	6,074	21,552
IMPERIAL COUNTY									
BRAWLEY	233,832	152,527	86,232	200,290	6,000	678,880	30,918	453,796	1,163,594
CALEXICO	351,374	229,199	127,166	300,972	6,000	1,014,711	46,460	681,911	1,743,082
CALIPATRIA	65,714	42,865	27,685	56,287	2,000	194,551	8,689	127,530	330,770
EL CENTRO	395,007	257,661	142,361	338,346	6,000	1,139,374	52,230	766,589	1,958,193
HOLTVILLE	55,445	36,167	24,109	47,492	2,000	165,212	7,331	107,602	280,146
IMPERIAL	165,218	107,771	62,337	141,519	4,000	480,845	21,846	320,638	823,329
WESTMORLAND	19,829	12,934	11,708	16,985	1,000	62,454	2,622	38,483	103,559
INYO COUNTY									
BISHOP	33,450	21,819	18,725	26,651	1,000	103,645	4,423	64,916	172,983
KERN COUNTY									
ARVIN	185,039	120,700	61,987	158,496	5,000	521,221	24,467	359,104	904,792
BAKERSFIELD	3,299,237	2,152,071	846,134	2,825,980	10,000	9,133,423	436,240	6,402,818	15,972,481
CALIFORNIA CITY	126,865	82,753	37,152	108,667	3,000	358,436	16,775	246,206	621,416
DELANO	454,376	296,386	120,670	389,198	7,500	1,268,129	60,080	881,805	2,210,014
MARICOPA	9,859	6,431	7,314	8,445	1,000	33,049	1,304	19,134	53,487
MCFARLAND	128,826	84,032	37,652	110,347	4,000	364,857	17,034	250,012	631,904
RIDGECREST	245,814	160,343	67,485	210,554	6,000	690,196	32,503	477,051	1,199,750
SHAFTER	164,357	107,209	46,712	140,781	4,000	463,059	21,732	318,967	803,757
TAFT	80,869	52,750	25,422	69,269	2,000	230,311	10,693	156,943	397,947
TEHACHAPI	122,933	80,188	36,149	105,299	3,000	347,569	16,255	238,575	602,399
WASCO	236,168	154,051	65,025	202,291	6,000	663,536	31,227	458,331	1,153,095
KINGS COUNTY									
AVENAL	132,238	86,258	40,504	113,269	4,000	376,268	17,485	256,633	650,386
CORCORAN	211,623	138,040	61,937	181,267	5,000	597,867	27,982	410,696	1,036,544
HANFORD	496,166	323,646	138,763	424,994	7,500	1,391,069	65,605	962,908	2,419,582
LEMOORE	220,825	144,043	64,422	189,149	6,000	624,439	29,199	428,555	1,082,193
LAKE COUNTY									
CLEARLAKE	135,751	88,550	54,359	116,279	4,000	398,939	17,950	263,452	680,341
LAKEPORT	43,786	28,562	20,785	37,505	2,000	132,639	5,790	84,976	223,404
LASSEN COUNTY									
SUSANVILLE	153,065	99,843	45,454	213,695	4,000	516,058	20,239	297,052	833,349

Local Streets and Roads - Projected Revenues

Estimated 22 January 2019

	2018-19		SBI		2019-20		SBI	
	Hwy Users Account	Loan Repayment	Road Mntnc Rehab Acct	TOTAL	Hwy Users Account	Loan Repayment	Road Mntnc Rehab Acct	TOTAL
Assembly District 1								
Chico	1,886,078	104,141	1,464,364	3,454,583	2,336,711	104,141	1,528,510	3,969,362
Ccounty of Butte	5,767,105	376,884	5,299,486	11,443,475	7,404,425	376,884	5,531,631	13,312,940
Susanville	428,315	20,239	284,586	733,140	516,058	20,239	297,052	833,349
County of Lassen	2,311,966	156,533	2,201,055	4,669,554	2,997,926	156,533	2,297,472	5,451,931
Alturas	62,008	3,234	45,478	110,720	76,025	3,234	47,470	126,729
County of Modoc	2,114,728	152,668	2,146,716	4,414,112	2,784,479	152,668	2,240,754	5,177,901
Grass Valley	321,279	14,706	206,791	542,777	384,618	14,706	215,850	615,174
Nevada City	83,347	3,638	51,155	138,139	99,015	3,638	53,396	156,048
Truckee	2,019,700	18,811	264,511	2,303,023	2,100,718	18,811	276,098	2,395,627
County of Nevada	3,319,485	188,581	2,651,698	6,159,764	4,137,279	188,581	2,767,856	7,093,716
Colfax	49,186	2,425	34,093	85,704	59,679	2,425	35,586	97,690
County of Placer	10,833,210	503,586	7,081,079	18,417,875	13,011,824	503,586	7,391,267	20,906,677
Portola	158,921	2,437	34,267	195,625	169,492	2,437	35,768	207,697
County of Plumas	2,203,977	125,586	1,765,905	4,095,468	2,753,971	125,586	1,843,260	4,722,817
Anderson	223,064	11,574	162,741	397,378	273,102	11,574	169,869	454,545
Redding	1,928,493	103,024	1,448,649	3,480,165	2,373,909	103,024	1,512,108	3,989,040
Shasta Lake	220,988	11,462	161,171	393,620	270,543	11,462	168,231	450,236
County of Shasta	5,503,567	347,084	4,880,463	10,731,114	7,011,397	347,084	5,094,253	12,452,734
Loyalton	35,964	867	12,194	49,026	39,727	867	12,728	53,323
County of Sierra	924,466	60,929	856,742	1,842,137	1,191,742	60,929	894,272	2,146,943
Dorris	25,776	1,089	15,318	42,183	30,487	1,089	15,989	47,566
Dunsmuir	66,399	1,895	26,640	94,933	74,593	1,895	27,807	104,294
Etna	21,185	839	11,798	33,822	24,814	839	12,314	37,967
Fort Jones	21,082	833	11,718	33,633	24,686	833	12,232	37,751
Montague	35,640	1,627	22,882	60,149	42,678	1,627	23,884	68,189
Mount Shasta	144,421	3,827	53,819	202,067	160,974	3,827	56,176	220,978
Tulelake	26,686	1,139	16,016	43,840	31,612	1,139	16,717	49,468
Weed	67,155	3,346	47,048	117,548	81,626	3,346	49,109	134,080
Yreka	169,589	8,841	124,319	302,750	207,828	8,841	129,765	346,434
Ccounty of Siskiyou	3,653,126	249,845	3,513,151	7,416,122	4,747,831	249,845	3,667,046	8,664,722
Total District 1	44,626,905	2,481,691	34,895,850	82,004,446	55,419,767	2,481,691	36,424,470	94,325,928

SB1 Road Repair and Accountability Act of 2017 - New Local Streets & Roads Funding - Ten Year Projections

Estimated: 11-Jan-18

	<u>2017-18</u>	<u>2018-19</u>	<u>2019-20</u>	<u>2020-21</u>	<u>2021-22</u>	<u>2022-23</u>	<u>2023-24</u>	<u>2024-25</u>	<u>2025-26</u>	<u>2026-27</u>	<u>TenYr Total</u>
LASSEN COUNTY											
SUSANVILLE	122,768	318,431	461,061	510,302	540,586	564,936	589,868	617,250	643,914	671,475	5,030,682
County of Lassen	944,703	2,450,541	3,520,000	3,920,000	4,130,000	4,280,000	4,470,000	4,670,000	4,880,000	5,050,000	38,305,244
Total City & County: Lassen	1,067,461	2,768,972	3,971,061	4,430,302	4,670,586	4,854,936	5,059,868	5,287,250	5,503,914	5,721,475	43,335,926

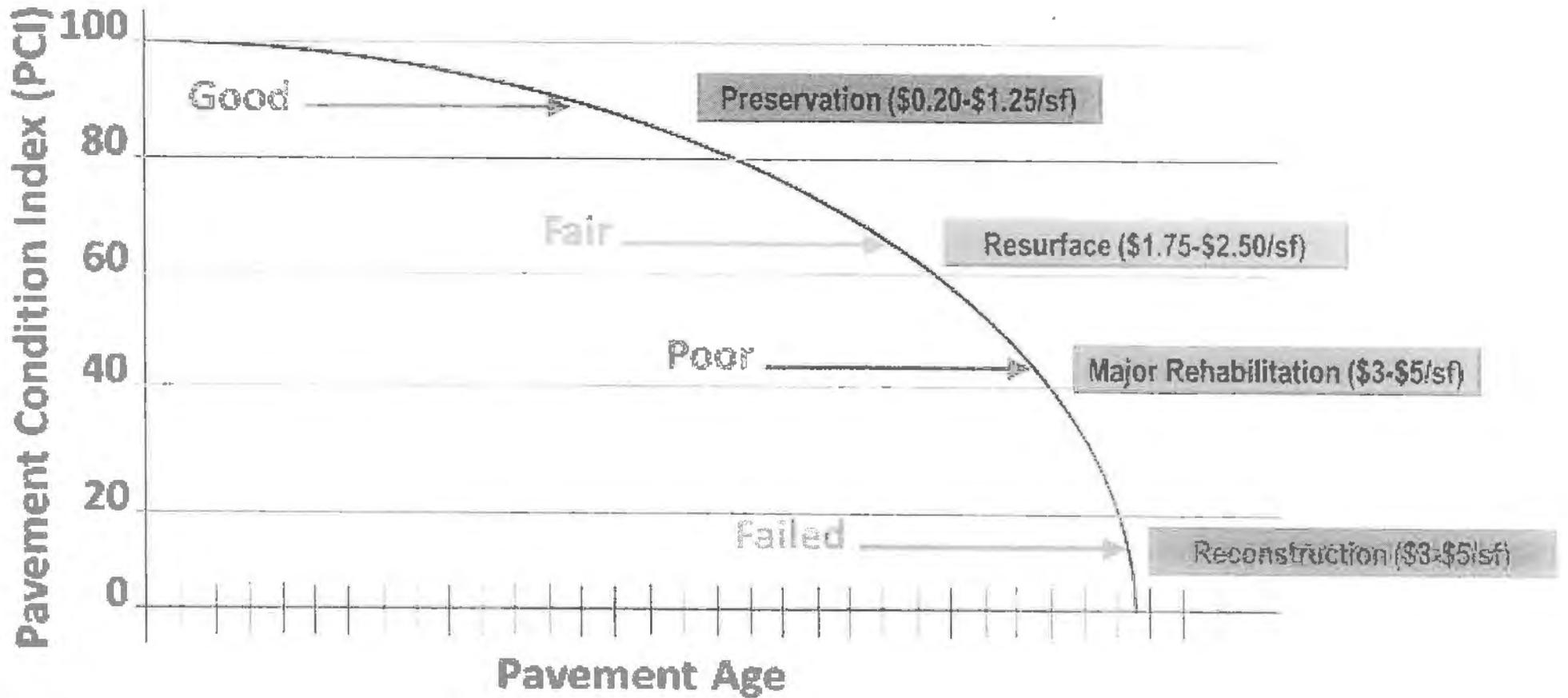
TABLE 4: WATER CAPITAL IMPROVEMENT/INFRASTRUCTURE REPLACEMENT PLANS (revised April 2019)

Project	1	2	3	4	5	Total	Removed Cost
	COST ESCALATED FROM 2015 DOLLARS (2%/YEAR)						
	2016/17	2017/18	2018/19	2019/20	2020/21		
CIP	Develop Well (Former Nathan Property)					\$ -	\$ 415,920
	Emergency Power Upgrades (Harris Booster, Spring Ridge Booster, Well 3)					\$ -	\$ 157,410
DEPRECIATION	S Gilman; Main St. to River St.			\$ 9,330	\$ 199,500	\$ 208,830	
	Richmond Rd.; Cypress to Riverside Dr.			\$ 6,370	\$ 136,250	\$ 142,620	
	Monrovia alley; Covina St. to East End				\$ 14,020	\$ 294,346	\$ 308,366
	Upland Alley; Covina St. to East End				\$ 8,900	\$ 186,806	\$ 195,706
	Paiute Ln; Glenn Dr. north	\$ 9,080	\$ 194,360			\$ 203,440	
	N. Pine St.; Burma Rd to View Dr.			\$ 7,020	\$ 150,120	\$ 157,140	
	N. Roop St.; North Alley to Willow St.			\$ 17,540	\$ 375,330	\$ 392,870	
	Third St; Cedar St. to Park St.	\$ 10,590	\$ 226,750			completed	\$ 237,340
	Main Street: Weatherlow to Park		\$ 680,331			\$ 680,331	
	Main Street: Park to Spring			\$ 642,776		\$ 642,776	
	Third St; Ash to Hall St.	\$ 132,530				completed	\$ 132,530
	Park St.; Fifth St. to Fourth St.				\$ 5,150	\$ 110,110	\$ 115,260
	Chestnut St.; Park St. 400' East				\$ 4,570	\$ 97,750	\$ 102,320
	Johnstonville Rd; San Francisco to Skyline to Well 3 (800' east of Commerical Road)			\$ 168,300	\$ 757,480	\$ 925,780	
TOTAL BY YEAR (2016)	\$ 152,200	\$ 1,101,441	\$ 851,336	\$ 1,651,320	\$ 689,012	\$ 4,075,439	\$ 943,200
2% ESCALATOR						\$ 4,156,948	

EXHIBIT 'A'

SUSANVILLE 'SB-1' PROJECT Construction Cost Estimate FY 2019-2020											
PROJECT SCOPE & PROGRAM REPORT				FY 19/20		FY 19/20		FY 19/20		FY 19/20	
				GRAND TOTAL ESTIMATED TO BE SPENT THIS FISCAL YEAR		NUMA - SKYLINE TO CAMERON (CRACK SEAL & TYPE II SLURRY)		BUNYAN - ASH TO EAST END (CRACK SEAL & TYPE II SLURRY)		GRAND - MAIN TO CHESTNUT (CRACK SEAL & TYPE II SLURRY)	
Item No.	Item Description	Unit	Unit Price	Quantity	Total	Quantity	Total	Quantity	Total	Quantity	Total
0	DESIGN/ENVIRONMENTAL/ADV/BID/SWPPP (2%)	LS	\$ 25,000	0	\$ 4,838.42	1	\$ 1,201.96	1	\$ 2,056.59	1	\$ 1,360.88
1	MOBILIZATION/DEMOBILIZATION (NOT TO EXCEED 5%)	LS	\$ 3,064	0	\$ 11,571.00	1	\$ 2,997.75	1	\$ 5,129.25	1	\$ 3,444.00
2	FOG SEAL PAVEMENT REHABILITATION	SY	\$ 4.00	0	\$ -	0	\$ -	0	\$ -	0	\$ -
3	CRACK SEAL - PAVEMENT REHABILITATION	LF	\$ 2.00	0	\$ 20,200.00	1,200	\$ 2,400.00	8,200	\$ 16,600.00	600	\$ 1,200.00
4	SLURRY SEAL PAVEMENT REHABILITATION	SY	\$ 6.00	0	\$ 189,200.00	6,200	\$ 37,200.00	12,100	\$ 72,600.00	9,900	\$ 59,400.00
5	EXISTING ASPHALT GRINDING	LF	\$ 4.00	0	\$ -	0	\$ -	0	\$ -	0	\$ -
6	MISC. ASPHALT PLUG FOR C&G, VG REPLACEMENT	LF	\$ 12.00	0	\$ -	0	\$ -	0	\$ -	0	\$ -
7	TYPE 'E' ASPHALT DIKE	LF	\$ 10.00	0	\$ 17,750.00	1,400	\$ 14,000.00	75	\$ 750.00	300	\$ 3,000.00
8	REMOVE MISCELLANEOUS CONCRETE (ALL TYPES)	LS	\$ 5,000.00	0	\$ -	0	\$ -	0	\$ -	0	\$ -
9	PCC VALLEY GUTTER	SF	\$ 45.00	0	\$ -	0	\$ -	0	\$ -	0	\$ -
10	PCC SIDEWALK / DRIVEWAY REPAIR (INCLUDES AB CLASS II)	SF	\$ 25.00	0	\$ -	0	\$ -	0	\$ -	0	\$ -
11	PCC CURB & GUTTER REPAIR (INCLUDES AB CLASS II)	LF	\$ 48.00	0	\$ -	0	\$ -	0	\$ -	0	\$ -
12	PCC CURB & GUTTER SIDEWALK REPAIR (INCLUDES AB CLASS II)	SF	\$ 60.00	0	\$ -	0	\$ -	0	\$ -	0	\$ -
13	ADA CURB RAMPS	EA	\$ 5,000.00	0	\$ -	0	\$ -	0	\$ -	0	\$ -
14	"PAFF" STRIPING - YELLOW TYPE 21-WHITE STOP BAR PED XING	LF	\$ 2.50	0	\$ 13,250.00	1,400	\$ 3,500.00	3,100	\$ 7,750.00	800	\$ 2,000.00
15	TRAFFIC CONTROL (NOT TO EXCEED 5%)	LS	\$ 6,385	0	\$ 11,020.00	1	\$ 2,855.00	1	\$ 4,885.00	1	\$ 3,280.00
				SUBTOTAL \$ 242,991		SUBTOTAL \$ 62,953		SUBTOTAL \$ 107,714		SUBTOTAL \$ 72,324	
				5% CONT. \$ 12,150		5% CONT. \$ 3,148		5% CONT. \$ 5,386		5% CONT. \$ 3,616	
				SUBTOTAL \$ 255,141		SUBTOTAL \$ 66,100		SUBTOTAL \$ 113,100		SUBTOTAL \$ 75,940	
				CONSTRUCTION		CONSTRUCTION		CONSTRUCTION		CONSTRUCTION	
				15% CON ENG \$ 38,271		15% CON ENG \$ 9,915		15% CON ENG \$ 16,955		15% CON ENG \$ 11,391	
				TOTAL \$ 293,412		\$ 76,015		\$ 130,065		\$ 87,331	
BUDGET / CONSTRUCTION PROGRAMMING NEEDED				CONSTRUCTION FUNDING NEEDED		\$ 76,015		\$ 206,080		\$ 293,412	
FUNDING SOURCES & AMOUNTS ESTIMATED AVAILABLE				SB1 \$ 19/20 \$ 297,052		City MOE \$ \$ -					
								TOTAL CONSTRUCTION FUNDING FROM SB1			

Exhibit B



Reviewed by:  City Administrator

- Motion Only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted By: Gwenna MacDonald, City Clerk

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: Design Review Committee membership

PRESENTED BY: Quincy McCourt, Project Manager

SUMMARY: At the April 3, 2019 City Council meeting, staff presented a report for discussion regarding the development of a City theme that represents the core values of the City. The intent is to encourage a community-wide discussion that would identify the priorities of the citizens, with the announcement of a community theme at the Lassen County Fair. This theme would become the basis to establish updated and unified design guidelines that would guide existing and future development within the City, and present Susanville as a healthy, vibrant community. An integral part of this process involves a Design Review Committee which was established in 1991. The Committee is comprised of seven persons, per Susanville Municipal Code Section 12.28.140, with vacancies to be filled by a majority vote of the City Council. The Code does not specify who those seven persons might be, therefore staff is requesting discussion and direction from City Council regarding the composition of the Committee. Options include appointment of representatives from the City Council, Planning Commission, Staff Members, Members from community groups or organizations, and/or members of the general public who have an interest in participating in this process. Staff recommends the appointment by title only, and with Council direction, will solicit letters of interest, with the interested parties being appointed at the Council's May 15, 2019 meeting.

FISCAL IMPACT: None.

ACTION REQUESTED: Motion to appoint membership by title to the Design Review Committee.

ATTACHMENTS: Susanville Municipal Code Title 12, Article IV. Design Review Committee

Susanville Municipal Code[Up](#)[Previous](#)[Next](#)[Main](#)[Collapse](#)[Search](#)[Print](#)[No Frames](#)[Title 12 STREETS, SIDEWALKS AND PUBLIC PLACES](#)[Chapter 12.28 MURALS](#)**Article IV. Design Review Committee****12.28.130 Appointment authority— Responsibilities.**

The mayor with the approval of the city council shall appoint a committee to evaluate, critique, consider and approve the design, painting and location of murals. It is the intent of the city council that this committee also be given the responsibility as the review committee of the following matters covered elsewhere in the Susanville Municipal Code: including but not limited to, architectural review, landscaping, community character, signs. (Prior code § 6-B.40)

12.28.140 Membership—Term—Vacancies.

The committee shall be known as the design review committee and shall consist of seven persons. Each person's term shall last for a period of three years from the date of appointment; vacancies shall be filled by majority vote of the city council. The members of the committee shall be subject to the terms of the Brown Act (Government Code Section 54950 et seq.) (Prior code § 6-B.42)

12.28.150 Meetings.

The design review committee shall meet the second Monday of each and every month, at the hour of four p.m., at City Hall, 66 North Lassen Street, Susanville, California, commencing with the second Monday in May, 1991. The design review committee may meet as often as required should more frequent meetings appear necessary. (Prior code § 6-B.44)

12.28.160 Establishment of guidelines.

The city council may by resolution adopt additional rules and regulations establishing guidelines for the design review committee and setting forth criteria and standards for mural design, painting, execution and location. (Prior code § 6-B.46)

View the [mobile version](#).

Reviewed by:  City Administrator

Motion only
 Public Hearing
 Resolution
 Ordinance
 Information

Submitted by: Quincy McCourt, Project Manager**Action Date:** April 17, 2019**CITY COUNCIL AGENDA ITEM****SUBJECT:** **Resolution No. 19-5626** consider the acceptance of the Paul Bunyan Connectivity Grant to secure state funding for selected infrastructure improvements.**PRESENTED BY:** Quincy McCourt, Project Manager

SUMMARY: In March of 2017, the City of Susanville executed a Sustainable Transportation Project Agreement with the California Department of Transportation (CalTrans) under the Sustainable Communities Grant Program to provide funding for the preparation of program ready designs and a connectivity plan for the northern section of Susanville. Utilizing funding from the Sustainable Communities Grant, city staff were able to conduct community outreach that gauged citizen interest in improving northern Susanville's multi-modal infrastructure. Citizens and stakeholders identified numerous changes to arterial and collector roads that are relatively poor in cyclist- and pedestrian-friendly paths. These proposed changes were submitted to Susanville Public Works for review, where they were narrowed down to options with the most financial and constructible viability. Sustainable Communities Grant funding was also used to hire private consulting engineers to draft concept ready drawings of potential changes. These drawings were returned to city staff for right-of-way review before finally being selected as future projects. The corridors and their priority levels are:

- State Route 139
- Skyline Road
- Paul Bunyan Road
- Radical Road
- Paiute Lane
- Numa / Spring Ridge Road
- Glenn Drive / Parkdale Avenue & Joaquin Street:
- Chestnut Street:
- Cherry Terrace:

Identifying the corridors of interest and the specific improvements to each one led to the writing of the Paul Bunyan Connectivity Report, wherein city staff outline the history of events leading to infrastructure assessment, citizen desire for safety and connectivity enhancements, benefits from such improvements, drawings of proposed changes, and funding sources. By adopting the above nine corridors into the Susanville City Project Layout Plan, the council will signal to city staff that it has approval to pursue new funding opportunities as they present themselves.

Staff is requesting a motion to:

1. Approve the acceptance of the Paul Bunyan Connectivity Final Report.
2. Incorporate selected corridor renovations into the City Project Layout Plan.

FISCAL IMPACT: None.

ACTION REQUESTED: Motion to approve Resolution No. 19-5626 approving the acceptance of the Paul Bunyan Connectivity Final Report and adopting outlined projects into the City Project Layout Plan.

ATTACHMENTS: Resolution No. 19-5626
Paul Bunyan Connectivity Final Report

RESOLUTION NO. 19-5626
A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SUSANVILLE
APPROVING THE SUSTAINABLE COMMUNITIES FINAL REPORT AND
INCLUDING THE NINE PROJECTS INTO THE PROJECT LAYOUT PLAN

WHEREAS, the City of Susanville was successful in receiving a Sustainable Communities grant from CalTrans in the amount of \$111,548; and

WHEREAS, said grant's directive necessitated community outreach, reimbursed staff time on project-relevant tasks, and allowed for drafting conceptual drawings for city infrastructure renovations; and

WHEREAS, a final report has been prepared detailing the top 9 projects at the following locations:

- State Route 139
- Skyline Road
- Paul Bunyan Road
- Radical Road
- Paiute Lane
- Numa / Spring Ridge Road
- Glenn Drive / Parkdale Avenue & Joaquin Street
- Chestnut Street
- Cherry Terrace

NOW THEREFORE, BE IT RESOLVED that the City of Susanville approves the acceptance of the Paul Bunyan Connectivity Final Report and the incorporation of the identified project sites into the City Project Layout Plan; and

APPROVED: _____
Kevin Stafford, Mayor

ATTEST: _____
Gwenna MacDonald, City Clerk

The foregoing Resolution No. 19-5626 was adopted at a regular meeting of the City Council of the City of Susanville, held on the 17th day of April, 2019 by the following vote:

AYES:
NOES:
ABSENT:
ABSTAINING

Gwenna MacDonald, City Clerk

APPROVED AS TO FORM: _____
Jessica Ryan, City Attorney

Paul Bunyan Connectivity Plan

*This project was made possible through CalTrans
Sustainable Communities Grant funding.*



Prepared by the City of Susanville's Grants Task Force
City of Susanville 66 N. Main St., Susanville, CA 96130

PURPOSE and NEED of this REPORT

The purpose of this report is to highlight nine identified projects. This was a direct result of community outreach efforts as outlined by the Sustainable Communities Grant provided by CalTrans. It is our intention to elaborate on the process and methodology and to outline potential funding sources. We will guide the reader through the timeline prepared by CalTrans. The need accomplished with this report is that of providing a layout plan and a strategy to prepare for and apply for project funding. We learned from this project the value of connecting with the humans who live here. Much effort went into discerning the various needs and wants of the community and the result is one that has the support of the people.

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I. PROJECT INITIATION

I.1 Project Kick-Off Meeting

In March 2017 the City of Susanville executed a Sustainable Transportation Project Agreement with the California Department of Transportation (CalTrans) under the Sustainable Communities Grant Program to provide funding for the preparation of program ready designs and connectivity plan for the northern section of Susanville. The project area has been identified, and is referred to herein, as

the Paul Bunyan Connectivity Grant Project area. The project area is bound by Spring Ridge Road and the Susanville Indian Rancheria Housing Authority development on the north, State Route 139 on the east, Chestnut Street on the south and Cherry Terrace, the Susanville Ranch Park and Paiute Lane on the west. Most of the existing roadways within the Project serve as residential streets with some functioning as collectors including State Route 139 (SR 139) along the eastern perimeter; a portion of Chestnut Street along the southerly boundary and Skyline Road which represents the central east-west transportation corridor (Figure 1).

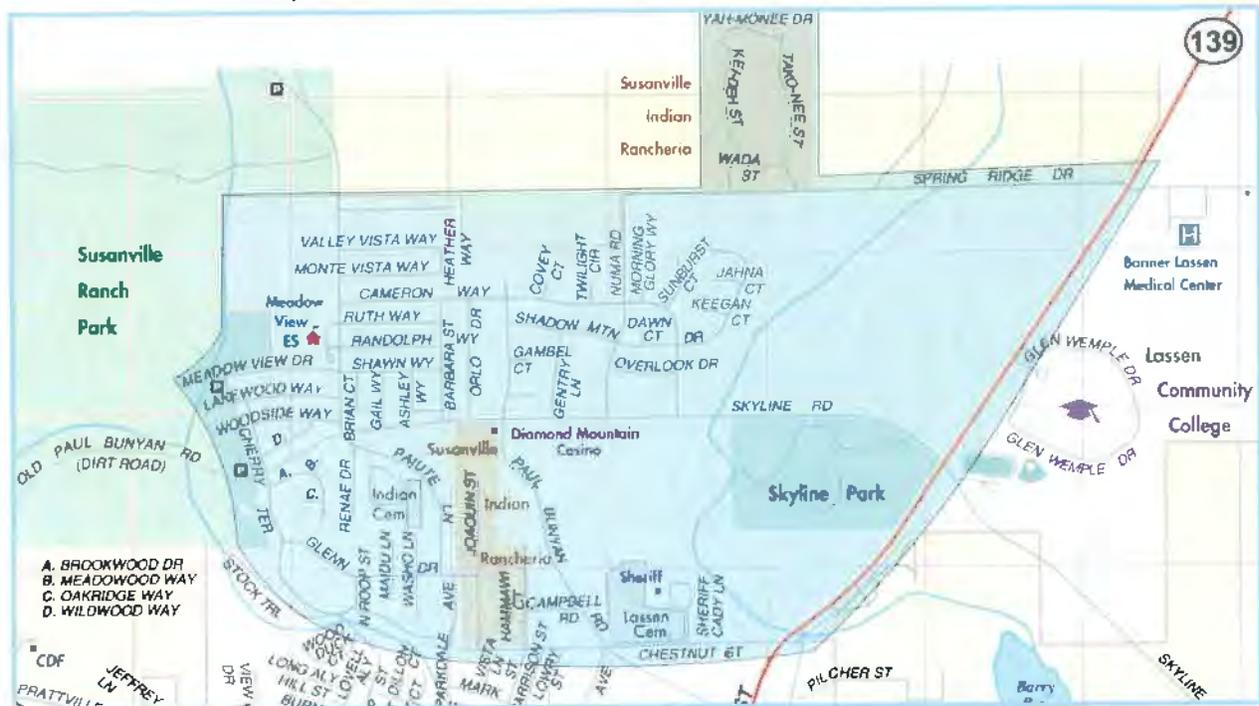


Figure 1

- o Proposed project area
- o Connectivity improvements have been identified within the zone shaded in blue
- o The boundaries were devised by city staff who identified a significant number of residences, public facilities, and private businesses that were and are being served disproportionately poorly by sidewalks and bike lanes.

The Sustainable Communities Grant has allowed Susanville to greatly enhance its ability to open and maintain dialogue with its citizens. Moreover, it has bestowed upon the city a framework for continuing to solicit citizens' opinions for future developments and changes, and for identifying endowments that can fund such projects. The intention of this report is to define project parameters and to outline the series of events that are involved in a successful grant application in a manner that makes the process replicable.

Nine individual locations and corridors were successfully identified as potential project sites (Figure 22). These were absorbed into the city project layout plan, where they can be considered independently for review and renovation in the future.

1.2 Consultant Solicitation

Internal review of the Paul Bunyan Connectivity project pointed to the benefits of hiring an outside consultant to aid in the technical portion of the work required to meet compliance for the Sustainable Communities Grant. Susanville City Hall invited over 30 consulting firms to explore the possibility of partnering over the Paul Bunyan Project but received only two responses. With the permission of Susanville City Council, the City Administrator of Susanville executed a partnership with a design firm known as KASL Consulting Engineers. The scope of services provided by KASL include:

- A presentation of preliminary concept level connectivity plans at a Community Workshop (Susanville City Hall, July 12, 2018)
- Development of Program Ready Concept Connectivity Plans with narrative descriptions, cross sections and aerial photo based exhibits
- Review of funding strategies and grant sources that most closely meet the City's connectivity needs and have the most likely opportunity for successful award
- Preparation of this Project Report which included review and input from City of Susanville Planning and Susanville Public Works staff and from other significant Connectivity Plan stakeholders

What worked well: Susanville's selected consultant produced a scope of work sufficient to address the numerous citizen and stakeholder concerns for infrastructure improvements. Bringing in outside eyes also allowed the city to see what it would do differently in future projects.

What we would do differently: Putting a greater emphasis on the value of project funding, identifying new funding resources, and readying projects. We would likely change the scoring criteria to make price a factor. We would weigh the funding portion of the consultant's contract at roughly 50% of total payment. We would likely grade on experience by asking prospective consultants show 5-10 years of past successes and shortcomings. We would also ask consultants to explain in some detail they intended to include 30% planning projects into the funding of previously outlined, all-encompassing endeavors.

I.3 Stakeholder Coordination

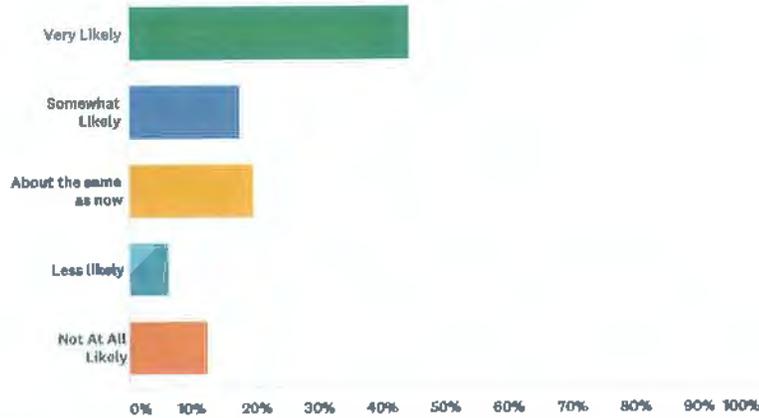
In order to gauge whether there existed a substantial amount of support for applying for a Sustainable Communities Grant, city staff reached out to stakeholders: those who might have a vested interest in improving infrastructure greater than that of the average user. Among the parties to whom the city reached out were:

- Lassen County representatives
- CalTrans
- MeadowView Elementary School
- Lassen Community College
- Susanville Indian Rancheria
- Diamond Mountain Casino
- Susanville School District
- City of Susanville Police Department
- City of Susanville Fire Department
- Susanville Public Works
- Susanville Area Bicycle Association

Many of the entities listed above drafted letters to voice their support for Paul Bunyan Connectivity improvement (Figure 4). Lassen Community College went as far as surveying students and staff about their commuting preferences and how supportive they would be of potential changes to infrastructure (Figure 3).

Q4 If a safe bicycle and walking path existed on Ash St/Hwy 139, all the way to the Lassen Community College campus, how much more likely would you be to bike or walk to campus?

Answered: 97 Skipped: 0



ANSWER CHOICES	RESPONSES	
Very Likely	44.33%	43
Somewhat Likely	17.53%	17
About the same as now	19.58%	19
Less likely	6.19%	6
Not At All Likely	12.37%	12
TOTAL		97

Figure 3

- o Sample page of questionnaire issued to Lassen Community College students and staff
- o In this question, students and staff were asked to respond with the likelihood that they, personally, would use a bicycle/walking path to commute to class.
- o Nearly half of surveyed personnel responded that they would be very likely to walk or bike to class. Only about 19% indicated that they would still be unlikely to bike or walk to class. If we extrapolate these numbers to the entire student body and all staff, we can realistically estimate several hundred adults biking or walking to and from class along State Route 139 on most days of the week.



Lassen Community College District Office of the Superintendent/President

February 27, 2019

To Whom It May Concern,

Lassen Community College fully supports the City of Susanville's application for the Cal Trans ATP Cycle 5 grant which will improve not only our college but our entire community. There are several specific reasons we are a full partner with the City. For one, we currently do not have a safe walking or biking path that reaches our campus from Main Street. Most of our students and many staff who live in town, reside where the proposed path would start thus allowing them to utilize carbon free modes of transportation (biking, walking, skateboarding) to get to classes at the college campus. Currently students and staff choose not to use one of these methods because the sidewalk ends, the road lacks a bike path and the speed limit increases to 55 mph, thus making it a treacherous stretch of road. In addition, by providing a bike/walk access to campus we would be able to increase our fitness course offerings for the community as we would have a safe route for a walking/running course that could start right from our college campus. Third, the proposed route would also increase the possibility of fitness minded events. Our nursing students put on a walk a thon fundraiser each year and could include the path in the route. New events similar to a walk a thon have the potential to draw individuals from outside of the area for bike races, marathons, triathlons, centuries and the like. These events would in turn generate revenue by our local businesses and help to spur a more robust economy in Lassen County. We hope you will fund this project to support our community and our college for the benefit of our staff and students.

Sincerely,

A handwritten signature in blue ink that reads "Dr. Hall".

Dr. Marlon Hall
Superintendent/President

P.O. Box 3000 Susanville, California 96130-3000 (530) 251-8820 FAX: (530) 251-8872

Figure 4

- o Letter of approval from Dr. Marlon Hall, LCC superintendent and president.
- o Dr. Hall graciously reaffirmed LCC's partnership with the city of Susanville and verbalized the benefits of a bike lane along SR 139.
- o Dr. Hall touches on perks such as service to a high number of students and staff, reduced carbon emissions, the inclusion of this trail in fitness-oriented courses and events, and corresponding economic stimulation such increased traffic would bring.

What worked well: Emailing preliminary project information, explaining the goals of said project, following up with phone calls, and answering any and all questions posed by stakeholders generated a high level of enthusiasm for project objectives. By informing stakeholders that the CalTrans Sustainable Communities grant was encouraging city staff to conduct outreach and consequently soliciting feedback from enthused stakeholders, we were able to communicate the value of stakeholder feedback and inform stakeholders of the weight their opinions carry.

What we would do differently: Stakeholder feedback proved to be vastly more valuable than the city anticipated. We would specifically ask stakeholders if there were any questions we were neglecting to ask them, and if there were any other potentially interested stakeholders that evaded our notice. Stakeholders referring other stakeholders to this project greatly enhanced the volume and quality of input.

I.4 Identifying Existing Conditions

I.4.1 California's Geographic Crossroads

Susanville lies at several different crossroads. Economically, its position at the foot of the mountains allowed it, historically, to support a rich timber and mining industry, while the land east was and still is a local breadbasket well-suited for ranching and growing grains that are both used locally and exported. In more recent years, the lumber mill situated in town closed, but Susanville also saw a simultaneous rise in the number of firefighters and prison workers taking residence. Even with these industries combined, making up a substantial portion of the workforce, there are still numerous people employed in healthcare, business, education, the sciences, and myriad other fields. This makes the community one of the more heterogeneous in the state, although it is still viewed by many as composed mostly of government workers and the unemployed.

This city also sits at a nexus of several different biomes: alpine to the west and south, riparian along a narrow corridor running through town, and high sage desert to the north and east. The climate is subalpine, characterized by high temperatures and low rainfall in the summer, and punctuated cloudiness and snow in the winter. The distinctness of these conditions allows all outdoors enthusiasts, from the novice to the expert, to take advantage of the numerous opportunities, be they hiking, biking, swimming, fishing, disc golf, hunting, skiing, snowmobiling, dirtbiking, or a suite of other activities. The city, which serves as a launching pad for said recreation, has a quaintness that can itself be considered an attraction by pedestrians and cyclists, with its shady groves, shops, and historical sites.

Susanville also serves as a gateway for through travelers. Lying east of the Sierra/Cascade crest, it acts as a de facto bottleneck for those passing between Reno, NV and destinations in southern Oregon and the northern California coast. In an ideal scenario, many of these travelers might opt to stay in Susanville for a spell and patronize businesses while absorbing the scenic beauty. As it

stands, however, community feedback gathered as a result of the Sustainable Communities grant points to the city being less welcoming than it could be. Closed businesses line streets that have insufficient lighting with inconsistent sidewalks and bike lanes. The current signage could do a better job of directing people to the attractions located both in town and out. And if people begin to follow bike or walking paths to destinations on the city's outskirts, they can expect a gradual deterioration of multi-modal conditions until they are walking on a road shoulder or a poorly used dirt road.

Because of its location and access to attractions, Susanville has the potential to act both as a waystation for those looking to temporarily escape urban life, and a pleasant small-town community with ready access to numerous amenities. As it stands, however, the city is hampered by shortcomings in infrastructure that frustrate travel between town and its attractions. The inadequate street lighting fails to provide a feeling of safety to those who wish to travel after dark, and the multi-modal travel options range from confusing to borderline dangerous. The end result is a community with many proximal sights and experiences that are being largely underutilized because of a simple lack of options allowing people to get from point A to point B.

1.4.2 Population

Susanville is 6.04 square miles with a density of 2,971 people per square mile. As of July 1, 2017, the population of Susanville was estimated to be 15,326 (Census.gov, n.d.). This represents a loss of more than 2,617 people, or 14.6%, since 2010 (Census.gov, n.d.) It is estimated that 14% of the population are minors and 7% are senior citizens. Bicycle and pedestrian travel offer an inexpensive transportation substitute for families with limited monetary resources, consequently liberating money for goods and services.

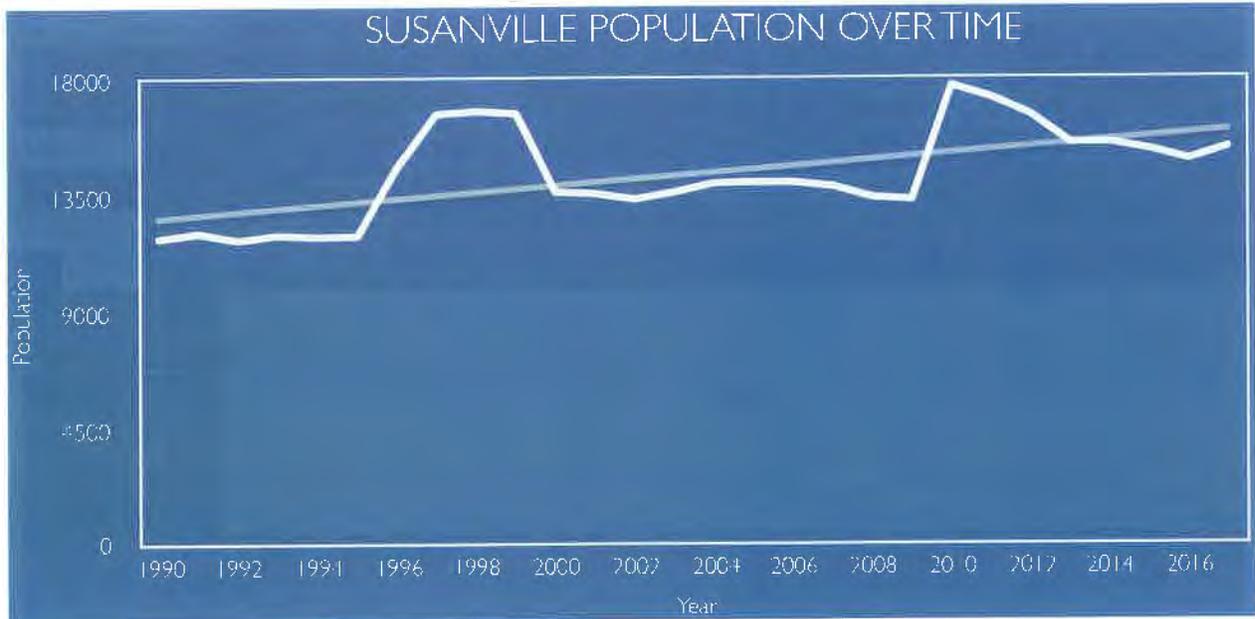


Figure 5

- Susanville Population tracked over time (U.S. Census Bureau.)
- Susanville's population has been gradually trending upwards since data collection began.
- This trend suggests that Susanville has room to grow. With ample space, jobs, and natural getaways, Susanville has resources to support more people than it currently has, so it is reasonable to expect steady growth. Higher numbers of people will put a greater strain on roads, so enhancing multi-modal infrastructure now will help proactively address a maintenance issue well in advance.

Susanville's population has been trending upwards for the last 30 years. Its abrupt rises and dips are largely due to shifts in incarceration rates at the prisons near town and the corresponding workforce and prisoner changes.

1.4.3 Economics

Nearly all of Susanville falls under the legal definition of "disadvantaged community," with a significant proportion of that qualifying as "severely disadvantaged." Those in poverty make up 22.8% of the total population. As of 2016, home ownership stood at 48.3%, which substantially trailed the greater Lassen County and the United States at 64.6% and 63.6%, respectively (datausa.io, 2016). In this same year, the number of households that had no access to cars or only one car stood at 27.5%. It can be inferred that many of these inhabitants are doing a significant amount of walking and biking on roads, or using public transportation to reach farther destinations.

1.4.4 Commute to Work

The average commute to work for Susanville's workforce is 16.8 minutes (Census.gov, n.d.), which is noticeably lower than the nationwide average of 26.4 minutes. When we factor in the substantial prison workforce who commute roughly 44 minutes to the Federal Correctional Institution or 16 minutes to drive to High Desert State Prison or California Correctional Center, the commute time for individuals who both live and work in town drops even further. And yet, a full 73% of workers

commute by personal vehicle, eschewing such modes of transport as walking, biking, or bus. With the majority of destinations requiring no more than two miles of travel, it is perplexing that so many who could very feasibly walk or bike would instead choose to drive. Increasing the number of commuters who bike and walk to work is an attainable goal for Susanville. With its density of traversable arterial roads in town and surplus of collector and local roads available roads leading to every corner of the city, Susanville is primed to encourage such travel to those who might otherwise opt to drive. In order to facilitate such a change, the routes by which those commuters travel should be upgraded.



Figure 6

- o Susanville mural depicting Isaac and Susan Roop, the founders of Susanville.
- o Susanville's Lassen County Arts Council upholds a proud tradition of honoring the historical figures who formed Susanville and the people and events that made it what it is.
- o Susanville and Lassen County feature a history rich in railroad culture, early timber harvesting, pioneer lifestyle, and independence from outside authority. Many of the people instrumental in the founding of various county institutions bore their namesakes to the mountains, lakes, and rivers that dot the area. History enthusiasts may explore the various events that color Susanville's past at the museums that catalog its history. Even the Bizz Johnson trail system, which connects Susanville to Westwood, CA, is itself a disused railroad track where hikers and bikers can view the engineering that supported the timber and mining industries in burgeoning mountain towns.

1.4.5 Health Concerns

Susanville is the only incorporated city in Lassen County, which faces several disconcerting health crises. The obesity prevalence is 23.9% and the diabetes prevalence is 7.3% (datausa.io, 2016).

While these statistics may not raise alarm, it is worth noting that healthcare coverage hovers around 13% prevalence. Safer, more efficient multi-modal transportation options presented to the Susanville community could help steer these numbers in a healthier, safer direction. Upgrading and diversifying the infrastructure in Susanville could ameliorate deficiencies not only in inadequate transportation routes, but also in health and safety shortcomings as well.

1.4.6 Current State of Roads

One glaring deficiency in traveling around Susanville is in the dearth of bicycle-specific markings on roads. Traffic lanes are shared, meaning that cyclists may legally share the right of way with motor vehicles regardless of the lack of explicit signage or pavement markings. However, sharing a lane tends to be awkward for all travelers involved. Drivers are often forced to slow down or come to sudden halts for cyclists they approach, often on significant inclines or declines. Cyclists are forced to put their trust in drivers to avoid accidents, increasing their stress. Compounding the matter is the fact that Susanville does not present usable paths specifically for cyclists. This forces riders to use a combination of sidewalks and roads. The sidewalks in and around town are inconsistent; they are degraded and may continue for several blocks before ending abruptly, forcing cyclists to stop or immediately veer into the road in order to continue.



Figure 7

- 4th Street near McKinley Elementary School.

- Conditions shown are representative of a typical winter day in Susanville. Children commute home in the mid-afternoon. They may walk on the sidewalk if it is available, but they routinely play by running into the road to grab snow to throw at each other. There are electronic speed signs in the area, but they are frequently disobeyed.
- The area pictured is the site of an accident that occurred near Hall St. and Fourth St. in 2012. A cyclist was struck by a car, causing the cyclist a visible injury. The speed limit is 30 mph on this road, and 15 within the school zone. If people here are at risk of being struck by cars even when the speed limit is fairly low, it would make sense to give pedestrians and cyclists a separate path.

The roads are not much safer. Some roads have shoulders, which cyclists may opt to use if they are free of parked cars and snow drifts, but this is often not the case. Cyclists who can't pedal on the shoulder are left with one option; ride in a motorist lane, where they are subject to the whims and safety of drivers. While this sharing works the majority of the time, it increases the risk of a collision and the stress on both cyclists and drivers (Figure 8).

Mixed Traffic Criteria		Prevailing Speed						
Number of lanes	Effective ADT*	< 20 mph	25 mph	30 mph	35 mph	40 mph	45 mph	50+ mph
Unlaned 2-way street (no centerline)	0-750			LTS 2	LTS 2	LTS 3	LTS 3	LTS 3
	751-1500			LTS 2	LTS 3	LTS 4	LTS 4	LTS 4
	1501-3000	LTS 2	LTS 2	LTS 2	LTS 3	LTS 4	LTS 4	LTS 4
	3000+	LTS 2	LTS 3	LTS 3	LTS 3	LTS 4	LTS 4	LTS 4
1 thru lane per direction (1-way, 1-lane street or 2-way street with centerline)	0-750			LTS 2	LTS 2	LTS 3	LTS 3	LTS 3
	751-1500	LTS 2	LTS 2	LTS 2	LTS 3	LTS 4	LTS 4	LTS 4
	1501-3000	LTS 2	LTS 3	LTS 3	LTS 3	LTS 4	LTS 4	LTS 4
	3000+	LTS 2	LTS 3	LTS 3	LTS 3	LTS 4	LTS 4	LTS 4
2 thru lanes per direction	0-8000	LTS 3	LTS 3	LTS 3	LTS 3	LTS 4	LTS 4	LTS 4
	8001+	LTS 3	LTS 3	LTS 4				
3+ thru lanes per direction	any ADT	LTS 3	LTS 3	LTS 4				

* Effective ADT = ADT for two-way roads, Effective ADT = 1.5*ADT for one-way roads

Figure 8

- CalTrans designations of traffic stress incurred by bicycle riders relative to road type and speed limit.
- Dark Green highlights indicate the lowest stress score while red highlights indicate the most stressful.
- Many of the arterials in Susanville's project area are 1 thru-lane per direction with high speed limits, consistently subjecting riders to LTS 3 and LTS 4.

CalTrans estimates that cyclists who are strong and fearless account for 7% of all riders, enthused and confident make up 5%, interested but concerned make up 51%, and those who refuse to ride fill out the remaining 37%. Providing potential cyclists with safer, less stressful path options could help nudge those individuals in Categories 3 and 4 into the class of riders who are confident and fearless in their ability to safely navigate traffic.

The pedestrian realm in Susanville, while adequate at a glance, can be deceptively inhospitable. Sidewalks exist in certain areas, and while they serve their purpose fairly effectively, there are few other amenities that might convince pedestrians to walk or bike these routes, or to spend significant amounts of time at locations other than their start and end. Street lamps exist in this area, but at an interval and luminosity low enough to disguise danger in the form of criminal activity or careless motorists. The outcome is a network that fails to properly illuminate pedestrians in the

dark, making those who do walk at this time incur greater risk. The situation is exacerbated off Main Street on side roads, where there is less traffic but sidewalks are largely absent and security lighting is infrequent.

Another factor that discourages pedestrian and cyclist activity is the lack of comfortable amenities. If someone is determined to walk from their start point to a destination in Susanville, they may technically do so without impediment. However, they will be on their feet for the duration of said walk and they will not have protection from the elements. There are limited benches to rest at with little to no trees to provide shade along many of the roads in and around Susanville. A walker who might wish to peruse shops will have nowhere to sit if they get tired. Compounding this is the lack of shade available to pedestrians. Susanville experiences a great number of sunny days throughout the year, with the temperature regularly climbing to 100° in the summer. Clouds are rare on sunny days and in addition to the direct radiant energy a pedestrian might feel from the sun, the convective current generated by asphalt adds to the heat. The end result is that a pedestrian walking near asphalt in the summer will be subject to uncomfortable heat, which only increases with the act of walking. A reasonable pedestrian might plan their walk to incorporate some rest on a bench or under a tree's shade, but few such refugia exist in Susanville. As a result, we see people who might otherwise opt to walk to their destinations instead drive and do minimal walking once there. We believe that, besides being aesthetically pleasing, trees and benches at choice locations along paths could encourage more walking and subsequently improve local air quality, safety, personal health, and commerce in Susanville.

Cyclists could also benefit from improved amenities in the form of bicycle racks near select destinations. Susanville is prone to theft, and cyclists who wish to travel most routes will not be able to leave their bikes and explore shops or parks if a proper anchor is not provided. Adding bicycle racks to commercial centers or parks could have the effect of allowing cyclists to turn a straightforward bicycle outing from point A to B into one that stops at numerous destinations.

In October 2015, a sidewalk inventory detailing the completion status and presence vs. absence of sidewalks in Susanville was completed in an effort to analyze the distribution of different sidewalk types. The picture generated reveals a network of routes with the potential for connectivity, but lacking consistency. As Figure 9 shows, Main Street exhibits the most robust sidewalk corridor in town, but the area north sees a dissipation of complete sidewalks, instead displaying a network of roads with some complete sidewalks, but frequently punctuated by spans without them. Going farther north into the project area, we see continuous stretches along SR 139, Chestnut Street, Paul Bunyan Road, Skyline Road, Spring Ridge Road, and Numa Road that lack sidewalks altogether. People who travel these roads are forced to compete with motorist traffic, and many who know of the lack of sidewalks simply choose to drive.

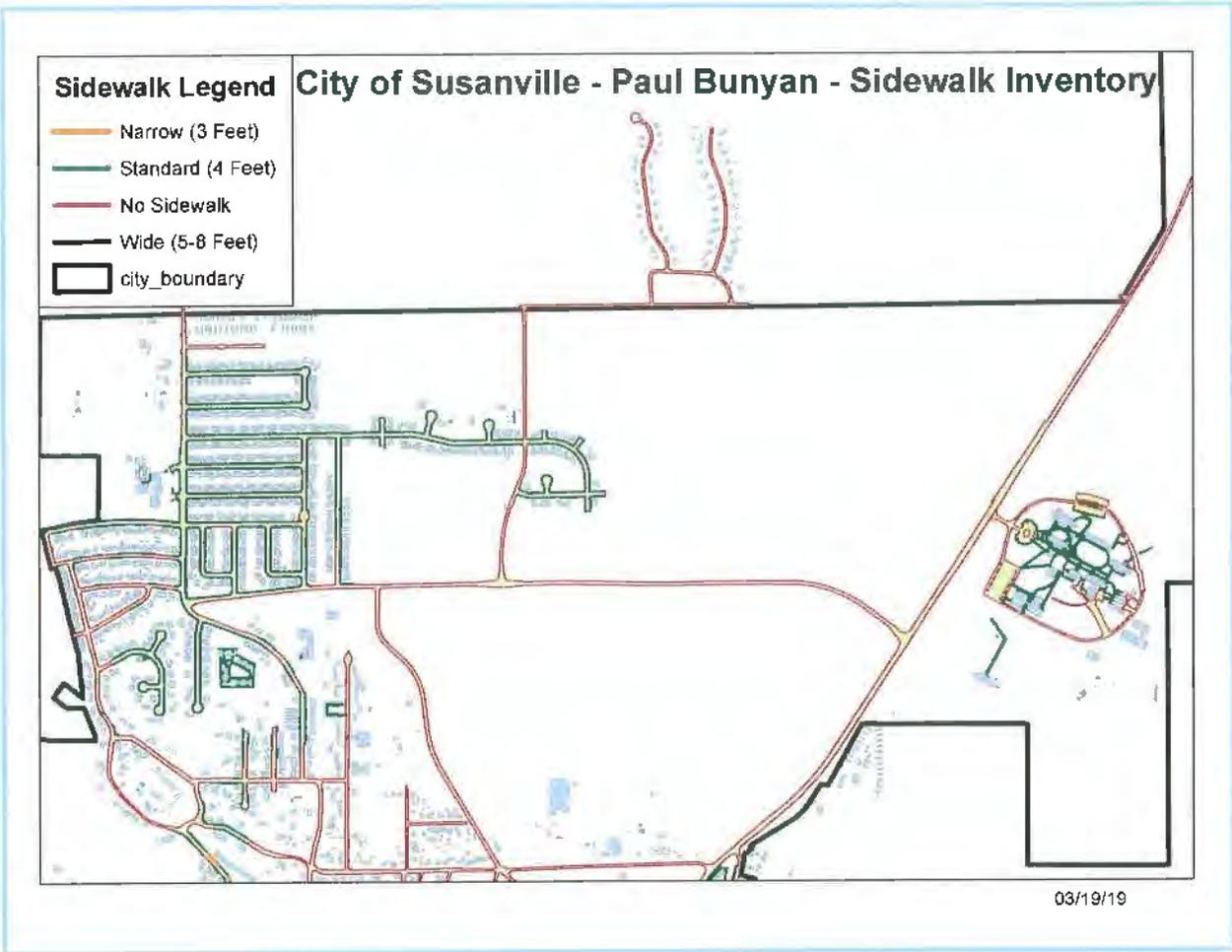


Figure 9

- o 2015 sidewalk inventory conducted by Susanville Public Works.
- o Red lines indicate roads where sidewalks are absent, green where they are present and standard width. Yellow indicates narrow sidewalks.
- o The patchwork of green and red in the project area paints a picture of roads that might have several hundred feet of sidewalk before losing them for no good reason. We also see the long, uninterrupted red lines on the arterials Spring Ridge Road, State Route 139, Numa Road, Chestnut Street, and Paul Bunyan Road. Pedestrians and cyclists travel these roads at their own risk.

State Route 139 has one through-lane per direction (that is, a 2-way street with a centerline.) Assuming a 4' or 5' width for the current lanes used for biking, the score of traffic stress experienced by cyclists, as estimated by CalTrans criteria, is between 2 and 4 along SR 139, with 4 being the highest amount of stress possible. Plainly, a cyclist biking along the right shoulder of SR 139 north of Chestnut Street can expect numerous vehicles coming from behind that routinely exceed 60 mph, while the cyclist plods along at 10 mph, give or take. This speed disparity, coupled with the size disparity between a cyclist and a motorist, confers undue stress on the individual (Figure 8). The outlined project may help remedy that stress.

In order to quantify the threat of pedestrian and cyclist accidents in Susanville, UC Berkeley's Transportation Injury Mapping System (TIMS) was used to generate maps (Figure 10) and statistics

(Figure 11) based around Main Street and the proposed project area. Between 2006 and 2017, 34 collisions between motorists and pedestrians were recorded in the area searched. This number includes two severe injuries and one fatality. While TIMS does not include narratives that might illuminate the exact circumstances that may have led to each crash, this data indicates that Susanville is not immune to traffic accidents and that they pose a serious threat to pedestrians and cyclists who use the roads. This danger manifests itself in either deterring people from biking or walking, or elevating the risk of bodily harm for those who do. Those who make a habit out of driving where they could walk may fall into a pattern of relative inactivity wherein overall health worsens, and increases strain on the city's medical facilities. The severity of these effects could be alleviated by providing people with a connectivity network that allows free travel on paths to insulate active transport users from traffic. Class II bike lanes on streets could serve as a viable, cost-effective solution to this situation. Assigning a dedicated lane to cyclists amidst traffic could effectively delineate the right of way between motorists and cyclists, while allowing free rein of the sidewalks for pedestrians.

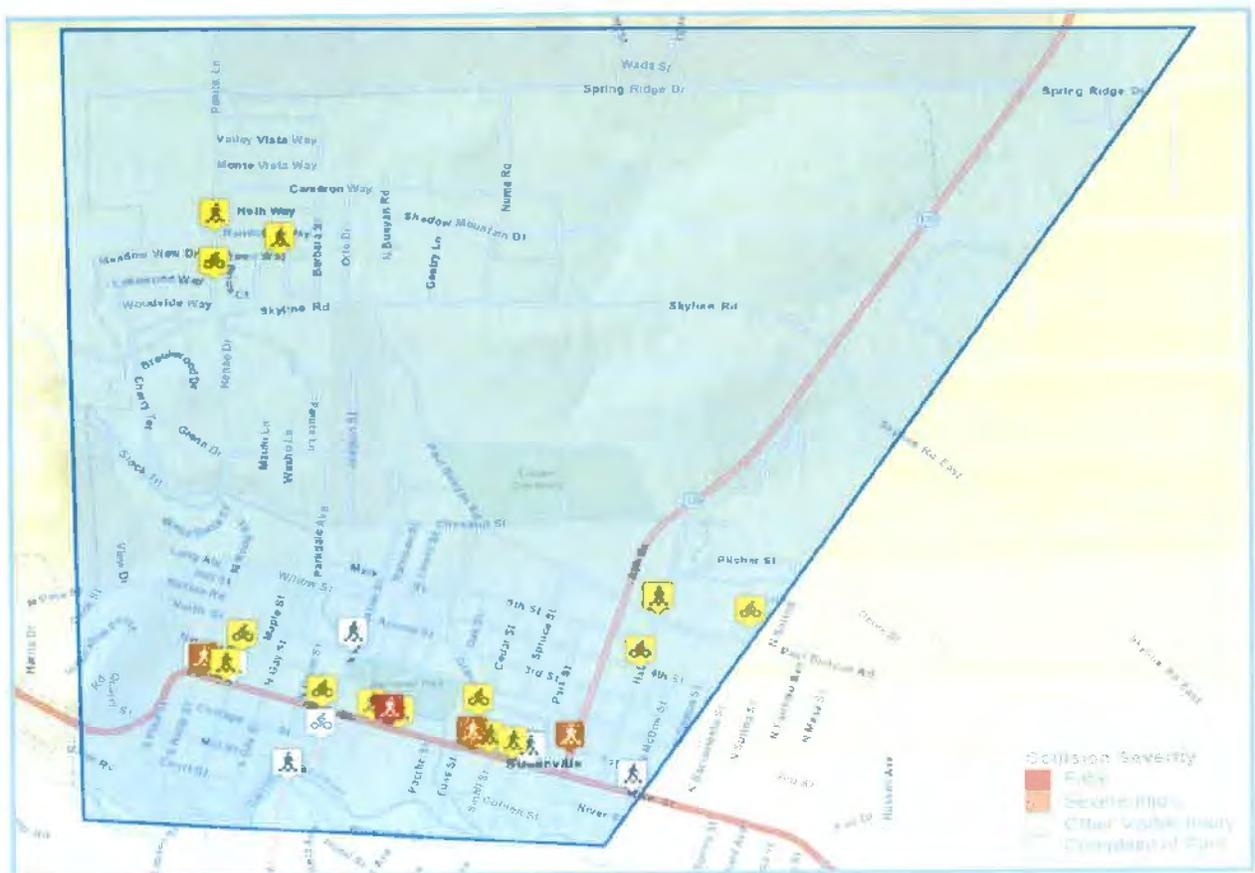


Figure 10

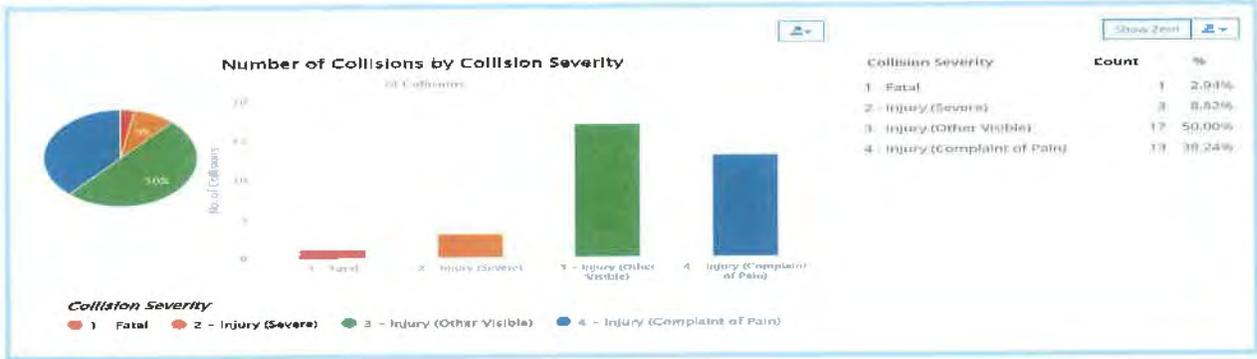


Figure 11

- Records and locations of collisions between cars and either pedestrians or cyclists in project area and Main Street between 2006 and 2017.
- Map shows accidents around the project area. Accidents occur on arterials, collector roads, and Main Street, which is not in the project area, but is so vital to life in Susanville that it draws people from the project area on a daily basis
- We see a number of pedestrian collisions along Main Street. This is to be expected because Main Street hosts approximately half of all businesses in Susanville. North of Main Street we see more accidents occurring on collector roads, particularly near Meadow View School in the north and McKinley Elementary in the east. This tells us that low speed limits are not enough to sufficiently protect children from oncoming cars.

Many areas of Susanville could be retrofitted with Class I bike lanes in various areas where sacrificing road width might not be a viable option. One prime example of this, for which the city did extensive research, is the State Route 139 corridor from Main Street to Spring Ridge Road. Once alerted to the existence of ATP grants capable of funding multi-modal infrastructure improvements in designated areas, the City of Susanville initiated an application process to request funding for Class I and II bike lanes leading from Main Street to Spring Ridge Road along State Route 139.

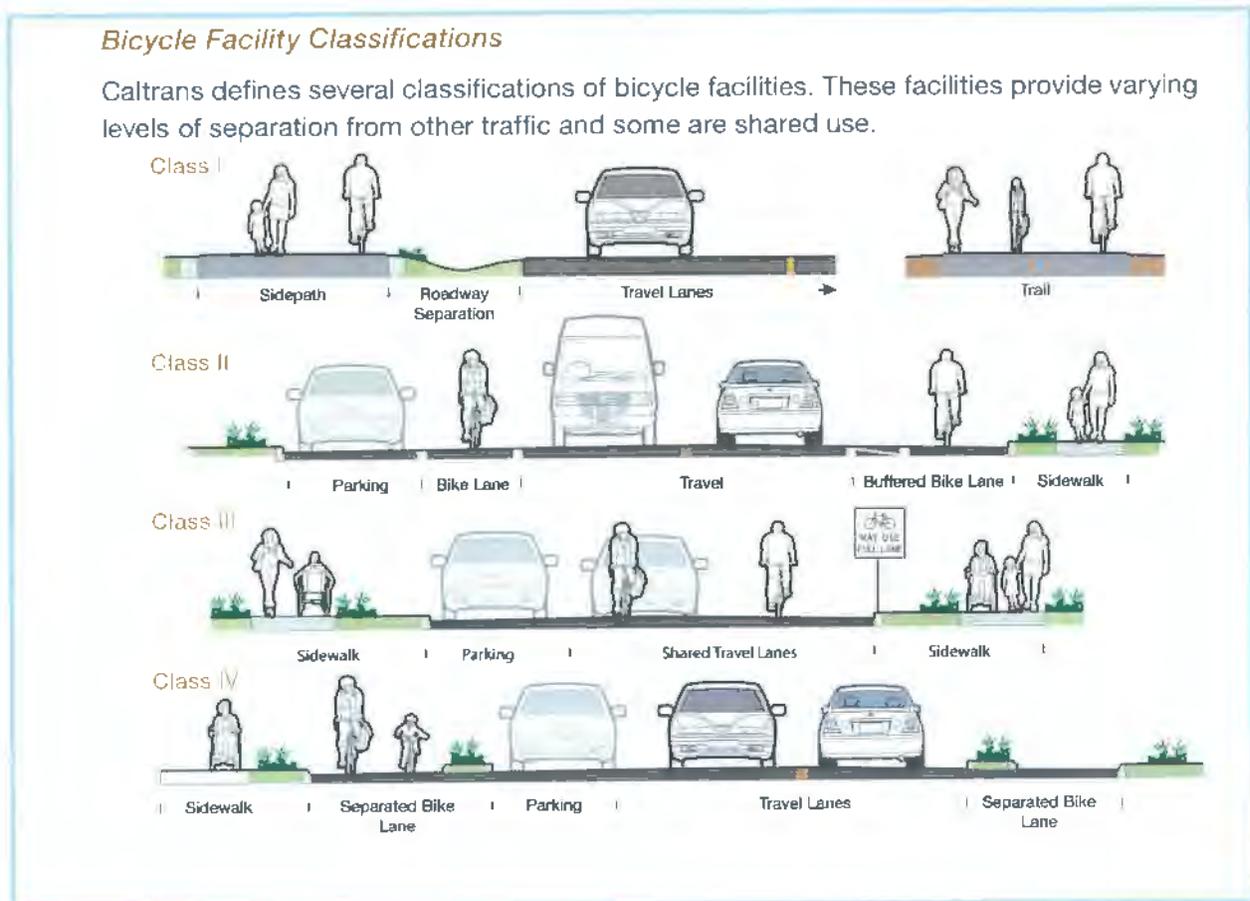


Figure 12

- o Figure depicting bike lane classes (CalTrans, n.d.)
- o The majority of roads in Susanville are LTS 4, meaning that cyclists must share the road with drivers.
- o This is the only bicycle path that forces cyclists to share a lane with traffic. It significantly increases the risk of an accident.

When staff conducted surveys of SR 139, they amassed 28.5 hours of observational point data spread out over eight days. During this time, they tallied 7,723 automobiles and other classes of vehicle driving on the road in question. Survey times ranged from 10 am to 7 pm, so while it would be unfair to extrapolate the total number of automobiles over the course of one calendar day using this data, it roughly equates to 184 cars per hour during daylight hours. Of the number of cars passing over the elapsed survey time, 37 were semi-trucks, 11 were ambulances, 16 were

police cars, and five were fire engines. The survey also tallied 110 walkers and 31 bikers during this time. Using these numbers, we were able to deduce that, during daylight hours, the average number of vehicles passing our survey points per hour was 277. Of that number, two vehicles were emergency response (fire, police, ambulance,) two were public transportation (private or public bus,) and 273 were private citizen vehicles. For comparison, the average number of walkers and cyclists passing our survey points per hour was five. If we figure that all vehicles pass all pedestrians during this time, we can estimate that there are 1,385 vehicle/pedestrian encounters per hour of sampled time. This native surveying would not have been performed without the support of the Sustainable Communities Grant.

STAFF SURVEY OF STATE ROUTE 139

Time (hours)	Single Driver	Carpools	Walkers	School Buses	Public Buses	Semi Trucks	Cyclists	Ambulance	Police	Fire
28.5	5,252	2,471	110	15	46	37	31	11	16	5

Table 1. Sums and distributions of State Route 139 travelers based on transportation method. Sums are aggregates of different surveys occurring at randomized times and locations along SR 139.

One of the unintended positive side effects of the Sustainable Communities Grant and the accompanying community outreach was the freedom it gave city staff to familiarize itself with legal code. Susanville's City Building Division came across a little known building code that heretofore was not being enforced within the city. The code specifically calls for private businesses that provide 10 or more parking spaces to also provide bicycle parking for 5% of prospective visitors or tenants. Ultimately, the Sustainable Communities Grant allowed Susanville to provide biking amenities by enforcing preexisting laws at no additional cost.

What worked well: Stakeholder assistance in identifying areas of opportunity proved very beneficial. Setting up standardized pneumatic traffic counts at these locations allowed Susanville to acquire a data set sufficient to accurately describe traffic patterns at distinct sites. We were also able to assign dedicated staff to personal traffic counts that could quantify vehicle types and multi-modal transport at these locations.

What we would do differently: Coordinating with Susanville Public Works to generate a weekly rotation of pneumatic traffic count collectors for the entire duration of the project would have yielded an even stronger sample size for extrapolating traffic patterns. Generating this data is so crucial that city staff intend to immediately proceed with this plan in the likely event that it will be pertinent to similar future projects.

1.5 Active Transportation Destinations

In order to narrow the scope of work for enhancing bicycle and pedestrian connections, city staff went about identifying those destinations that were deemed attractive to residents and would theoretically see disproportionately higher levels of pedestrian and cyclist traffic. In addition to public facilities like parks, social service centers, and employment centers, staff also identified commercial, educational, recreational, and medical institutions that could potentially benefit from more interconnected multi-modal infrastructure.

1.5.1 Public facilities



Figure 13

- Figure denoting places of interest within the project area.
- Blue=park. Yellow=educational center. Red=medical center. Orange=Susanville Indian Rancheria housing. The yellow line is State Route 139 from Spring Ridge Road on the north to Main Street on the south.
- This figure displays how dispersed public attractions are in Susanville. The hospital and college are far enough from town that almost anyone trying to visit either one of them will travel on SR 139, which features only a shoulder for walking and biking.

The proposed project sites pass within a stone's throw of numerous public and private entities that serve multitudes of Susanville residents annually.

- Skyline Park, located in the eastern section of the project area, will serve as the future site of a disc golf course. The rolling, desert hills can support 30 holes for all ages. The city expects this coming attraction to draw in numerous citizens and even those outside of the city.
- Susanville Ranch Park, which abuts the western edge of the project area, acts as Susanville's gateway to forest. Walkers and cyclists can follow a network of trails from the entrance deep into Lassen National Forest.
- Susanville's W.I.C. office in the southern project area serves the city's low-income residents by administering nutrition assistance and providing food security for those who might have trouble obtaining it otherwise.
- Susanville's Public Health Department, which promotes wellness through education and prevention, is located in the southern project area.
- Lassen Career Network seeks to find gainful employment for citizens. It is located in the southern project area.
- Lassen County Sheriff's headquarters, along with their juvenile and adult detention facilities, are located in the southern project area.

1.5.2 Medical Facilities

- Banner Lassen Medical Center and Northeastern Rural Health Clinic both sit at the far northeastern corner of the project area. They field thousands of calls annually for medical issues ranging from the chronic to the urgent. From Susanville, people must travel up SR 139 to reach either one of them, and this road features only a narrow shoulder for pedestrians and cyclists.
- Banner Lassen Medical Center Lab Draw Station and Banner General and Orthopedic Surgery Centers both reside in the southern portion of the project area.
- Offices of Dr. Marason, one of Susanville's optometrists, are located in the southern project area.

1.5.3 Educational Centers

- Lassen Community College, the county's only higher education entity, sits at the northeastern project boundary.

- Meadow View Elementary School serves students in Susanville's northwest. The school lies in a residential area, and students that live within a certain radius of the school are expected to walk or bike to and from school rather than taking the bus.
- Diamond View Elementary School, just outside the project area, nevertheless hosts hundreds of students that commute to and from school via major arterials that make up the project area.

1.5.4 Commercial Centers

- Diamond Mountain Casino and Resort serves as the project area's chief economic hub. It draws tens of thousands of visitors per year from within the county and beyond it.
- Other commercial entities located in the project area that would be served by better multi-modal infrastructure include Sezzi Concrete and Materials, Inc., Millview Apartments, Frosty Mill Hamburgers, Eagle Lake Village Senior Living, Susanville Garden Apartments, and a multitude of others.

1.6 Active Transportation Impediments

1.6.1 Motorist-dominated Streets

Many of Susanville's streets span wide curb-to-curb distances while accommodating only two lanes for motorists. This alters the perception of safety for both motorists and pedestrians. Pedestrians are forced to walk longer distances to cross streets, often passing behind parked cars and out of view of active motorists. Motorists may be less inclined to slow down on such wide avenues.

1.6.2 Inconsistent Multi-modal Network

Pedestrian and cyclist infrastructure in any given area of Susanville ranges from well-formed to entirely absent, often alternating between the two. This arrangement can be credited to highly localized and temporally periodic improvements in sidewalk repair that yield an inconsistent end product. The system has the effect of luring pedestrians and cyclists onto otherwise nice shoulders or sidewalks before funneling them into traffic or off the road entirely due to abrupt cessation of sidewalk or shoulder. This lack of continuity puts multi-modal travelers at risk while also encouraging those familiar with the system to consider different modes of transportation altogether.

1.7 Stakeholder Meetings

In addition to the community at large, Susanville also went about consulting stakeholders that might have a vested interest in such connectivity projects. While suggestions for changes within the project area varied, it became clear that each of the identified stakeholders had ideas on how improvements could positively impact denizens. Many of the suggested improvements had to be discarded because they didn't serve as many people directly and weren't prioritized collectively. However, an overarching theme shared by most was the desire for major arterials to increase their capacity for pedestrian and cyclist traffic.

Some of the key stakeholders and their specific concerns are listed below.

• **Meadow View Elementary School** - Meadow View Elementary School (Figure 14) sits at the northwest corner of town on Paiute Lane, one of the corridors of interest for this project. Many students commute to and from this school on Skyline Road, Paul Bunyan Logging Road, and Cherry Terrace, which are also singled out for renovation. Meadow View was kind enough to grant the city some statistics on student commuting trends. They estimated that 150 to 160 students out of 364 take the bus to school, which means that more than half of the school walks, bikes, or rides in private vehicles. The immediate consequence of this is a semi-daily convergence of children and numerous vehicles at the school, which heightens the risk of a collision.



Figure 14

- Entrance to Meadow View School
- Picture shows where students walk when commuting to and from school.
- A pedestrian was struck by an automobile here in 2014. Given how many students commute to and from this school and must compete with private vehicles picking up and dropping off kids, it is perhaps not surprising that this area may be prone to accidents.

• **Diamond Mountain Casino** – Diamond Mountain Casino (Figure 15) serves as the economic epicenter of the proposed project area, offering employment, retail, and recreation. Its major connectors are Skyline Road, Paul Bunyan Road, and Cherry Terrace. Its physical separation from Main Street and Susanville's population center mean that people living near the casino may

walk, bike, or drive, depending on distance and willingness. A disproportionately high number of drunk driving instances occur near this business, elevating the risk of an accident.



Figure 15

- Entrance to Diamond Mountain Casino and Resort
- This picture serves as a common example of how sidewalks may exist near a destination but immediately disappear.
- This Casino is relatively distant from the weighted population center of Susanville, yet relies on citizens near that center for its livelihood. This means that most people visiting the casino will take Paul Buryan Road (pictured) to reach it. If one chooses to walk or bike this route, they will be doing so on a shoulder that is routinely wet and dirty.

• **Susanville Indian Rancheria** – A huge asset and partner to the City of Susanville is the Susanville Indian Rancheria (SIR) in partnership with the Susanville Indian Rancheria Corporation (SIRCO) and the Susanville Indian Rancheria Housing Authority (SIRHA) operates the Diamond Mountain Casino and its hotel, Lava Rock Grill, 24 Hour Café, and numerous housing units within Susanville. Local patronage is vital to the commercial success of these entities. SIR was kind enough to grant the city of Susanville access to their own traffic counts, which highlighted a significant number of people driving to destinations when they could be walking or biking.

• **Lassen Community College** – Lassen Community College is located in northeastern Susanville off of State Route 139. It has one point on this highway for both ingress and egress (Figure 16). Those who wish to reach the college on foot or bike must compete with cars on the

shoulder of SR 139, where speeds regularly exceed 55 mph. A multi-modal pathway could easily convince students to walk or bike.



Figure 16

- Entrance and exit to Lassen Community College
- This picture shows the only point of ingress and egress serving the college.
- Building a dedicated bicycle path past the college could convince more students and staff to bike or walk to class, while simultaneously mitigating the traffic slowdowns that accompany start and end times.

• **Banner Lassen Medical Center** – Banner Lassen Medical Center sits just beyond Lassen Community College in the far northeastern corner of the project area. It is also accessible only from a connecting street from SR 139. Banner receives enough low income patients that it now administers special grant funding towards purchasing bus passes for patients who otherwise would not be able to return home after a stay. A trail leading to and from the hospital could alleviate some of the cost associated with returning patients to their homes.

BANNER LASSEN MEDICAL CENTER LOW INCOME PUBLIC
TRANSPORTATION BUDGET

Type of bus pass	Unit Cost (\$)	Number of Passes	Total Cost (\$)
Anyone/Anywhere	5.00	402	1895.00
Anyone/In Town	3.00	1156	3468.00
Seniors and Disabled/Anywhere	2.50	31	77.50
Seniors and Disabled/In Town	1.50	162	243.00
Monthly	40.00	15	600.00
Total	-	1766	6283.50

Table 2

- o The number and cost of bus passes issued to low income patients following their hospital stays between 2017 and 2019.
- o The hospital purchases bus passes for those who do not have the means to return home following a hospital visit. This chart tallies the number of passes issued and the cost of each pass.
- o We see that the lion's share of passes go to destinations within town. A multi-modal trail to the hospital could give those without means other options for getting home.

Other key stakeholders are listed below:

- **Lassen County**
- **CalTrans**
- **Meadow View Neighborhood Representatives**
- **Numa Road Neighborhood Representatives**
- **Cit of Susanville**

What worked well: Making stakeholders comfortable and reassuring them that their input was crucial to the process invited some unfiltered feedback. City staff reiterated the value of feedback and how it reflected on the respective stakeholder constituencies, and in return it received a great diversity of opinions.

What we would do differently: Stakeholder meetings proved to be shorter than ideal for capturing the range of valuable opinions on different projects. City staff would devote more time to these in the future. City would also make sign-in sheets at meetings for quantifying attendance.

2. PUBLIC OUTREACH

In September of 2015, Lassen County's Chamber of Commerce was able to conduct a community survey concerning Main Street and the tactics most ideal for its rejuvenation. Of the 720 respondents (approximately 8% of Susanville's non-incarcerated population,) 21.31% responded that they would like to see better connectivity to trailheads and bike paths from Main Street. Those who wished for more pedestrian-friendly and bicycle friendly paths made up 45.4% and 9.89%, respectively. While these numbers represent an obvious link between citizen desire to walk or bike and the feasibility of doing so, many of the other questions asked also pertain to multi-modal transportation. For instance, 20.89% of respondents wanted paths to be well lit with artificial lighting fixtures. 32.73% wanted better signage directing visitors to nearby attractions like trailheads, parks, and river access; all places to which people would most likely walk or bike. Significant numbers also wanted textured sidewalks, ample landscape features, more consistent bench seating, and more flowers. While these things might seem superfluous at a glance, they all suggest that there are numerous walkers and bikers in the area that would benefit from the enhanced safety and aesthetic pleasance of paths that catered to walker and biker. It stands to reason that these improvements could also influence those who might otherwise drive to walk or bike instead.

Respondents were also asked to offer improvements in their own words. While no two were the same, some predominant themes became apparent. Apart from asking for business beautification in the form of better signage and demolishing long-vacant storefronts, people offered a plethora of ideas that would improve travel for both pedestrians and cyclists. These included managing wild areas for invasive species like goat heads, whose pointy seeds scatter themselves over roads and walkways and frequently pop bicycle tires. Pedestrians also repeatedly requested flowers in baskets hanging from signs or planted in public areas. This indicates that those who already walk around town frequently would like a nicer view as they do so. Trees along paths were also a common request, presumably because the shade they provide helps insulate pedestrians from heat in the summer. If we are able to group the comments pertaining to a need for lighting, better walkways, need for bike lanes, signage directing people to natural attractions, providing benches with cover, and aesthetics meant to appeal to those passing by on foot, we find that the vast majority of

comments fall under different ways of asking for improvements to pedestrians and cycling infrastructure and experience.

Soliciting community involvement and receiving feedback at rates higher than expected led city staff to explore more options that could incorporate citizen wishes.

2.1 Paul Bunyan Connectivity Sustainable Communities Grant

The Sustainable Communities Grant furnished by the California Department of Transportation allowed Susanville to enhance community outreach in new and efficient ways. By funding community workshops, door to door surveys, and traffic/pedestrian counts, Susanville's City Hall was able to ascertain with some clarity what citizens desired to change about their city. This project also funded research to identify potential sources of funding for similar upcoming projects. Moreover, it provided a framework for grant acquisition that can be used for future construction projects.

The Sustainable Communities Grant encouraged partnership with independent agencies to identify key stakeholders, draft concept-level drawings for infrastructure improvements, and organize data generated by surveys and workshops. The City of Susanville partnered with KASL Consulting Engineers to further their project needs.

2.2 Ads and Preparation for Community Meetings

In order to learn what community members sought from state funding sources, Susanville set about organizing multiple community workshops.



Figure 18

- Paul Bunyan workshop flyers generated by city staff.
- Text on the flyers informs citizens of CalTrans/Susanville partnership and what changes might be possible, then invites citizens to attend and share their ideas.
- Soliciting community input proved to be a valuable tool not only in narrowing the scope of projects to those citizens actually desired en masse, but also to instilling public trust and giving citizens a voice.

Time and place of these workshops were advertised in the local newspaper, on the radio, and on the city's official website. All citizens were invited to attend a meeting led by city staff, wherein the parameters of the Sustainable Communities Grant were laid out. Following staff explanation of the grant, attendees were asked to fill out a questionnaire that asked people how strongly they felt about various bicycle path-related improvements in the city.

What worked well: Including announcements in billings was a good starting point for spreading the word about upcoming projects. Conveying the city's enthusiasm for projects in meetings seemed to pass on excitement to attendees, and generated more participation through word-of-mouth.

What we would do differently: Putting notifications in every billing for the duration of the project would enhance public recognition of invitation to solicit ideas. Making flyers was delayed unexpectedly; had they been deployed from the outset along with surveys, we feel we would have received even more input from interested parties. Running online surveys quarterly would allow city staff to continuously track shifts in public opinion of projects. Using links in billings could remind people to fill out surveys.

2.3 Community Workshop #1

Attendees of the first community workshop concerning Paul Bunyan Connectivity in February of 2018 were informed of the city's application to partake in the Sustainable Communities grant and what type of changes said grant could fund. These project objectives were:

- Enhancing key corridors and addressing multi-modal connectivity issues
- Enhancing the existing multi-modal path for commute and recreation purposes
- Adding and improving bike routes and lanes
- Closing existing gaps in sidewalk network
- Adding and improving crosswalks
- Addressing parking issues in Susanville Ranch Park
- Monitoring the success of multi-modal facilities
- Adding signage for out-of-towners

Once parameters were defined, attendees were given maps of Susanville and asked to mark specific infrastructure changes that would benefit the community (Figure 19). Some of the chosen improvements were:

- A continuous Bike Path along Skyline Road
- Improvements to the existing Skyline Road Bike Path including crossings at drainage courses
- Parking along Radical Road to serve Susanville Ranch Park
- Bike Lanes along Chestnut Street, east of Paul Bunyan Road
- Safe Routes to School improvements (sidewalks, bike lanes) along Paiute Lane
- High visibility crosswalks on Paiute Lane near Meadow View School
- One way street (southbound) improvements on Cherry Terrace
- One way street (northbound) improvements on Paul Bunyan Road
- Sidewalks, striping, street lights, safety measures on Paul Bunyan Road
- Sidewalks on Numa Road and Spring Ridge Road, Skyline Road to Wada Street
- Bike Path improvements, SR 139
- Curb, gutter, sidewalks improvements, both sides of Chestnut Street, Paul Bunyan Road to SR 139 for better access to County Facilities
- Extension of Paul Bunyan Road north across Skyline Road
- Extension of Cameron Way south to connect to Skyline Road
- Walkway improvements at extension of Numa Road to Chestnut Street (Paul Bunyan Logging Trail alignment)
- Roundabout at Chestnut Street / Paul Bunyan Road / Grand Avenue Road intersection

Other notes provided by City staff from the early community workshop included:

- Residents expressed concerns about high speeds and lack of safety along Paiute in the vicinity of the Meadow View School. Traffic calming measures, high visibility crosswalks, bulb-outs, raised crosswalks, continuous sidewalks are needed

- In general, participants expressed the need to provide bike paths and bike lanes to connect corridors and retail / employment / recreation sites in the Paul Bunyan Connectivity Project area.

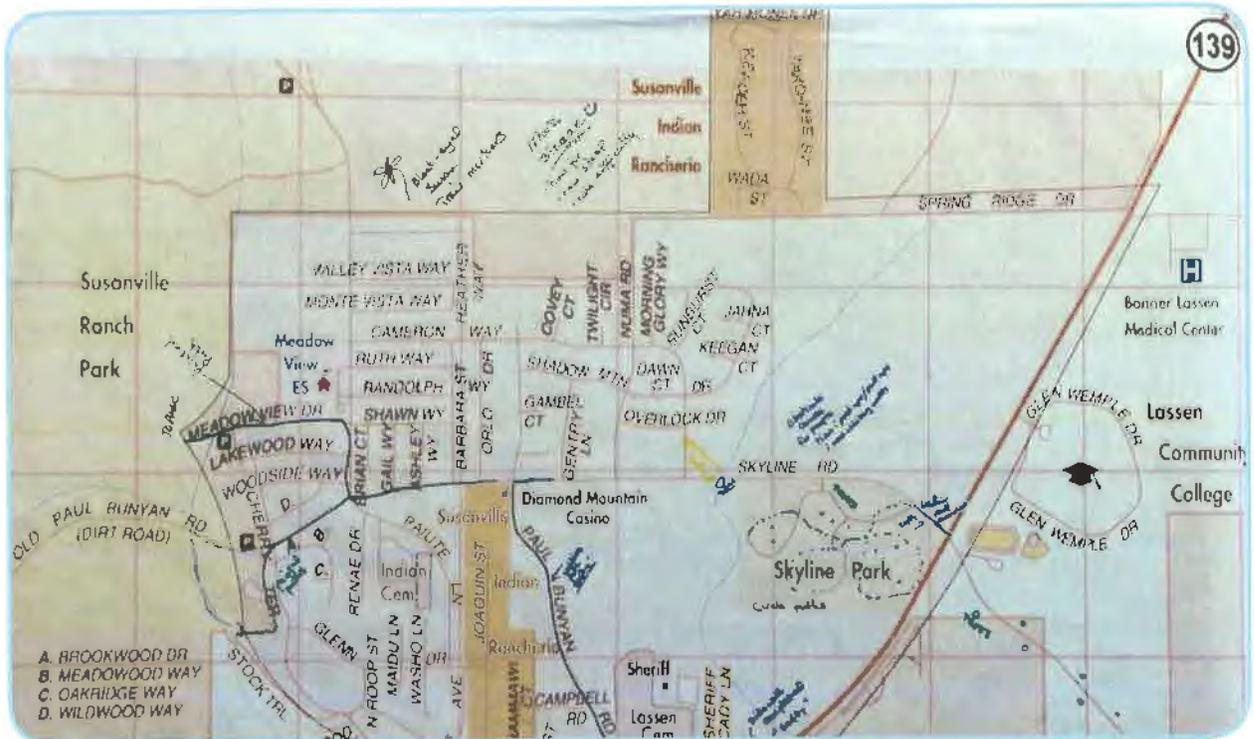




Figure 19

- o Scans of posters showing the project area on which workshop attendees were allowed to draw and brainstorm their ideas for infrastructure improvements.
- o Markings on the posters indicate citizen eagerness for better parking, a disc golf course, a bike path along SR 139, and numerous other ideas.

- o Allowing and encouraging brainstorming for infrastructure improvements allowed citizens to voice their specific concerns and exactly which projects they want to see come to fruition.

What worked well: Providing attendees with multiple marker colors to draw on map posters allowed uninhibited feedback and a way to differentiate one's ideas from their neighbors'.

What we would do differently: Advertising the results on the city's website could have shown potential users how their input was being displayed and cataloged. Directing readers to the website link on the information insert on each billing could encourage more future participation.

2.4 Community Workshop #2

On July 12, 2018, a follow up Community Workshop was held at the Susanville City Hall for the Paul Bunyan Connectivity Sustainable Communities Grant Project. Paul Bunyan Project area residents and business owners were specifically invited to participate. A PowerPoint presentation was made by KASL Consulting Engineers and City of Susanville staff. The PowerPoint presentation included a video with an introduction to Class IV Bike Tracks. Alternative and recommended roadway, bikeway and pedestrian way connectivity improvements were presented for each of the Paul Bunyan Community Sustainable Communities Grant Project corridors identified. Colored maps using aerial photos for backgrounds were presented for each of the corridors. Ground level photos were also used to provide street views along each corridor.



Figure 20

- o Attendees of the second public workshop.
- o This workshop featured conceptual ideas drafted by KASL Consulting Engineers and limitations of the project.
- o Answering specific questions about the project served an important role in keeping expectations for project achievements realistic and cost-appropriate.

At the conclusion of the PowerPoint presentation, a question and answer session was held. Specific questions and comments presented during this session included the following:

- Residents questioned whether there was any evidence that roundabouts improve intersection safety for pedestrians and bicycles.

- An alternative access to Banner Lassen Medical Center and to Lassen College was promoted by a member of the Banner Lassen Medical Center staff.
- Joaquin Street was suggested as a better alignment for a bike route than Parkdale Avenue.
- Paul Bunyan Road connectivity and safety improvements were suggested by several attendees as their top priority connectivity improvements.
- It was recommended that the City secure the Stock Trail right of way (west of Chestnut Street) as an alternative bike path to access Susanville Ranch Park and to existing mountain bike trails.
- Support for bike trails and sidewalks along Cherry Terrace was voiced.
- Residents want the City to close gaps in sidewalks.

In general, there was little support at this time for the Chestnut Street / Paul Bunyan / Grant Avenue Roundabout. There was little support for separate pedestrian improvements along Paul Bunyan Trail east of Paul Bunyan Road. While the residents strongly support improved pedestrian and bicycle connectivity, additional information regarding Class IV bike tracks is needed to support this type of bikeway. Class IV bike tracks appears to be best suited where there are few driveways or intersection conflicts. Skyline Road, east of Paul Bunyan could be a good candidate.

Susanville residents and stakeholders want safe connectivity both for bicyclists and pedestrians. Sidewalk extensions and closing gaps in sidewalks suggested for improved connectivity were well received. Bicycle connectivity is also supported. However, at this point, residents and stakeholders do not have a particular interest in the type of bicycle connectivity (Class I, II, III, IV) as long as adequate safety is provided.

After the "open forum" Q & A session participants were invited to have a closer review of the alternative connectivity improvements displayed for each corridor. At each corridor "station" there were opportunities to ask questions of KASL and City staff and to identify alternative features that they strongly supported, supported, disliked or strongly disliked. A Sample Survey Form is presented below (Figure 21).

PAUL BUNYAN CONNECTIVITY SUSTAINABLE COMMUNITIES GRANT
COMMUNITY WORKSHOP
SKYLINE ROAD Connectivity Elements

Strongly Support: SKYLINE to Chestnut connectivity excellent
idea

Sidewalk Connectivity throughout project, excellent
Great Bike Paths

Support: NORTH POOP connectivity excellent

No Comment: _____

Do Not Like: Round about skeptical / but new design
presented may work!

Strongly Dislike: _____

Other Comments: _____

What worked well: Including consultant involvement and professionally drafted conceptual drawings proved to community members that their ideas were being taken seriously and that progress realizing them had been made.

Figure 21

- A sample comments sheet submitted by a workshop attendee.
- Attendees were encouraged to react in their own words to the proposed projects and offer why they might be good or bad ideas.
- Inviting free thought on specific projects allowed city staff to accept comments from all citizens who wished to submit it, and compile answers favoring or opposing various projects.

3. PLAN DEVELOPMENT

3.1 Develop Design Concepts

After identifying connectivity improvements desired by city residents via surveys and at workshops, the City of Susanville Planning Department and Susanville Public Works set about developing proposals that could accomplish the changes put forth during community input.

The project area is enclosed by State Route 139 on the east, Spring Ridge Road on the north, Cherry Terrace on the west, and Chestnut Street on the South. Skyline Road bisects the project area running east and west, and Paul Bunyan Road, Numa Road, and Paiute Lane act as major arterials running north and south. The low-density residential zones on the west and north will be served by trails that could take them to Lassen College, Skyline Park, Banner Lassen Medical Center, and Northeastern Rural Health Clinic on the east, Meadow View School and Diamond Mountain Casino on the west, and all of town's amenities to the south.

The following narrative summaries and cross sections present the program ready concept level connectivity improvements suggested for the Paul Bunyan Connectivity Sustainable Communities Grant Project area. The following suggested improvements were developed from meetings and discussions with City of Susanville Planning and Susanville Public Works staff, early community workshop input, the July 12, 2018 Community Workshop and input provided by Project Stakeholders.

All of the suggested improvements presented in the following narratives and sections are subject to securing adequate design and construction funding with either State and/or Federal Grants or through the City's Capital Improvement Funds. While design and construction of the suggested connectivity plans could be completed as one Project, it is likely that funding and coordination / approval from, stakeholders (including Susanville Indian Rancheria, Meadow View School, Caltrans, Lassen County) will result in connectivity improvements approved, designed and constructed in several segments or phases.

All of the connectivity improvements presented herein will be subject to detailed engineering design and design review and approval by the Susanville Public Works Department.

PRIORITIZATION OF SUGGESTED CONNECTIVITY IMPROVEMENTS

The “Top 9” connectivity improvements (Figure 22) suggested for the Paul Bunyan Connectivity Sustainable Communities Grant Project have been identified in this Project Report. Implementation of any and all of these corridor improvements would respond to the city’s connectivity project objectives. Further prioritization of the Top 9 list can be conducted by evaluating each with respect to the following criteria:

Connectivity Benefits

Which of the corridor improvements provides the most connectivity benefits to the greatest number of Susanville residents, property owners and business interests?

Safety Benefits

Which of the corridor improvements provides the most significant safety benefits to the city of Susanville?

Constructability

Which of the corridor improvements can be constructed with the fewest right of way, jurisdictional and/or environmental (e.g. wetlands) constraints?

Funding

Which of the Paul Bunyan Project area improvements are most likely to receive grant funding?

Costs

Which of the corridor improvements can be conducted with the least cost and the best cost to benefit ratio?

Connectivity

All of the corridor improvements presented in this project report will improve project area connectivity. From the findings of this study and, in particular, the input from the community at the July 12, 2018 Community Workshop, the most significant connectivity improvements would likely be provided by suggested corridor improvements to:

- Paul Bunyan Road
- Skyline Road
- State Route 139

High levels of project area connectivity would also be provided by suggested improvements to:

- Chestnut Street
- Glenn Drive / Parkdale Avenue / Joaquin Street
- N. Roop Street / Cherry Terrace

While the remaining three corridor improvements provide important connectivity features, implementation would benefit fewer City of Susanville residents than the other corridor improvements in the Top 9 list. The Paiute Lane improvements would principally benefit Meadow View School. The Numa Road / Spring Ridge Road improvements would principally provide connectivity for North Susanville and Susanville Indian Housing residents. The Radical Road improvements would principally provide improved connectivity to the west end of the Project area and to Susanville Ranch Park.

Safety Benefits

All of the corridor improvements suggested in this study would improve bicycle and pedestrian safety. The level of safety provided is impacted by current and projected levels of traffic, traffic speeds, and projected levels of bicyclists and pedestrians using the project corridors. Based on the findings developed in this study and traffic data available from the city, Lassen County and CalTrans, the greatest safety benefits would be provided by suggested corridor improvements on:

- Paiute Lane
- Paul Bunyan Road
- State Route 139
- Skyline Road

Significant safety benefits would also be provided with suggested improvements to:

- Chestnut Street
- Numa / Spring Ridge Road

Because the remaining Paul Bunyan corridor improvements principally serve residential streets with lower traffic volumes and fewer projected bicyclists and pedestrians, the following could have a lesser, but still important, safety benefit:

- Glenn Drive / Parkdale Avenue / Joaquin Street
- N. Roop Street / Cherry Terrace
- Radical Road

Constructability

Available right of way, required coordination with other stakeholders, and environmental issues were considered in evaluating the constructability of suggested corridor connectivity improvements. The following corridor improvements can be completed without the need for additional right of way acquisition and with few apparent environmental issues (to be verified). Coordination with

other stakeholders (Meadow View School, Project residents with frontage along corridor, Susanville Indian Rancheria for example) will still be required:

- Paiute Lane
- Numa / Spring Ridge Road
- Glenn Drive / Parkdale Avenue / Joaquin Street

With respect to constructability, implementation of the following corridor improvements are encumbered, to some degree, by the need to acquire right of way, by potential or apparent environmental issues, and / or with significant coordination required with other stakeholders:

- Skyline Road (R/W needed from Susanville Indian Rancheria and Lassen County)
- Paul Bunyan Road (Potential wetlands and drainage issues)
- Radical Road (drainage issues)

Construction of the Class I / Multi-Use Trail along State Route 139 will require right of way acquisition, a CalTrans Encroachment Permit and an approved CalTrans Project Study Report. Completion of improvements along Chestnut Street will require detailed right of way determination and coordination with Lassen County.

Funding

Based on findings to date, the corridor improvements that would most likely receive grant funding support include:

- Paiute Lane
- State Route 139
- Paul Bunyan Road
- Numa / Spring Ridge Road

Given the appropriate "nexus" between project benefits, costs, and available grant funds the next suggested funding tier is believed to be the improvements proposed on:

- Chestnut Street
- Skyline Road
- Radical Road

Costs

The lowest cost of the Paul Bunyan corridor improvements suggested in his study are those proposed for:

- Cherry Terrace / N. Roop Street
- Numa / Spring Ridge Road

Moderate cost corridor improvements include:

- Radical Road
- Paiute Lane
- Glenn / Parkdale / Joaquin Street
- Chestnut Street

Highest cost of the corridor improvements presented in this study are those suggested along:

- Skyline Road
- Paul Bunyan Road
- State Route 139

Benefit to Cost Ratio

Considering the connectivity and safety benefits of each corridor improvement and comparing these benefits with respect to projected costs, it is believed that despite their projected relatively high costs, the best Benefit to Cost Ratios would be provided by connectivity improvements at:

- Skyline Road
- Paul Bunyan Road
- SR 139

The next highest benefit to cost ratios are projected to be provided by connectivity improvements at:

- Chestnut Street
- N. Roop / Cherry Terrace
- Numa / Spring Ridge Road
- Paiute Lane

Summary – Prioritization

A visual summary of the prioritization criteria presented above and the priority ranking of each of the corridor improvements. Based on the previously present criteria and corridor rankings, the following are suggested as the highest priority connectivity projects included in the Top 9 improvements of the Paul Bunyan Connectivity Improvement Project. All of the criteria presented herein have been weighted equally.

- Skyline Road
- Paul Bunyan Road
- Paiute Lane
- Numa / Spring Ridge Road
- State Route 139

Corridor / Location	Connectivity Benefits	Safety Benefits	Constructability	Funding	Costs	Benefits to Costs	Summary Priority
Radical Road	Moderate	Moderate	Constructible	Fundable	Moderate Cost	Fair	3 rd Tier
Skyline Road	Very High	Very High	Constructible	Fundable	High Cost	Best	Top Priority
Paul Bunyan Road	Very High	Very High	Constructible	Very Fundable	High Cost	Best	Top Priority
Paiute Lane	Moderate	Very High	Very Constructible	Very Fundable	Moderate Cost	Good	Top Priority
Numa / Spring Ridge Road	Moderate	High	Very Constructible	Very Fundable	Low Cost	Good	Top Priority
State Route 139	Very High	Very High	Constructability Issues	Very Fundable	High Cost	Best	Top Priority
Glenn Drive / Parkdale Ave. & Joaquin Street	High	Moderate	Very Constructible	Fundability Issues	Moderate Costs	Fair	3 rd Tier
Chestnut Street	High	High	Constructability Issues	Fundable	Moderate Costs	Good	2 nd Tier
Cherry Terrace	High	Moderate	Very Constructible	Fundability Issues	Lot Cost	Good	2 nd Tier

Figure 22

- o Chart depicting viability of various Paul Bunyan projects.
- o KASL was able to use cost and time calculations and pair them with funding opportunities that could allow completion of projects.
- o Reducing projects to their value and priority level allowed city staff to compare which projects could be accomplished.

What worked well: Taking consultants to the physical locations of each project identified while stating corresponding community desires for them aided in the comprehension of prospective changes.

What we would do differently: We would provide for our consulting engineers a template with the types of details that may be included in conceptual drawings suitable for sharing with the public at preliminary stages.

3.2 Funding and Implementation

One of the benefits of working with a consultant was the discovery of state funding sources for infrastructure projects that, until recently, had been in the City's blind spot. Partnering with KASL Consulting Engineers helped to open up a multitude of monetary opportunities that could potentially help Susanville realize some of its future projects.



The City of Susanville was awarded funding for the Paul Bunyan Connectivity Project through the CalTrans Sustainable Communities Grant Program. With program ready connectivity plans prepared as part of this grant program, the City of Susanville can now proceed with applications for design and construction funding. For certain grant funding programs, applications can be made sequentially. For example, a project may be programmed for environmental work without being programmed for plans, specifications and estimates (PS&E). A project may be programmed for design without being programmed for right of way acquisition or construction. A project may be programmed for right of way acquisition without being programmed for construction.

State and Federal programs which best fit the benefits and conditions of various components of the Paul Bunyan Connectivity Project and the City's need for environmental, design, right of way and construction funding include:

- State Transportation Improvement Program (STIP)
- Active Transportation Program (ATP)
- Bicycle Transportation Account (BTA)
- Congestion Mitigation and Air Quality (CMAQ) Improvement Program
- Environmental Enhancement and Mitigation Program (EEMP)

3.2.1 STATE TRANSPORTATION IMPROVEMENT PROGRAM (STIP)

The STIP is a multi-year capital improvement program for transportation projects on and off the State Highway system. A STIP grant application could, therefore, be submitted by the city for the SR 139 Class I / Multi-Use Trail improvements as well as for corridor improvements described in this Project Report for the other 8 Paul Bunyan Connectivity Corridors. To apply for STIP funding the City would work through the Lassen County Transportation Commission. Guidelines adopted for the 2018 STIP and summarized herein and are more fully presented in:

[atc.gov/program/10/2018-stip-guidelines-adopted-3161.pdf](#)

Specific STIP guidelines include:

- Senate Bill 1 stabilized the funding in the State Highway Account that is directed to fund the STIP.
- Bicycle and pedestrian projects may be programmed in the STIP as long as they are eligible for State Highway Account or Federal Funds.
- For Projects located on a State Highway (SR 139, for example), the STIP requires that a Project Study Report (PSR) be prepared. For projects not on a State Highway, a PSR equivalent is required.
- A PSR equivalent must be adequate to define and justify the project scope, cost and schedule to the satisfaction of the regional agency (in this case, the Lassen County Transportation Commission).
- STIP program applications may be made sequentially; for environmental work only, for PS&E only, for right of way acquisition, for construction.
- STIP applications are evaluated with respect to how the Project's goals and objectives compare to, and help implement, the region's Regional Transportation Plan. Roadway improvements on Chestnut Street and Paiute Lane and bikeway / pedestrian improvements on Paul Bunyan Road, Skyline Drive, Spring Ridge Road, Numa Road, Cherry Terrace and SR 139 are included in the 2017 Lassen Project Transportation Plan. Key corridor improvements along these roadways and included in this Paul Bunyan Connectivity Project Report are consistent with, and would help implement, the 2017 Lassen Regional Transportation Plan.

To best qualify for STIP funding benefits specific to the Paul Bunyan Connectivity Project should be quantified with respect to:

- Change in Vehicle Mile Traveled (VMT) per capita
- Change in commute mode sphere (travel to work or school)
- Change in fatalities and serious injuries per capita
- Change in fatalities and serious injuries per VMT
- Increased capacity benefits including new or upgraded bicycle lane / sidewalk miles on State Highways or on local streets and roads

STIP programming cycles begin with the release of proposed fund estimates in July of odd-numbered years. Once the fund estimate is adopted, regional planning agencies (in this case, the Lassen County Transportation Community) prepare transportation improvement plans for submittal by December 15 of odd numbered years. The City of Susanville should, therefore, work with the Lassen County Transportation Commission and apply for 2019 STIP funding. Paul Bunyan Community Project improvements suggested in this Study Report for Paul Bunyan Road, Skyline

Drive, Numa / Spring Ridge Road, Cherry Terrace and SR 139 are the most likely candidates to be considered for STIP funding during the 2019 funding cycle.

3.2.2 ACTIVE TRANSPORTATION PROGRAM (ATP)

The purpose of the California Transportation Commissions Active Transportation Program (ATP) is to encourage increased use of active modes of transportation, such as biking and walking. The goals of the ATP are to:

- Increase the proportion of trips accomplished by biking and walking
- Increase the safety and mobility of non-motorized users
- Advance the active transportation efforts of regional agencies to achieve greenhouse gas reduction goals as established pursuant to Senate Bill 375 (Chapter 728, Statutes of 2008) and Senate Bill 391 (Chapter 585, Statutes of 2009)
- Enhance public health, including reduction of childhood obesity through the use of programs including, but not limited to, projects eligible for Safe Routes to School Program funding.
- Ensure that disadvantaged communities fully share in the benefits of the program
- Provide a broad spectrum of projects to benefit many types of active transportation users

All of the Paul Bunyan corridor improvements suggested in this Project Report would conform to the goals of the ATP.

Each ATP programming cycle includes four years of funding. New programming capacity for the 2019 ATP will be available for fiscal years 2019-2020, 2020-2021, 2021-2022 and 2022-2023. ATP funds are distributed to Metropolitan Planning Organizations (MPO) as well as to small urban and rural areas with populations of 200,000 or less. The City of Susanville is not located within an MPO. Ten percent of ATP funds are specifically designated for small urban and rural areas such as Susanville. Fifty percent of ATP funds are also competitively awarded statewide by the California Transportation Commission (CTC). A minimum of 25% of the funds in the statewide competitive program must benefit disadvantaged communities.

A summary of key ATP points applicable to the City of Susanville and the Paul Bunyan Connectivity Project are presented herein.

- The CTC does not require a matching fund for the ATP.
- Funding from ATP may be used to fund the development of community-wide active transportation plans encompassing disadvantaged communities and including bike, pedestrian, safe routes to schools, or comprehensive active transportation plans.
- The City of Susanville is eligible to apply for ATP Funds.

- The City may also partner with Caltrans to expand ATP funding opportunities. Partnering with Caltrans for ATP funding would be applicable for the Class 1 / Multi-Use Trail improvements suggested along SR 139.
- Eligible ATP Projects include infrastructure projects and includes environmental, design, right of way and construction phases of a capital (facilities) projects.
- To be considered for ATP funding a new infrastructure project will require a complete Project Study Report (PSR). A PSR would be specifically required for the SR 139 Class 1 / Multi-Use Trail Project. A PSR equivalent would be sufficient for the other Paul Bunyan Connectivity Corridor Projects.
- To qualify as a Disadvantage Community (DAC), Susanville must show that the Median Household Income is less than 80% of the statewide average or meet the requirements of the California Communities Health Screening Tool 3.0 or show that at least 75% of the public school students in the Project are eligible to receive free or reduced price meals under the National School Lunch Program.
- To qualify as a Safe Route to School Project, projects must directly increase safety and convenience for public school students to walk and / or to bike to school. Paul Bunyan connectivity project corridor improvements suggested along Paiute Lane, Skyland Road, SR 139, Paul Bunyan Road and Numa / Ridge Road would clearly qualify as Safe Routes to School projects.
- The City of Susanville would likely apply for ATP funds in the Medium Project Infrastructure only or Infrastructure / Non-Infrastructure Property Category with a total project cost between \$1.5 Million to \$7.0 Million or for ATP funds available in the Small Project, Infrastructure only or Infrastructure / Non-infrastructure Project Category with a total project cost of less than \$1.5 Million. A sample Medium Infrastructure only ATP project application is included in the attachments to this Project Report.

3.2.3 BICYCLE TRANSPORTATION ACCOUNT (BTA)

The Bicycle Transportation Account (BTA) is an annual program that provides state funds administered by the Caltrans Local Assistance Programs for City and County projects that improve safety and convenience for bicycle commuters. To be eligible for BTA funds there must have been adopted a Bicycle Transportation Plan. In compliance with this requirement, the Lassen County Transportation Community has adopted the Lassen County Bikeway Master Plan. With the Paul Bunyan Connectivity Project area the Lassen County Bikeway Master Plan (BMP) includes proposed Class 1 Bike Paths on:

- Skyline Road & Skyline Park
- Radical Road
- SR 139
- Paul Bunyan Logging Trail
- Spring Ridge Road

Class 2 Bike lanes are proposed in the Lassen County BMP on:

- Chestnut Street
- Parkdale Avenue
- Paiute Lane

Class 3 Bike Routes are shown on Cherry Terrace.

BTA projects are intended to improve safety and convenience for bicycle commuters and may include:

- New bikeways serving major transportation corridors
- New bikeways removing travel barriers to potential bicycle commuters
- Secure bicycle parking at employment centers park-and-ride lots, rail and transit terminals and ferry docks and landings
- Bicycle-carrying facilities on public transit vehicles
- Installation of traffic control devices to improve the safety and efficiency of bicycle travel
- Elimination of hazardous conditions on existing bikeways
- Planning
- Improvement and maintenance of bikeways

Eligible BTA project activities include:

- Project Planning
- Preliminary Engineering
- Final Design
- Right of way Acquisition
- Construction and/or Rehabilitation

At a minimum, Paul Bunyan Connectivity Corridor Projects included in this Project Report that are eligible for BTA funds include:

- Skyline Road
- SR 139
- Radical Road
- Numa Road / Spring Ridge Road
- Paiute Lane
- Parkdale Avenue / Glenn Drive
- Chestnut Street
- Cherry Terrace

3.2.4 CONGESTION MITIGATION AND AIR QUALITY (CMAQ) IMPROVEMENT PROGRAM

The Congestion Mitigation and Air Quality (CMAQ) Improvement Program was established by the 1991 Federal Intermodal Surface Transportation Efficiency Act (ISTEA) and reauthorized with the

passage of Transportation Equity Act for the 21st Century (TEA-21). A wide and diverse variety of programs are eligible for CMAQ funding. Specific project types include Bicycle and Pedestrian Facilities and Programs. To be eligible for CMAQ funding, projects must be located in a non-attainment area for ozone, carbon monoxide, or particulate matter (PM-10). Lassen County is a non-attainment area for PM-10.

The CMAQ program is funded by the Federal Highway Administration and administered by the CalTrans Local Assistance Program. CMAQ funds may be used to establish new or expanded transportation projects or programs that reduce emissions, including capital investments in transportation infrastructure, congestion relief efforts, diesel engine retrofits or other capital projects.

Bicycle and pedestrian facilities and programs eligible for CMAQ funding include:

- Constructing bicycle and pedestrian facilities (paths, bike racks, support facilities, etc.) that are not exclusively recreational and reduce vehicle trips
- Non-construction outreach related to safe bicycle use

Each of the Paul Bunyan Connectivity Project elements include the construction of bicycle and pedestrian facilities. Each of the Top 9 Corridor Projects identified in this Project Report would, therefore, be eligible for CMAQ funding.

3.2.5 ENVIRONMENTAL ENHANCEMENT AND MITIGATION PROGRAMS (EEMP)

The Environmental Enhancement and Mitigation Program (EEMP) grants to local, state and federal agencies funds to mitigate the environmental impacts caused by new or modified public transportation facilities. Grants for individual projects are generally limited to \$500,000 each. Matching funds are not required for EEMP grants; however, leveraging other sources of funds can make projects more competitive.

EEMP funding must be used to mitigate the environmental impacts of an existing transportation facility or the impacts associated with the construction of a new transportation facility. Mitigation of the environmental impacts of improvements to an existing transit facility (widening of Paul Bunyan Road or reconstruction of the Skyland Park Class I Bike / Multi-Use Trail, for example) or construction of a new facility (Radical Road improvements) would qualify for EEMP funding.

Improvements must be included in a Regional Transportation Program (2017 Lassen Regional Transportation Plan), Environmental Compliance (CEQA / NEPA) must be completed at the time of the EEMP grant application. Particularly successful EEMP grant projects include Best Management Practices that increase water conservation.

EEMP grants are selected based on:

- Review of application for completeness and initial eligibility
- Site visits by representatives of the EEMP
- Final evaluation and recommendations to the Resource Agency Secretary
- Recommendations to the California Transportation Commission

Of the Top 9 Paul Bunyan connectivity corridor locations, the Paul Bunyan Road and the Radical Road corridor projects would likely have the best chance to qualify for an EEMP grant.

What worked well: Sharing identified projects with CalTrans allowed city staff to learn about new funding opportunities of which they were not previously aware. This practice should be repeated.

What we would do differently: Identifying new grants proved so important to the process that employing a specialized grants expert could have helped to identify even more funding resources that would further city goals. Susanville would consider hiring two consultants for a similar hypothetical project; one that could plan and draft concepts, and one for program-ready preparation.

3.3 Draft Plan

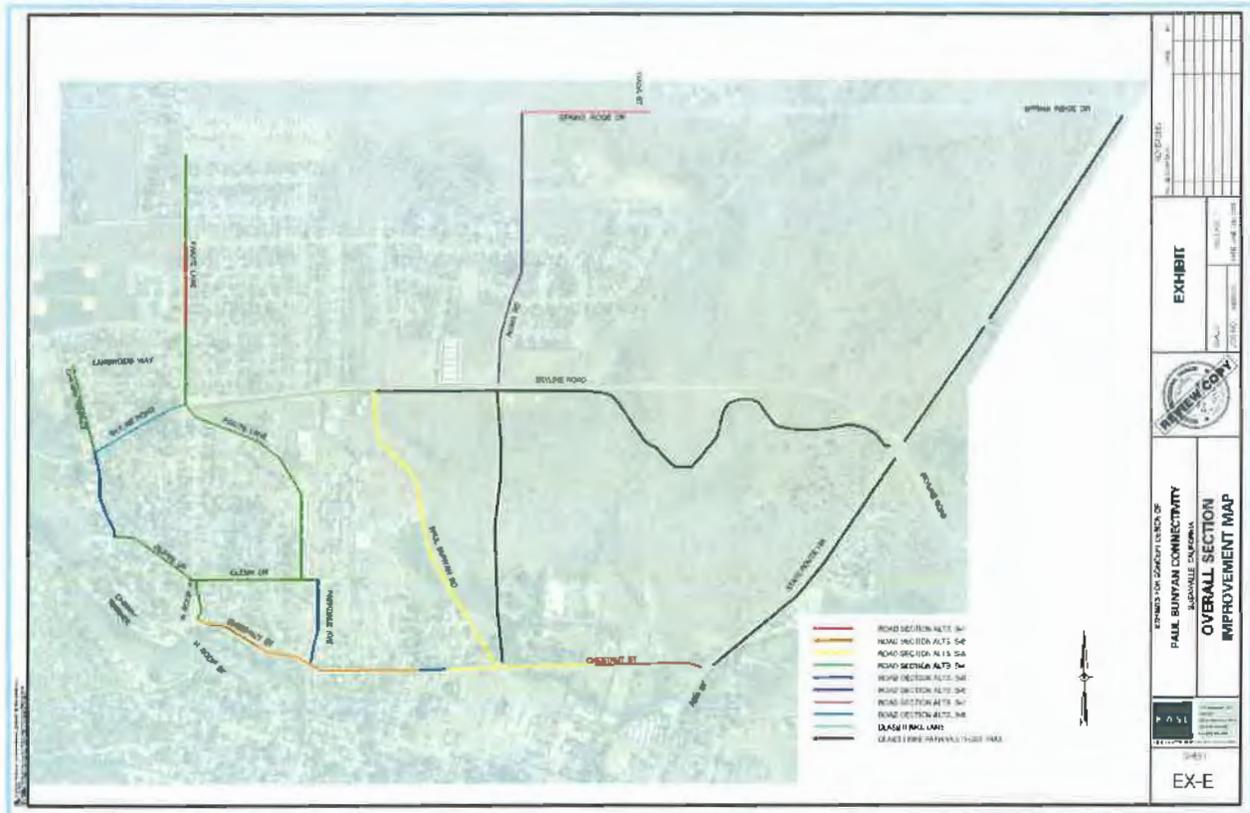


Figure 23

- KASL's preliminary map of potential projects
- Map depicts different road sections and Class I and II bike lanes
- Allowing staff to see locations of projects could help staff select a string of projects that could maximize connectivity.

What worked well: The accumulation and cataloging of sequential data positioned Susanville to track traffic patterns against the existing network and narrow the scope of study.

What we would do differently: Using the experience of completing this grant, Susanville will begin the drafting of a report at the very beginning of hypothetical projects, rather than beginning one within the last six months. This report may serve as a suitable template.

3.4 Final Plan

The Final Plan is the this report including the attached preliminary drawings in the attached KASL Engineering Consulting assessment.

What worked well: The collective enthusiasm and motivation demonstrated by city staff, stakeholders, and residents to turn assignments into fully funded projects propelled us to put forth our best foot.

What we would do differently: Requesting quarterly feedback throughout the drafting of the final plan would have kept city staff abreast of which plans concurring with their vision and plans that weren't.

4. BIBLIOGRAPHY

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5. APPENDIX

Appendix A: RFP - Consultant Solicitation

CITY OF SUSANVILLE

County of Lassen
State of California



REQUEST FOR PROPOSAL

for

Sustainable Communities Grant
GRANT ADMINISTRATION SERVICES

Project Name: Design Services - Paul Bunyan Connectivity - 2018

Due Date/Time: April 13, 2018, 3:00 PM

Contact: Quincy McCourt, City Project Manager

gmccourt@cityofsusanville.org (530) 252-5110

Delivery Location: 66 North Lassen Street, Susanville, CA 96130

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Introduction

The City of Susanville is soliciting proposals to provide professional design and consulting services for preparing planning conceptual designs that are ready for programming to enhance the multi-modal network located within the Northern Section of the City of Susanville. Funding is currently available for this phase of the project and funded with CalTrans State dollars. The plan will include existing conditions, planning conceptual design, and strategies for implementation and funding.

All planning design work will be in conformance with the CalTrans Local Assistance Procedures Manual, the latest draft version of the PROWAG and AADAG or any other adopted standards affecting the placement of ADA complaint facilities along the proposed street segments. **The consultant will draft a connectivity plan developed from community input and recommendations. The report will include the existing conditions report, a summary of transportation improvement needs, a list of prioritized projects, preferred streetscape concepts, public outreach materials, and a funding and implementation strategy guide.**

The final project deliverable from the consultant will be to provide a program ready final plan including planning design services related specifically to developing any needed ADA compliant and relevant facilities adjacent to the local City streets to enhance the multi-modal network that supports California state goals to increase walking and biking rates and decrease GHG emissions. This Request for Proposal (RFP) describes the project, the required scope of services, the consultant selection process, and the minimum information that must be included in the submitted proposals. The City will select one consultant.

The City intends to compare and evaluate all qualifying submittals and select the most qualified candidate based on proposal content, and responsiveness to the City's criteria and goals.

Project Description

The City of Susanville Sustainable Transportation Project was signed into contract in March of 2017 to provide funding to further study existing multi-modal conditions and prepare a program ready design and connectivity plan. The project area is bounded by Tribal housing off of SR 139, Chestnut Street, Spring Ridge Drive, and the Susanville Ranch Park (see map - Appendix B).

This project will be to determine new collectors, bike paths, public transit to enhance vitality and safety along both primarily residential streets with some segments within the areas of uptown Susanville. Most streets act in a local classification with some functioning as collectors. It is expected that existing ramps, where present, be upgraded to current standards and certain portions of street frontage be considered for additional facilities where warranted. The acquisition of right of way is not anticipated nor programmed and relocation of utilities should be avoided as much as feasible.

The consultant will consider alternative solutions to meet current ADA standards in order to avoid the need for acquisition of property, avoiding conflicts with utilities and private improvements. Encroachments into the City right of way will be handled on a case-by-case basis.

The project will serve to improve the ride-ability of streets within the incorporated area of the City within the boundaries of the attached map and minimize excessive maintenance efforts by the City as a result of the poor condition of the streets. Some additional work will include reconstruction of failed structural sections, re-establishing drainage patterns with regard to or re-conforming street profiles, developing proper road prisms (cross slopes) and updating existing pedestrian facilities (ADA complaint) with upgrades to handicap ramps and sidewalk areas as funding allows.

Please offer up during the RFP any alternative means and methods to accomplish the overall goal of enhancing multi-modal transportation and safety by identifying small deliverables and prioritizing them with respect to available funding.

Request for Proposals

The City of Susanville is seeking a CONSULTANT to provide services to the City for the administration and implementation of the identified projects listed below in the Scope of Work section. The response to the "Request for Proposals" must be made according to the requirements set forth in this RFP. Failure to adhere to these requirements or to include conditions, limitations or misrepresentations may be cause for rejection of the submittal.

CONSULTANT's proposal shall include all oversight, documentation, coordination, consultation, meetings, reviews, etc. as necessary to comply with CalTrans requirements for the design and planning identified above. This shall include, but is not limited to the following:

- A. A brief history and detailed summary of your firm's qualifications and specific experience;
- B. A statement of your firm's policy regarding affirmative action, and indication if your firm is a small business and/or minority or woman owned business.
- C. Experience of firm and individual team members as it pertains to CalTrans grant funded project contract and grant administration and implementation, including CalTrans funding.
- D. List of similar work performed by the firm, including location and type of project. The City may contact past clients for references. Provide the name and telephone number of a reference for each project listed;
 1. A description of the proposed scope of work and methodology.
- E. A proposed schedule;
- F. For each activity (i.e. do not provide a lump sum for all activities), provide a description of proposed costs including:
 1. Provide the hourly rate for each project team member who would be assigned to this project and the estimated number of hours required for each task.
 2. Indicate other expenses that are requested to be reimbursed.
 3. CONSULTANT shall identify any personnel and overhead costs associated with periods of project inactivity or delay.
- G. Organizational chart applicable to this project identifying the project manager, key personnel, and supporting staff. Specific responsibilities of each person should be detailed;
- H. Knowledge of the City of Susanville (list any previous experience with the City);
- I. Acknowledgement that the standard contract for the City of Susanville (see Exhibit A

for the sample) is acceptable as presented or as amended (include the proposed amendments).

- J. Consultant must comply with the restrictions of the Restricted Grant Agreement (RGA) between CalTrans and the City of Susanville. Pages 6 through 15 - see Exhibit C.
- K. Please include DBE goal.

Scope of Work

The Scope of Work to be performed by the CONSULTANT shall include the following activities:

- I. Project Objectives
 - A. To identify small deliverables and prioritize them in the plan towards eligible funding
 - B. Recognize safety and connectivity issues for multi-modal transportation users
 - C. Develop conceptual planning design for corridor enhancement in conjunction with the community vision
 - D. Encourage active transportation through community engagement and future plan implementation
- II. Project Framework Potential Plans
 - A. Enhance key corridors and address multi-modal connectivity issues
 - B. Enhance the existing multi-modal path for commute and recreation purposes
 - C. Add and improve bike routes and lanes
 - D. Add sidewalk and improve ADA accessibility
 - E. Close existing gaps in the sidewalk network
 - F. Add and improve crosswalks
 - G. Address parking issues at the Susanville Ranch Park
 - H. Monitor the success of multi-modal facilities
 - I. Signage for Out-Of-Towners
- III. Develop Planning Design Concepts 1.0
 - A. Information provided by The City of Susanville
 1. Existing Conditions
 2. Stakeholder Input
 3. Community Input
 4. Planning Design Sketch
 - B. Provide professional planning design concepts.
 1. With the information provided by The City of Susanville, prepare professional conceptual planning designs ready for presentation. These will be presented at the Community Workshop.
 2. Timeframe: 3 weeks
 3. Meet at the City of Susanville City Hall
 4. Discuss
 5. Prepare planning design
- IV. Community Workshop
 - A. Present design concepts to attendants at City of Susanville City Hall Workshop
 - B. Collect Feedback
- V. Plan Development
 - A. Develop Plan Design Concepts 2.0
 1. Meet at City of Susanville City Hall to discuss
 - a) Ten projects will be selected
 2. Prepare final conceptual planning designs
 - a) One overview graphic incorporating all designs
 - b) One design for each of the ten projects
 - B. Funding and Implementation
 1. Identify and prepare for a successful application process on projects to come
 - C. Draft Plan
 1. Plan should be a professional report in a color binder as well as digital editable fully hyperlinked connecting table of contents to sub sections in a common format.

- D. Final Plan
 - 1. Complete a program ready final report.
- VI. Provide Hourly Rates
- VII. Develop Funding Strategies
 - A. Review past funding sources
 - B. Identify potential funding sources
 - 1. STIP – State Transportation Improvement Program
 - 2. ATP – Active Transportation Program
 - 3. BTA – Bicycle Transportation Account
 - 4. SR2S – Safe Routes to School
 - 5. BFU – Bicycle Facilities Unit
 - 6. EEM – Environmental Enhancement and Mitigation Program
 - 7. ADA Improvements
 - 8. Others and overlapping opportunities
 - C. Define Implementation Strategies
 - D. Prioritize Projects
- VIII. Draft Connectivity Plan
 - A. Existing conditions report
 - B. Summary of transportation improvement needs
 - C. Prioritized projects
 - D. Preferred streetscape concepts
 - E. Public outreach materials
 - F. Funding implementation strategy guide
- IX. Prepare Final Connectivity Plan
 - A. Review Plan
 - 1. Circulate for corrections/feedback
 - B. Edit Plan
 - C. Include credit of the financial contribution on cover
 - D. Submit four hard copies to Cal Trans
 - E. Submit four electronic copies to Cal Trans

Submittal of Proposals

Three copies of the Proposals shall be submitted by **Friday, April 13, 2018 at 4:00 p.m.** either in person or by mail to the following address:

City of Susanville
Administrative Services Department
Attn: Quincy McCourt
66 N. Lassen St. Susanville, CA 96130

Late or incomplete proposals will not be considered. The City shall have the sole discretion in determining the completeness of each proposal. This solicitation of proposals is not construed as a contract of any kind. The City is not responsible for any pre-contractual expenses incurred by firms responding to this RFP. All proposals shall become the property of the City and will not be returned. All proposals received may become public records under the laws of the State of California and the public may be given access to them after the formal selection process has been completed.

It is understood and agreed that the Proposer claims no proprietary rights to the ideas and written materials contained in or attached to its proposal. Proposals will be maintained as confidential until recommendation is submitted to the City Council at which time all proposals will be public record.

Proposers warrant and covenant that no official or employee of the City, or any business entity in which an official of the City has an interest, has been employed or retained to solicit or aid in the procuring of the contract of this project.

The successful proposer will have the status of an independent Consultant and will not be either an officer or an employee of the City.

CONSULTANT shall not employ discriminatory practices in the treatment of persons in relation to the circumstances provided for herein, including assignment of accommodations, employment of personnel, or in any other respect on the basis of race, religious creed, color, national origin, ancestry, physical disability, mental disability, medical condition, marital status, sex, age, or sexual orientation.

Method and Criteria for Selection

The initial review of all proposals will be to evaluate to ensure they meet the following minimum requirements:

- A. The proposal is complete and is in compliance with the RFP.
- B. Prospective firm agrees to meet, **by inclusion of such statement in submittal letter**, all State and Federal requirements included in this RFP.

Failure to meet these requirements may result in the proposal being rejected. No proposal shall be rejected if it contains minor irregularities, defect, or variation of the irregularity; defect or variation is considered by the City to be immaterial or inconsequential. In such case, the Proposer will be notified of the deficiency in the proposal and given the opportunity to correct. The City may elect to waive the deficiency and accept the proposal as submitted.

The City reserves the right to reject any and all proposals submitted, to request clarification of information submitted, to request additional information from any and/or all applicants, and to waive any irregularity in the proposal and review as long as City procedures remain consistent with the State Department of Housing and Community Development procurement requirements.

Cost, while an important factor, will not be the sole determining factor. Proposals will be evaluated based on the following criteria:

Evaluation Score Methodology:	Points Available
Quality of staff and Compliance with RFP Instructions	30
Understanding the Need and Type of Work to be completed	20
Thoroughness of the Scope of Work, milestones, schedule, cost analysis, and approach to completing the project	20
Experience & Qualifications Working with Agencies involved	15
Demonstrated ability to perform tasks/projects in a timely and efficient manner & estimated cost proposal.	15
Total	100

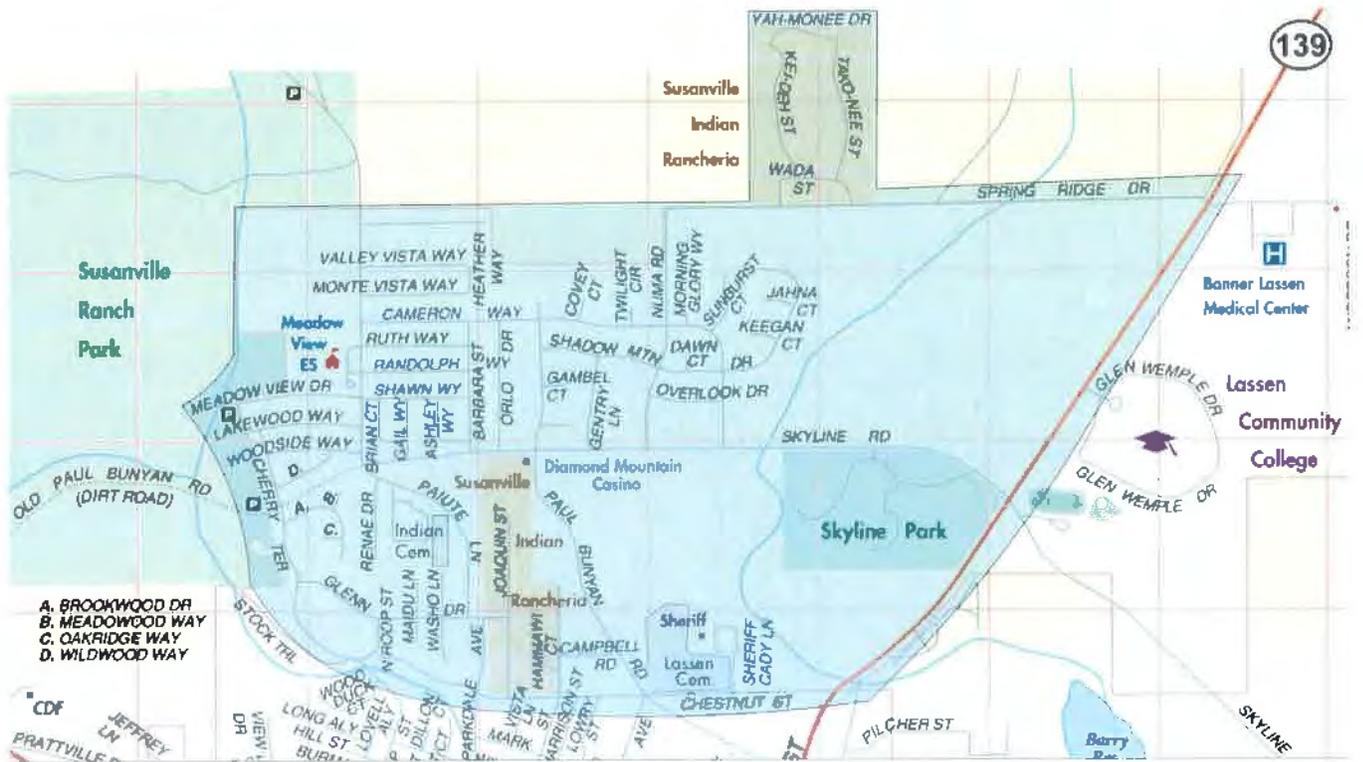
The City may elect to interview the most qualified firms as evidenced by the submitted proposals to negotiate final costs/anticipated effort for each milestone. The contract will be awarded to the proposer who submits the most favorable overall proposal, as determined by the City in its sole discretion, and may be awarded to other than the lowest proposer. Selected CONSULTANT will be required to submit a final cost proposal for City review and determination of award.

The terms and scope of the contract will be determined based on negotiations between the City and the prospective CONSULTANT. If the City and the prospective CONSULTANT fail to reach a contractual agreement, the City may negotiate with any other qualified firm.

After selection of the CONSULTANT, all applicants will be notified of the City's decision.

Exhibit B: Project Map

City of Susanville Area of Paul Bunyan Connectivity Grant Project Influence



6. ADDENDUM

KASL Consulting Engineers Preliminary Design Report



PROJECT REPORT
CITY OF SUSANVILLE
PAUL BUNYAN CONNECTIVITY SUSTAINABLE COMMUNITES GRANT

December 2018

**Master Plan
Prepared For:** City of Susanville
66 N. Lassen Street
Susanville, CA 96130

**Master Plan
Prepared By:** KASL Consulting Engineers, Inc.
7777 Greenback Lane, Suite 104
Citrus Heights, CA 95610



**CITY OF SUSANVILLE PROJECT REPORT
PAUL BUNYAN CONNECTIVITY SUSTAINABLE COMMUNITIES GRANT**

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PAUL BUNYAN CONNECTIVITY SUSTAINABLE COMMUNITIES GRANT**

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- Concept Connectivity Exhibits A-1 through H-2
- Sample Funding Applications



**CITY OF PLYMOUTH
WASTEWATER COLLECTION SYSTEM MASTER PLAN**

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PROJECT REPORT PAUL BUNYAN CONNECTIVITY SUSTAINABLE COMMUNITIES GRANT

I. INTRODUCTION

In March 2017 the City of Susanville executed a Sustainable Transportation Project Agreement with the California Department of Transportation (Caltrans) to provide funding for the preparation of a program ready design and connectivity plan for the northern section of Susanville. The Project area has been identified, and is referred to herein, as the Paul Bunyan Connectivity Grant Project area. The Project area, as shown in **Figure 1**, is bound by Spring Ridge Road and the Susanville Indian Tribal Housing development on the north, State Route 139 on the east, Chestnut Street on the south and Cherry Terrace, the Susanville Ranch Park and Paiute Lane on the west. Most of the existing roadways within the Project serve as residential streets with some functioning as collectors including State Route 139 (SR 139) along the eastern perimeter, a portion of Chestnut Street along the southerly boundary and Skyline Road which represents the central east-west transportation corridor.

CONNECTIVITY PROJECT OBJECTIVES

Objectives of the Paul Bunyan Connectivity Project include:

- Enhance key corridors and address multi-modal connectivity issues
- Enhance the existing multi-modal path for commute and recreation purposes
- Add and improve bike routes and lanes
- Add sidewalk and improve ADA accessibility
- Close existing gaps in the sidewalk network
- Add and improve crosswalks
- Address parking issues at the Susanville Ranch Park
- Monitor the success of multi-modal facilities
- Signage for Out-Of-Towners

SCOPE OF SERVICES

The Scope of Services provided to the City of Susanville to complete the Paul Bunyan Connectivity Study Plan included:

- Compiling Right of Way Maps, property owner information, improvement plans available for the Project area
- Field reviews of Project area transportation conditions including existing roadways, walkways, bike paths, bike trails, ADA compliant ramps, pedestrian crossings, parking issues, drainage issues, pavement conditions
- A high resolution aerial planimetric survey of the Project area developed with horizontal control
- Development of preliminary connectivity plans for improved vehicle, bicycle, pedestrian circulation and access from the City's early public outreach efforts and with direction from City staff regarding key connectivity issues.

CITY OF SUSANVILLE, PAUL BUNYAN CONNECTIVITY
SUSTAINABLE COMMUNITIES GRANT

FIGURE 1

- A presentation of preliminary concept level connectivity plans at a Community Workshop (Susanville City Hall, July 12, 2018)
- Development of Program Ready Concept Connectivity Plans with narrative descriptions, cross sections and aerial photo based exhibits
- Review of funding strategies and grant sources that most closely meet the City's connectivity needs and have the most likely opportunity for successful award
- Preparation of this Project Report which included review and input from City of Susanville Planning and Public Works staff and from other significant Connectivity Plan stakeholders

PROJECT REPORT ORGANIZATION

In response to the City's request and their agreement with Caltrans, this Project Report has been organized as follows:

- I. Introduction
- II. Preliminary Concept Connectivity Plans
- III. July 12, 2018 Community Workshop
- IV. Suggested Program Ready Concept Level Connectivity Plans
- V. Funding Opportunities and Strategies

Attachments

- Concept Connectivity Exhibits
- Sample Funding Applications

II. PRELIMINARY CONCEPT LEVEL CONNECTIVITY PLANS

Sustainable Communities Grants are State funded, Caltrans administered grants that encourage reductions in vehicle miles traveled (VMT), reduced greenhouse gas (GHG) emissions and promote a shift in automobile use to more efficient travel via transit, bicycles and walking. Project goals and objectives listed in Section I of this Report focus potential connectivity plans on key Project corridors. For the Paul Bunyan Connectivity Sustainable Communities Grant the key corridors include the following:

- Skyline Road
- State Route 139 (SR 139); Ash Street locally
- Chestnut Street
- Cherry Terrace
- Paiute Lane
- Numa Road
- Spring Ridge Road
- Paul Bunyan Road
- Paul Bunyan Logging Trail
- N. Roop Street, Parkdale Lane, Glenn Drive
- Chestnut Street / Grand Avenue / Paul Bunyan Road Intersection

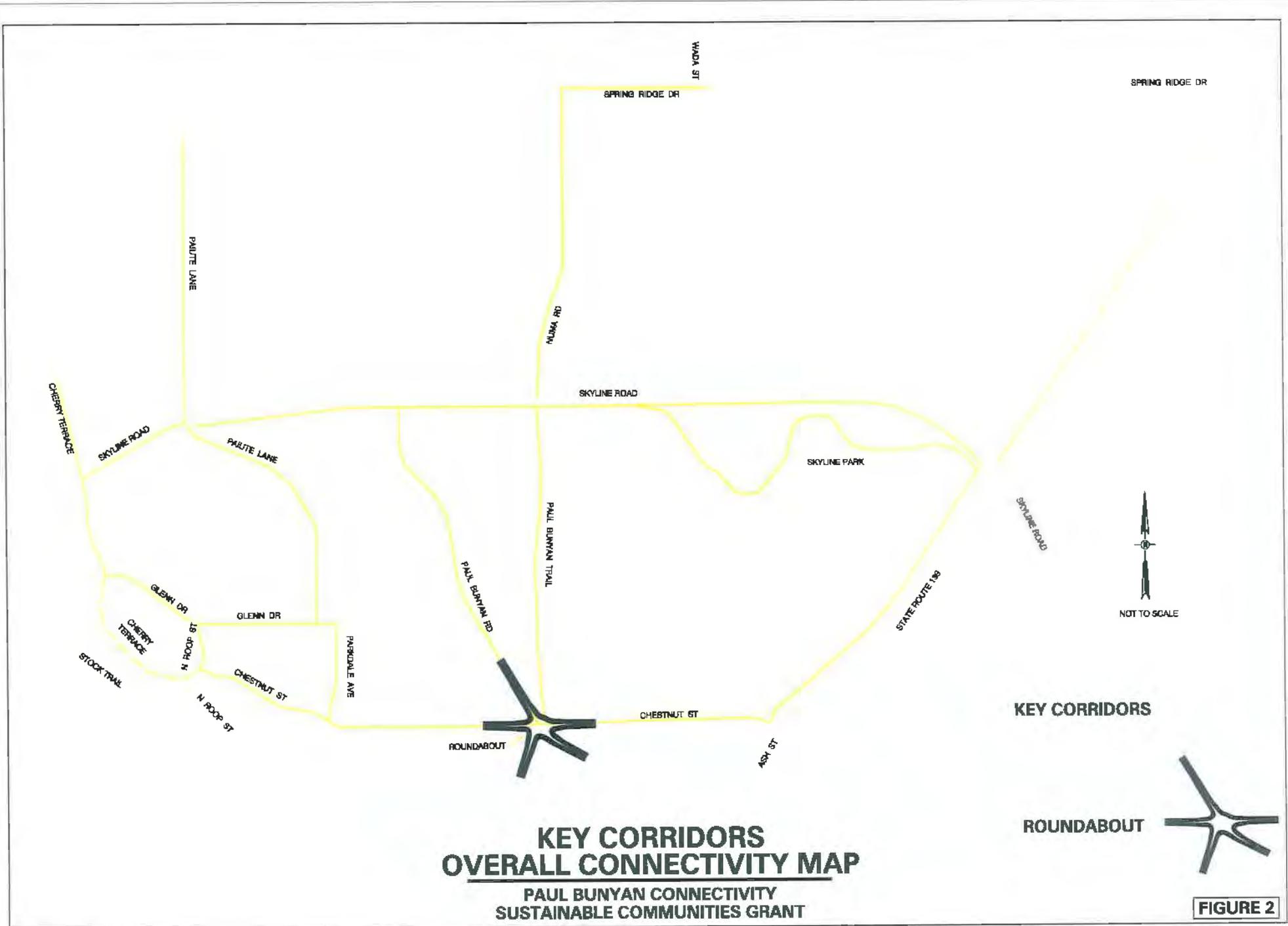
Key corridors within the Paul Bunyan Project area are presented in **Figure 2**.

KEY CONNECTIVITY FEATURES

Key connectivity features for the corridors considered in concept plan development include:

Skyline Road / Radical Road

- Skyline Road / Radical Road is the central east-west connector for the Project Area.
- Connectivity improvements should “build on” the existing Skyline Park Class I Bike Trail.
- Skyline Road / Radical Road improvements would connect Susanville Ranch Park on the west with Skyline Park on the east.
- Skyline Road provides connectivity to the centrally located Diamond Mountain Casino. This is the employment, retail and entertainment center of the Project area.
- Skyline Road provides connectivity to Lassen Community College on the east.
- Bike path improvements on Skyline Road are consistent with, and would implement provisions of, the Lassen County Bikeway Master Plan.
- Improvements to Radical Road could include one way traffic (westbound) and on-street parking for Susanville Ranch Park.
- A Fire Hall / Outside Fitness Center is envisioned along Skyline Road on the north side of Skyline Park.



**KEY CORRIDORS
OVERALL CONNECTIVITY MAP**
PAUL BUNYAN CONNECTIVITY
SUSTAINABLE COMMUNITIES GRANT

KEY CORRIDORS

ROUNDABOUT



FIGURE 2

(SR 139) Ash Street

- SR 139 is a key north – south connector for the Project area.
- SR 139 provides links to Project area connections at Chestnut Street, Skyline Road and Spring Ridge Drive.
- A SR 139 Bike Trail could provide bicycle / pedestrian connectivity for Lassen Community College and Banner Lassen Medical Center.
- An existing safe crossing exists with the signalized intersection at Skyline Road.
- Bike path improvements suggested along SR 139 would be consistent with recommendations included in the Lassen County Bikeway Master Plan.

Chestnut Street

- Chestnut Street serves as access for County buildings and retail areas located along the south limits of the Project area
- Chestnut Street connects with SR 139, Paul Bunyan Road, Parkdale Avenue / Paiute Lane and Cherry Terrace.
- A central "Hub" of the Paul Bunyan Connectivity Project area is the Paul Bunyan Road / Grand Avenue / Chestnut Street Intersection.
- Critical access to employment, retail and community services is provided by Chestnut Street.
- There are potential R/W issues, east side of Chestnut Street between Paul Bunyan Road and SR 139.
- Improved sidewalk connectivity is needed at a minimum.

Cherry Terrace

- Cherry Terrace serves as the westerly limit and western connectivity corridor for the Project area.
- R/W issues may limit connectivity options and require further analysis.
- Alternative west side connectivity may be available via Glenn Drive, N. Roop Street and Parkdale Avenue.

Paiute Lane

- Paiute Lane serves Meadow View School.
- Reduced speeds, traffic calming, improved safe pedestrian and bicycle access for school children to / from Meadow View Elementary is needed.
- South of Meadow View Elementary School, Paiute Lane improvements could provide needed connectivity to Chestnut Street via Glenn Drive and Parkdale Avenue.

Spring Ridge Road / Numa Road

- Spring Ridge Road and Numa Road improvements could provide critical north-south connectivity for the northern half of Project Area.
- These are relatively new streets and serve new North Susanville residential areas and the Susanville Indian Rancheria housing tract.
- These streets would function as effective connectors with better bicycle and pedestrian facilities.
- At a minimum, complete continuous sidewalks, one side, to / from Skyline Road are needed.

Paul Bunyan Road

- Paul Bunyan Road is the key north-south connector for the south half of the Project area.
- Traffic calming, reduced vehicle speeds, improved safety and visibility for bicyclists and pedestrians are needed.
- Paul Bunyan Road serves as a connector to centrally located Diamond Mountain Casino. This is the employment, retail and recreation center of the Project area.
- Sidewalks, bicycle facilities, street lights, safe crosswalks are needed along Paul Bunyan Road.

Paul Bunyan Logging Trail

- "Coupled" with Paul Bunyan Road, the Paul Bunyan Logging Trail now serves as a default "cut through" walkway from Skyline Road to Chestnut Street.
- The existing unimproved trail could be improved as a Class I Bike Path / Multi-Use Trail with paving and lighting
- R/W available for pedestrian / bicycle access along the Paul Bunyan Logging Trail needs to be verified.

N. Roop Street, Parkdale Lane, Glenn Drive

- Pedestrian and bicycle improvements along these streets located in the southwest area of the Project area would provide connectivity from Chestnut Street to Cherry Terrace and to Skyline Road and could serve as an alternative for southwest area connectivity
- Connectivity focus in this area is sidewalks, bike lanes, safe, high visibility crosswalks.

Chestnut Street / Grand Avenue / Paul Bunyan Road Intersection

- This intersection is a key Connectivity "Hub" for the Project area.
- To accommodate trucks, busses, delivery vehicles with roundabout improvements, additional R/W will be needed.
- A Roundabout at this location may be considered as a longer term improvement that would need more input from property owners, public works, utility companies.

CONNECTIVITY IMPROVEMENTS CONSIDERED

Concept level connectivity improvements considered for the Paul Bunyan Sustainable Communities Grant Project area include the following:

Roadway Connectivity Improvements

- Striping
- Traffic Calming Measures
- High Visibility Crosswalks / Markings
- Lighting
- Roadway Signs
- Intersection Improvements
- Pavement Rehabilitation
- Drainage Improvements

Bikeway Connectivity Improvements

- Class I Bike Paths / Multi-Use Paths
- Class II Bike Lanes
- Class III Shared Vehicle Lanes
- Class IV Separated Bikeways / Cycle Tracks

Pedestrian Walkway / Path of Travel Connectivity Improvements

- New Sidewalks
- Connecting Sidewalks by Closing “Gaps” in Sidewalk Improvements
- ADA Compliant Ramps
- High Visibility Crosswalks
 - Ladder Type Crosswalk Striping
 - Block Type Crosswalk Striping
 - Enhanced Crosswalks with LED Lit Pedestrian Crosswalk Signs
 - Enhanced Crosswalks with In Roadway Lighting (IRWL) Improvements

EARLY COMMUNITY WORKSHOP INPUT

Prior to initiation of the tasks included in this Project Report, City of Susanville Planning and Public Works staff conducted a Community Workshop with Project area residents, business owners and key stakeholders. Key connectivity suggestions received from this early community workshop are presented in **Figure 3** and are summarized as follows:

- A continuous Bike Path along Skyline Road
- Improvements to the existing Skyline Road Bike Path including crossings at drainage courses
- Parking along Radical Road to serve Susanville Ranch Park
- Bike Lanes along Chestnut Street, east of Paul Bunyan Road
- Safe Routes to School improvements (sidewalks, bike lanes) along Paiute Lane
- High visibility crosswalks on Paiute Lane near Meadow View School
- One way street (southbound) improvements on Cherry Terrace
- One way street (northbound) improvements on Paul Bunyan Road
- Sidewalks, striping, street lights, safety measures on Paul Bunyan Road
- Sidewalks on Numa Road and Spring Ridge Road, Skyline Road to Wada Street
- Bike Path improvements, SR139
- Curb, gutter, sidewalks improvements, both sides of Chestnut Street, Paul Bunyan Road to SR 139 for better access to County Facilities
- Extension of Paul Bunyan Road north across Skyline Road
- Extension of Cameron Way south to connect to Skyline Road
- Walkway improvements at extension of Numa Road to Chestnut Street (Paul Bunyan Logging Trail alignment)
- Roundabout at Chestnut Street / Paul Bunyan Road / Grand Avenue Road intersection

CITY OF SUSANVILLE, PAUL BUNYAN CONNECTIVITY
SUSTAINABLE COMMUNITIES GRANT

Community Input from Earlier Workshops

FIGURE 3

Other notes provided by City staff from the early community workshop included:

- Residents expressed concerns about high speeds and lack of safety along Paiute in the vicinity of the Meadow View School. Traffic calming measures, high visibility crosswalks, bulbouts, raised crosswalks, continuous sidewalks are needed
- In general, participants expressed the need to provide bike paths and bike lanes to connect corridors and retail / employment / recreation sites in the Paul Bunyan Connectivity Project area.

KEY PROJECT STAKEHOLDERS

- Project Area Residents
- Project Area Business Owners
- Meadow View Elementary School
- Diamond Mountain Casino
- Susanville Indian Rancheria
- Lassen Community College
- Banner Lassen Medical Center
- Lassen County and County Supervisors
- Caltrans
- Meadow View Neighborhood Representatives
- Numa Road Neighborhood Representatives
- Susanville City Council
- Susanville Fire Department
- Susanville Public Works / City Engineering
- Susanville Recreation and Parks Department

Input from these stakeholders has been provided to the City and to KASL Consulting Engineers in the development of the preliminary and recommended Concept Level Connectivity Plans presented her for the Paul Bunyan Connectivity Sustainable Communities Grant Project.

III. JULY 12, 2018 COMMUNITY WORKSHOP

On July 12, 2018 a follow up Community Workshop was held at the Susanville City Hall for the Paul Bunyan Connectivity Sustainable Communities Grant Project. Paul Bunyan Project area residents and business owners were specifically invited to participate. A PowerPoint presentation was made by KASL Consulting Engineers and City of Susanville staff. The PowerPoint presentation included a video with an introduction to Class IV Bike Tracks. Alternative and recommended roadway, bikeway and pedestrian way connectivity improvements were presented for each of the Paul Bunyan Community Sustainable Communities Grant Project Corridors itemized in Section II. Colored maps using aerial photos for backgrounds were presented for each of the corridors. Ground level photos were also used to provide street views along each corridor.

At the conclusion of the PowerPoint presentation a question and answer session was held. Specific questions and comments presented during this session included the following:

- Residents questioned whether there was any evidence that roundabouts improve intersection safety for pedestrians and bicycles
- An alternative access to Banner Lassen Medical Center and to Lassen College was promoted by a member of the Banner Lassen Medical Center staff
- Joaquin Street was suggested as a better alignment for a bike route than Parkdale Avenue
- Paul Bunyan Road connectivity and safety improvements were suggested by several attendees as their top priority connectivity improvements
- It was recommended that the City secure the Stock Trail right of way (west of Chestnut Street) as an alternative bike path to access Susanville Ranch Park and to existing mountain bike trails.
- Support for bike trails and sidewalks along Cherry Terrace was voiced.
- Residents want the City to close gaps in sidewalks

In general, there was little support at this time for the Chestnut Street / Paul Bunyan / Grant Avenue Roundabout. There was little support for separate pedestrian improvements along Paul Bunyan Trail east of Paul Bunyan Road. While the residents strongly support improved pedestrian and bicycle connectivity, additional information regarding Class IV bike tracks is needed to support this type of bikeway. Class IV bike tracks appears to be best suited where there are few driveways or intersection conflicts. Skyline Road, east of Paul Bunyan could be a good candidate.

Susanville residents and stakeholders want safe connectivity both for bicyclists and pedestrians. Sidewalk extensions and closing gaps in sidewalks suggested for improved connectivity were well received. Bicycle connectivity is also supported, however, at this point, residents and stakeholders do not have a particular interest in the type of bicycle connectivity (Class I, II, III, IV) as long as adequate safety is provided.

After the "open forum" Q & A session participants were invited to have a closer review of the alternative connectivity improvements displayed for each corridor. At each corridor "station" there were opportunities to ask questions of KASL and City staff and to identify alternative features that they strongly supported, supported, disliked or strongly disliked. A Sample Survey Form is presented in **Figure 4**.

**PAUL BUNYAN CONNECTIVITY SUSTAINABLE COMMUNITIES GRANT
COMMUNITY WORKSHOP**

SKYLINE PARK Connectivity Elements

Strongly Support: _____

Support: _____

No Comment: _____

Do Not Like: _____

Strongly Dislike: _____

Other Comments: _____

COMMUNITY WORKSHOP SURVEY INPUT

Survey input from community workshop attendees included in the following:

Skyline Road / Radical Road

- "I am strongly In favor of connecting Skyline Park to Susanville Ranch Park via Skyline Road for cyclists and pedestrians".
- "Make a paved trail between Paiute Lane and Susanville Ranch Park... but provide vehicle access to neighbor's back yards".
- "Very strongly support grade separated trail built on Skyline (Park) Trail where it meets west side of Highway 139".
- "(A) new class I Bike Path on Skyline road next to Paul Bunyan Road interferes with the use of the land. Currently zoned R-1, but the Susanville Indian Rancheria is proposing a zoning change, possibly put a health clinic on the property".
- "(Strongly support) Honey Lake Valley Rim Trail".

Paul Bunyan Road / Paul Bunyan Logging Trail

- "Skyline to Chestnut connectivity, excellent idea. Sidewalk connectivity throughout Project (area), excellent. Great Bike Paths proposed!"
- "Lights and sidewalks. (Strongly supports). Have seen many students walking along Paul Bunyan Road going to the High School in the winter when it is dark and they have to walk in the roadway".
- "(Complete) Paul Bunyan Road connectivity before constructing separate trail along Paul Bunyan Logging Trail".
- "Build Class 1 bike and pedestrian trail, 10 feet wide, Paul Bunyan Road is highest priority. Lots of use. Dangerous in Fall through Spring when dark in early evening to early morning".

Paul Bunyan / Chestnut Street / Grand Avenue Roundabout

- "Do not like Roundabout, skeptical but new design presented may work". (Note, roundabout design presented at Community Workshop is presented in **Figure 5**).
- "Although in many cases I do like roundabouts, I don't think it's necessary in this location".
- "Do it!" "Provide bike / ped access to trail along (Paul) Bunyan Road.
- "Good idea, but maybe not a priority as we don't have a lot of traffic problems there".

State Route 139 / Ash Street

- "Need Class I grade separated 10 foot wide paved bike / ped trail to college and up to hospital. Lots of walkers and cyclists use shoulder of high speed Highway 139. Make it safe for bike / peds by building grade separated paved trail".
- "I strongly support connectivity from the college to town via SR 139 for both bicyclist and pedestrians".
- "Would be great for college connectivity with time".
- "This is a must for our community".

CITY OF SUSANVILLE, PAUL BUNYAN CONNECTIVITY
SUSTAINABLE COMMUNITIES GRANT

PRELIMINARY ROUNDABOUT DESIGN

FIGURE 5

Paiute Lane

- “Make segment to Meadow View School safer”.
- “(I Support) a high visibility crosswalk by Meadow View School”.

Spring Ridge Road and Numa Road

- “This is needed. I live in this area”.

Cherry Terrace

- (Very Strongly Support) Chestnut to Stock Trail and back to Cherry Terrace via dirt Bunyan Road”.
- “Extend sidewalks along Cherry Terrace to connect to North Roop towards Main Street”.
- “Yes, Pleasefrom uptown to the Susanville Ranch Park and Meadow View School”.

Chestnut Street

- “A very congested (street). This is a must. I ride on Chestnut Street all the time, would like a bike lane”.

North Roop / Glenn Drive

- “Northern Roop to Glenn Drive is too steep will not get much cycling use”.
- “Move bike use to Joaquin and to Glenn Drive”.
- North Roop connectivity, excellent”.
- “(Strongly Supports) Joaquin Road to Glenn Drive to Cherry Terrace. Bicyclists use Joaquin now lowest pavement grade to get up to Cherry Terrace”.

RADICAL ROAD SURVEY

Following the July Community Workshop, Susanville City staff conducted door to door surveys of residents located immediately adjacent to, or near, Radical Road. The purpose of this survey was to identify homeowner’s opinions of suggested connectivity improvements to Radical Road. A copy of the survey form follows this section of the Project Report. A copy of Radical Road improvements similar to Connectivity Exhibit Attachment A-1 (Refer to Report Attachments) was presented to residents. A summary of the survey results received from approximately 40 Radical Road area respondents is as follows:

1. 71% either strongly agree or agree with construction of Radical Road improvements (as shown on Exhibit Attachment A-1).
2. 68% either strongly agree or agree with the construction of roadway improvements on Radical Road.
3. 71% either strongly agree or agree with the construction of a roadway that will carry vehicular / pedestrian and bicycle traffic.
4. 57% either strongly agree or agree with the construction of a trail that will carry only bicycle and pedestrian traffic.

5. 73% strongly agree or agree with the need to improve the City's transportation network for pedestrians, bicycles and motor vehicles connecting Skyline Road to Cherry Terrace.
6. 56% either strongly support or support vehicle traffic on Radical Road but would like the speed limit very much reduced.
7. 61% strongly disagree or disagree that speed limits (on Radical Road) are not important.
8. 32% of the respondents access their property from the existing (unimproved) Radical Road R/W.

MULTI-MODAL TRANSPORTATION SURVEY - Radical Road 2018

For the below questions, please reference the drawing presented by the surveyor.

Please indicate your response to the following questions by circling the most appropriate response:

- 1. Strongly Disagree
- 2. Disagree
- 3. Neither Agree or Disagree
- 4. Agree
- 5. Strongly Agree

1. The conceptual drawing presented represents an improvement that I would like to see constructed.

1	2	3	4	5
<input type="checkbox"/>				

2. I support the construction of a roadway that will carry motor vehicle traffic.

1	2	3	4	5
<input type="checkbox"/>				

3. I support the construction of a roadway that will carry vehicular/pedestrian and bicycle traffic.

1	2	3	4	5
<input type="checkbox"/>				

4. I support the construction of a trail that will carry only bicycle and pedestrian traffic.

1	2	3	4	5
<input type="checkbox"/>				

5. There is a need to improve the transportation network for pedestrians, bicycles and motor vehicles by connecting Skyline Road to Cherry Terrace.

1	2	3	4	5
<input type="checkbox"/>				

6. I support vehicular traffic on Radical Road, but would like the speed limit very much reduced.

1	2	3	4	5
<input type="checkbox"/>				

7. I support vehicular traffic and the speed is not important to me.

1	2	3	4	5
<input type="checkbox"/>				

8. I access the dirt road from my home.

1	2	3	4	5
<input type="checkbox"/>				

9. If these improvements were to happen I will spend more time in my neighborhood.

1	2	3	4	5
<input type="checkbox"/>				

10. I will be more active as a direct result of these improvements.

1	2	3	4	5
<input type="checkbox"/>				

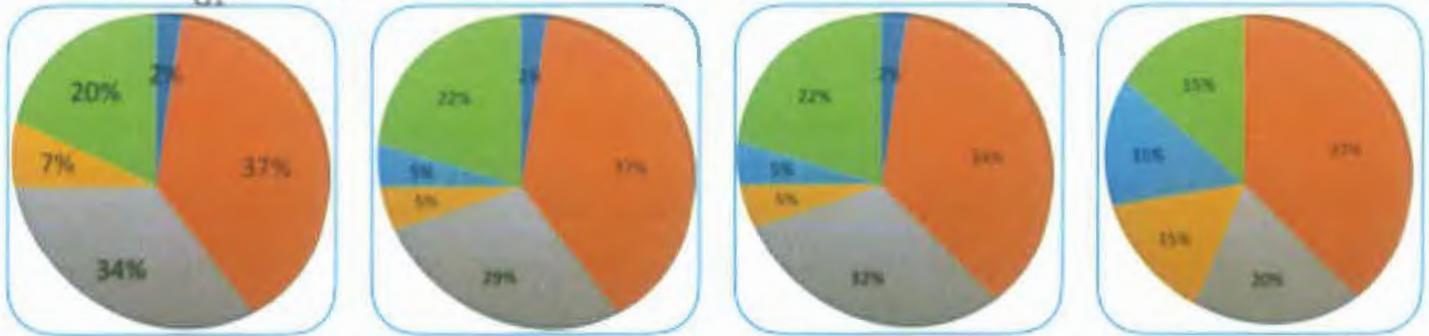
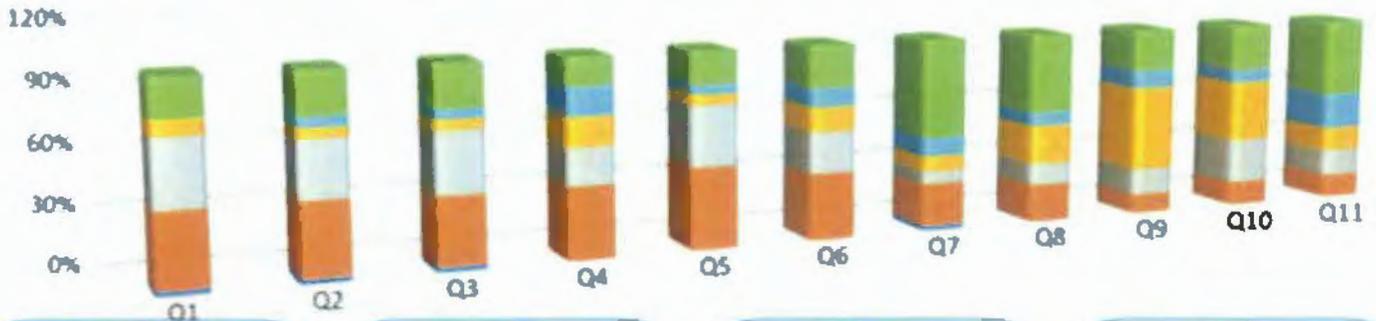
11. I have observed illegal dumping on the dirt road, Radical Rd.

1	2	3	4	5
<input type="checkbox"/>				

12. Please provide any ideas for improvements you would like to see here at "Radical Road."

Results:

■ N/A
 ■ Strongly agree
 ■ agree
 ■ neither
 ■ disagree
 ■ strongly disagree



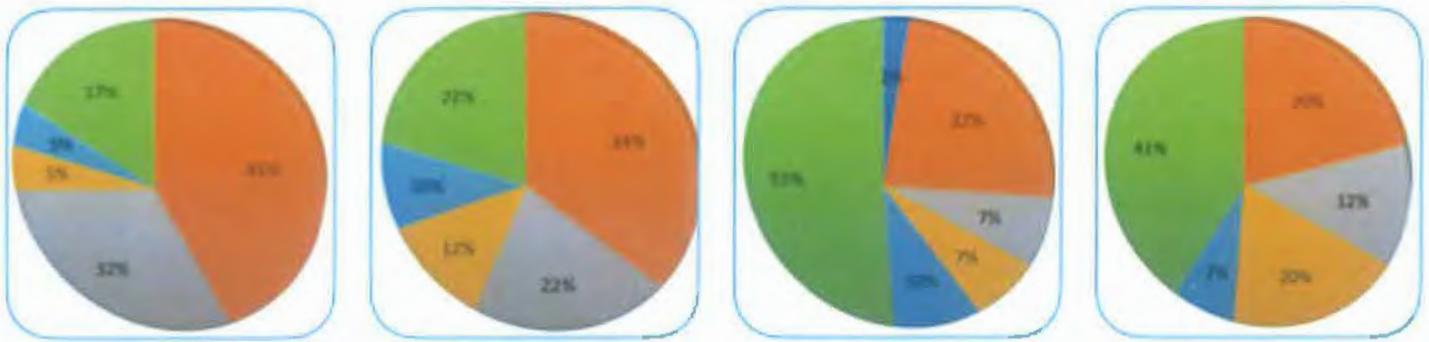
1 The conceptual drawing presented represents an improvement that I would like to see constructed.

1 The conceptual drawing presented represents an improvement that I would like to see constructed.

2 I support the construction of a roadway that will carry motor vehicle traffic.

3 I support the construction of a roadway that will carry vehicular/pedestrian and bicycle traffic.

4 I support the construction of a trail that will carry only bicycle and pedestrian traffic.



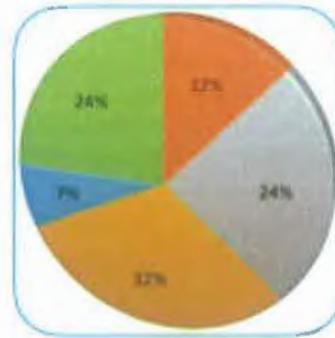
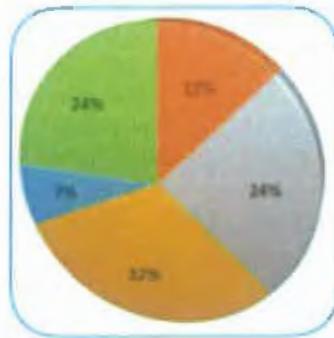
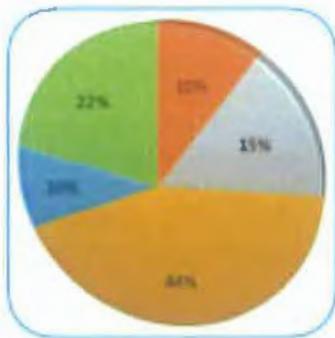
5 There is a need to improve the transportation network for pedestrians, bicycles and motor vehicles by connecting Skyline Road to Cherry Terrace.

5 There is a need to improve the transportation network for pedestrians, bicycles and motor vehicles by connecting Skyline Road to Cherry Terrace.

6 I support vehicular traffic on Radical Road, but would like the speed limit very much reduced.

7 I support vehicular traffic and the speed is not important to me.

8 I access the dirt road from my home.



9

10

11

9 If these improvements were to happen I will spend more time in my neighborhood.

10 I will be more active as a direct result of these improvements.

11 I have observed illegal dumping on the dirt road, Radical Rd.

12 Ideas:

Make it accesible and light up

Walk and biking trail

Other roads need repair

Paved for vehicle and bikes would be a welcom addition

Gutter lines properly installed, Side walks, 2 way traffic roadway

Survey Question	1	2	3	4	5	6	7	8	9	10	11
N/A	2%	2%	2%	0%	0%	0%	2%	0%	0%	0%	0%
Strongly Agree	37%	37%	34%	37%	41%	34%	22%	20%	10%	12%	12%
Agree	34%	29%	32%	20%	32%	22%	7%	12%	15%	24%	15%
Neither	7%	5%	5%	15%	5%	12%	7%	20%	44%	32%	12%
Disagree	0%	5%	5%	15%	5%	10%	10%	7%	10%	7%	20%
Strongly Disagree	20%	22%	22%	15%	17%	22%	51%	41%	22%	24%	41%

IV. SUGGESTED PROGRAM READY CONCEPT LEVEL CONNECTIVITY PLANS

In the following narrative summaries and cross sections are presented the program ready concept level connectivity improvements suggested for the Paul Bunyan Connectivity Sustainable Communities Grant Project area. The following suggested improvements were developed from meetings and discussions with City of Susanville Planning and Public Works staff, early community workshop input, the July 12, 2018 Community Workshop and input provided by Project Stakeholders.

The narratives and cross sections presented in this report are supported by the Exhibit Attachments which follow the Report text.

All of the suggested improvements presented in the following narratives and sections are subject to securing adequate design and construction funding with either State and/or Federal Grants or through the City's Capital Improvement Funds. While design and construction of the suggested connectivity plans could be completed as one Project, it is likely that funding and coordination / approval from, stakeholders (including Susanville Indian Rancheria, Meadow View School, Caltrans, Lassen County) will result in connectivity improvements approved, designed and constructed in several segments or phases.

All of the connectivity improvements presented herein will be subject to detailed engineering design and design review and approval by the Susanville Public Works Department.

**Radical Road
(Refer to Exhibit Attachment A-1)**

As shown in Exhibit Attachment A-1, concept level connectivity improvements on Radical Road, Cherry Terrace to Paiute Lane, include two traffic lanes, each 10 feet wide, a parallel parking lane along the south side of the road and a 10 foot wide Class 1 Bike Lane / Multi-use Trail along the north side. The Class 1 Bike Lane would be separated from traffic lanes by a 7 foot wide buffer and drainage swale. The traffic lanes, parking lane, Class 1 Bike Lane / Multi-use Trail and drainage swale could be constructed within the existing 50 foot wide Right of Way (R/W). Drainage improvements along the north side of Radical Road would be confined to the existing 20 foot wide drainage easement. Retaining walls will be required along a portion of the south R/W to implement the suggested Radical Road improvements and to maintain existing driveways which access rear lots along Brookwood Drive, south of Radical Way. Provisions would be made in the design of retaining walls to permit rear lot drainage to continue north across Radical Road.

Radical Road drainage improvements will likely be needed beginning at the intersection of Paiute Lane, continuing with improvements to the existing drainage ditch on the north side of Radical Road and finishing with improvements to the existing drainage facilities which cross Cherry Terrace. Existing storm drain headwalls and conduits may need to be improved or replaced. The extent of these improvements will be determined with the completion of detailed drainage studies, design level topographic and boundary surveys and the preparation of improvement plans.

Radical Road improvements may also include striping and signing as well as crosswalk improvements at the intersection of Cherry Terrace and Paiute Lane. Concept level cross sections follow this narrative. All improvements would be subject to the successful acquisition of design and construction funding. All improvements would be subject to review and approval by the Susanville Public Works Department.

**Skyline Road, Paiute Lane to State Route 139
(Refer to Exhibit Attachments A-2, A-3, A-4 and A-5)**

Skyline Road is the main east-west connectivity corridor of the Paul Bunyan Connectivity Sustainable Communities Project area. It serves the Diamond Mountain Casino, Skyline Park, Lassen Community Colleges at the east limits and the Susanville Ranch Park and Meadow View Elementary School at the west limits. Suggested connectivity improvements on Skyline Road received strong support from the community and from Project area stakeholders at community outreach meetings.

Skyline Road improvements under consideration include two, 11 to 12 foot wide traffic lanes, Class 2 Bike lanes, each side, City of Susanville infill curb, gutter and sidewalk improvements together with striping, signage and storm drainage facilities. Skyline Road concept level connectivity improvements are shown in Exhibit Attachments A-2, A-3, A-4 and A-5. Concept level sections prepared for suggested Skyline Road improvements follow this narrative.

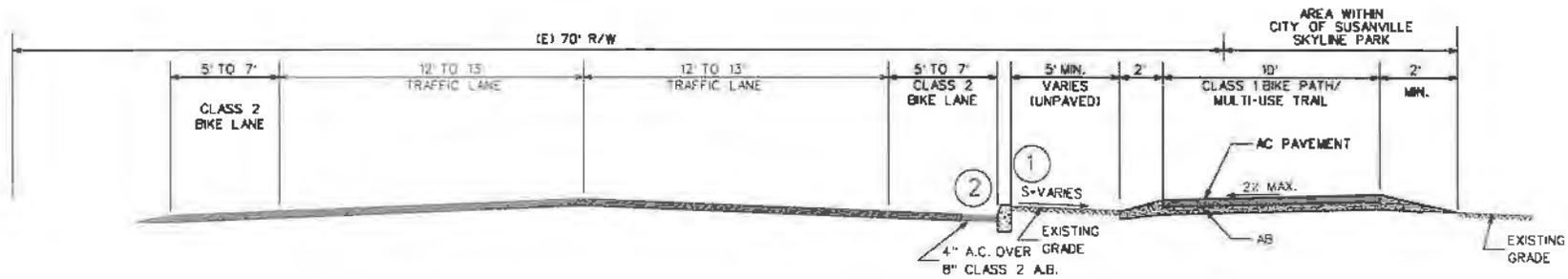
A key element of the Skyline Road Connectivity improvements is enhancement of the existing Class I bikeway which has been constructed through the City's Skyline Park. It is proposed that this facility be reconstructed along its present alignment as a 10 foot wide Class 1 Bike Trail / Multi-Use Path with grade separated structures at existing major drainage crossings. Future improvements at Skyline Park may include a disc golf course, outdoor amphitheater, Dog Park and pump track. These future attractions will further increase the connectivity improvements needed along Skyline Road. The Skyline Road Class 1 Bike Trail is an important element of the Lassen County Bikeway Master Plan adopted by the Lassen County Transportation Commission in November 2011.

With improvements to the existing Skyline Park Bike Trail, extension of this Class I facility west along the south side of Skyline Road is important. Accordingly, suggested Skyline Road improvements west of Skyline Park include the construction of the Class I Bike Path / Multi-Use Trail to Paiute Lane. Concept level sections prepared for suggested Skyline Road improvements follow this narrative. Implementation of Skyline Road connectivity improvements will require cooperation from the Susanville Indian Rancheria and from Lassen County. Additional right of way is needed from these stakeholders to complete the Class I Bike Lane / Multi-Use Trail west of Paul Bunyan Road.

Implementation of Skyline Road connectivity improvements depends and adequate funding and may require successful award to the City a Federal or State Grant Fund. All Skyline Road improvements are subject to review and approval by the Susanville Public Works Department.

Skyline Road (Looking East)

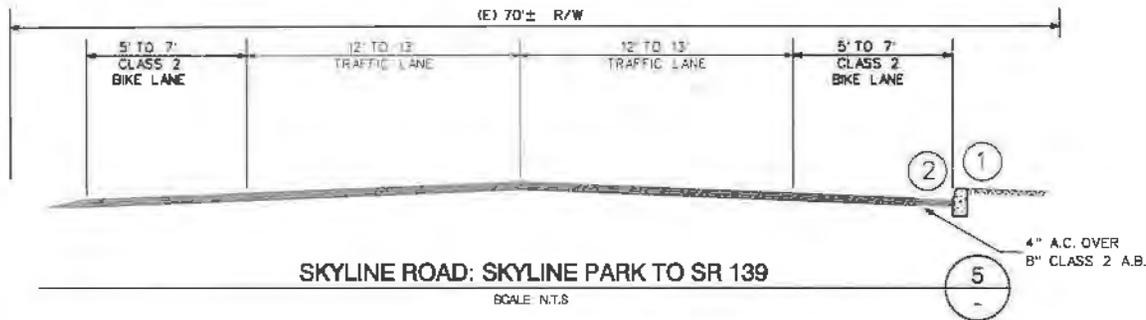
***Existing Class I Bikeway
(Skyline Trail)***



SKYLINE ROAD: NUMA ROAD TO SKYLINE PARK

SCALE: N.T.S

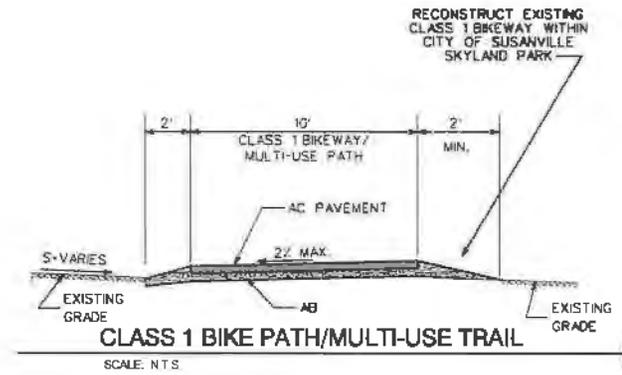
4



SKYLINE ROAD: SKYLINE PARK TO SR 139

SCALE: N.T.S

5



CLASS 1 BIKE PATH/MULTI-USE TRAIL

SCALE: N.T.S

6

NOTES:

- ① FULL DEPTH CONCRETE CURB
- ② ADDITIONAL PAVEMENT AS REQUIRED FOR CURB AND CLASS 2 BIKE LANE
- ③ CONSTRUCT STORM DRAINAGE IMPROVEMENTS PER CITY OF SUSANVILLE STANDARDS

**CONCEPT LEVEL CROSS SECTIONS
SKYLINE ROAD**

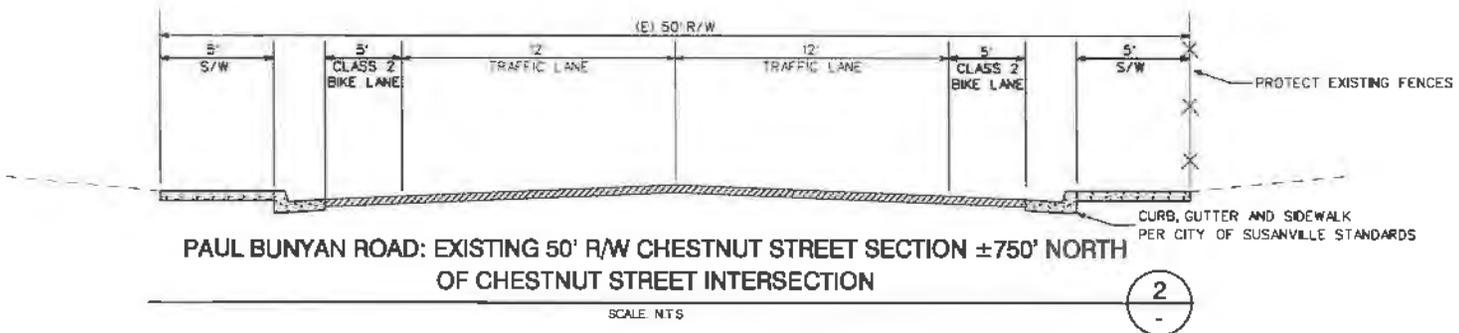
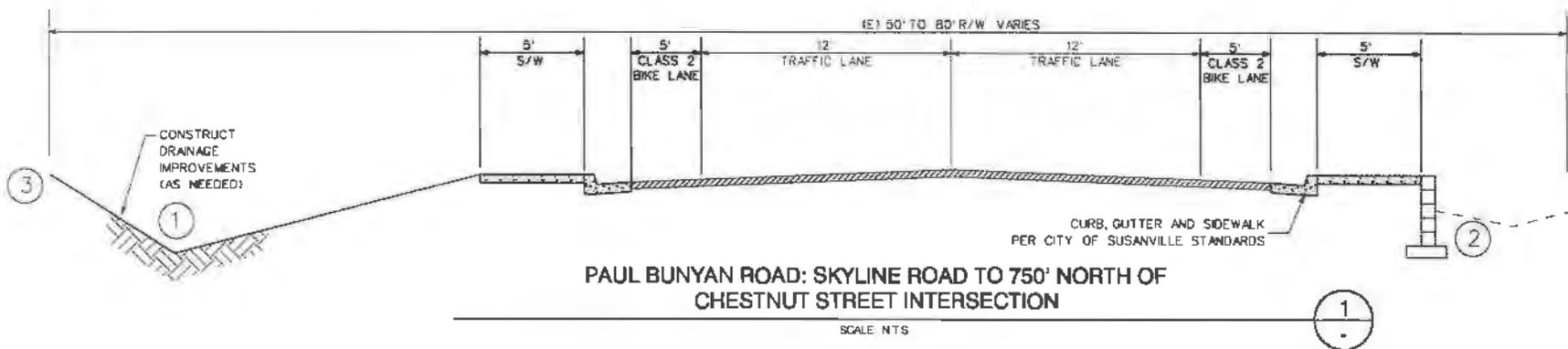
Paul Bunyan Road
(Refer to Exhibit Attachments B-1 and B-2)

Shown in Exhibits B-1 and B2 attached to this report, are concept level connectivity improvements for Paul Bunyan Road between Skyline Road and Chestnut Street. Suggested improvements include two traffic lanes, each 12 feet wide, Class 2 Bike Lanes, each side, City of Susanville Standard curb, gutter and sidewalks, each side, together with striping, signing and storm drainage features. Improvements under consideration also include City of Susanville standard street lights. The need for safe, well lit, travel lanes, bicycle lanes and pedestrian improvements along this section of Paul Bunyan Road was expressed by the public at previous public outreach and community meetings. Proposed improvements could be constructed within the available R/W. Along the northerly portion, available R/W is \pm 80 feet. This R/W width reduces to 50 feet for the southerly 750 feet closest to the intersection with Chestnut Street.

Paul Bunyan Road improvements will need to address conveyance of storm drainage flows. As shown in Exhibit Attachment B-1, there are existing storm drain culverts which cross Paul Bunyan Road south of the Diamond Mountain Casino. Storm drainage needs to be safely conveyed to existing drainage ditches along the east side of the street. Along both the east side and west side of this street, there are potential wetland areas. Retaining wall may be required to avoid fill within jurisdictional limits. The design of storm drainage improvements would be conducted with detailed topographic surveys and drainage studies. The delineation of wetlands would be subject to future environmental determinations. Concept level sections prepared for suggested Paul Bunyan Road improvements follow this narrative. Paul Bunyan Road Project improvements suggested here are subject to the City receiving adequate design and construction funding. All improvements are subject to review and approval by the Susanville Public Works Department.

Paul Bunyan Rd (Looking South)

Paul Bunyan Rd (Looking South)



NOTES:

- ① DRAINAGE IMPROVEMENTS SUBJECT TO APPROVED DRAINAGE STUDY FINDINGS AND RECOMMENDATIONS
- ② CONSTRUCT RETAINING WALL (AS NEEDED) TO PROTECT EXISTING WETLAND AREAS, WEST SIDE, EAST SIDE
- ③ CATCH POINT TO BE RETAINED WITHIN EXISTING R/W AND DETERMINED WITH DESIGN SURVEYS AND APPROVED PLANS
- ④ STREET LIGHTS PER CITY OF SUSANVILLE STANDARDS

**CONCEPT LEVEL CROSS SECTIONS
PAUL BUNYAN ROAD**

**PAUL BUNYAN
ROAD SECTIONS**

Paiute Lane
(Refer to Exhibit Attachments C-1, C-2, C-3 and C-4)

Paiute Lane connectivity improvements are important to the Paul Bunyan Connectivity Sustainable Communities Grant Project because they represent the principal north-south connectivity corridor along the west side of the Project area. Paiute Lane improvements are also very important because they directly serve the Meadow View Elementary School. Meadow View Elementary School provides public school education for 3rd to 5th grade students throughout the Susanville School District. All Susanville School District 3rd to 5th grade public school students are either driven, bike or walk to this school. The focus of improvements suggested along Paiute Lane, therefore, includes connectivity, traffic calming and safe routes to school features. Concept level Paiute Lane improvements are shown in Exhibit Attachments C-1, C-2, C-3 and C-4.

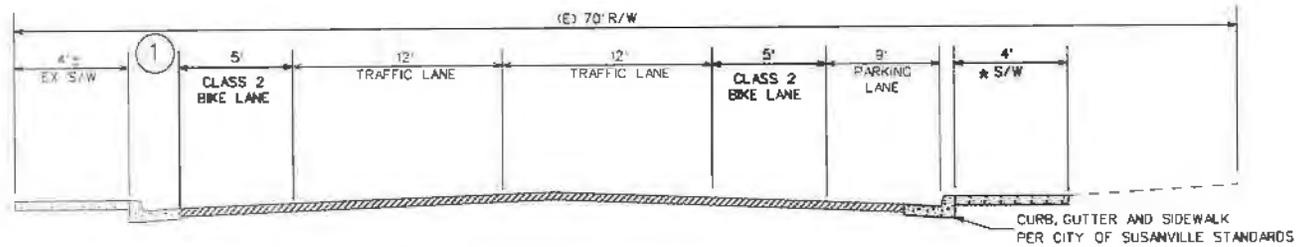
To accommodate adequate vehicle lanes, Class 2 bike lanes and sidewalks from the north end of Paiute Lane to Glenn Drive, parking lane restrictions may be required along portions of this roadway. On-street parking restrictions will require action by the Susanville City Council and amendments to existing Susanville Municipal Codes. Continuous sidewalk improvements, both sides of Paiute Lane, will require infill, curb, gutter and sidewalk improvements together with storm drainage facilities, signing and striping. At each intersection, new sidewalk improvements with curb ramps are proposed in conformance with ADA requirements.

In the vicinity of Meadow View Elementary School, raised crosswalks and bulbouts are currently suggested at the intersection of Monte Vista Way and the intersection of Randolph Way. The purpose of these improvements is to provide traffic calming and safe, high visibility, pedestrian crossing in the vicinity of Meadow View Elementary School. Proposed raised crosswalk improvements are subject to review and approval by the Susanville Fire Department. The proposed location of the raised crosswalks and bulbouts are subject to review and approval by the Meadow View Elementary School Administration.

Concept level sections prepared for suggested Paiute Lane improvements follow this narrative.

With the suggested Connectivity, Traffic Calming and Safe Routes to School elements of the Paiute Lane improvements, an Active Transportation Program (ATP) grant is a likely nexus between the City's and School District's needs and available grant funds.

Paiute Lane improvements suggested in the exhibit attachments and in the following cross sections are subject to review and approval by the Susanville Public Works Department.

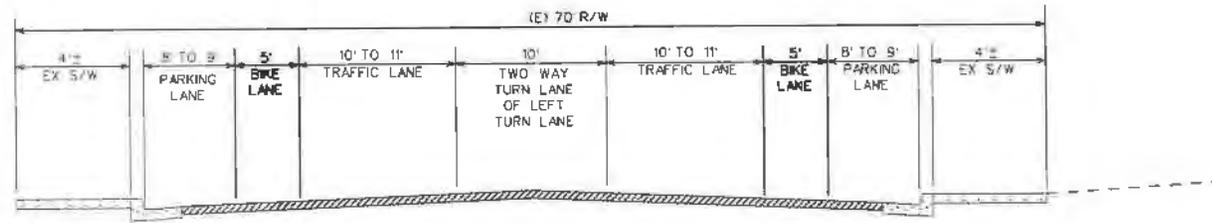


PAIUTE LANE: NORTH END TO CAMERON WAY

SCALE: N.T.S. * BEGIN SIDEWALK AT MONTE VESIA WAY

1

TYPICAL SECTION



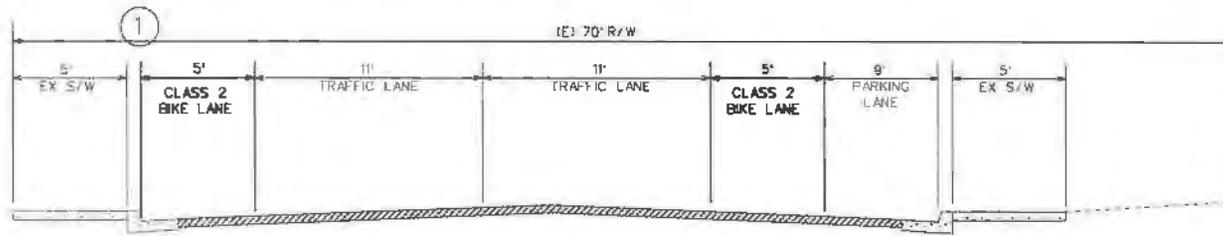
PAIUTE LANE: CAMERON WAY TO SHAWN WAY

SCALE: N.T.S.

2

NOTES:

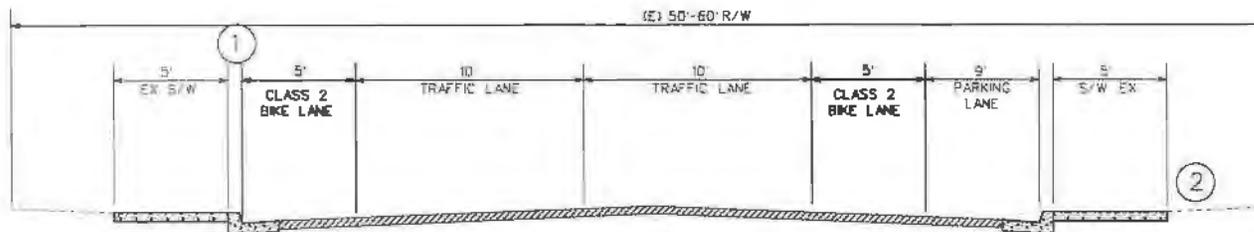
- ① ONSTREET PARKING RESTRICTION SUBJECT TO CITY OF SUSANVILLE ACTION AND AMENDMENT(S) TO THE SUSANVILLE MUNICIPAL CODE



PAIUTE LANE: SHAWN WAY TO SKYLINE ROAD

SCALE: N.T.S

3
-



INFILL CURB, GUTTER AND SIDEWALK PER CITY OF SUSANVILLE STANDARDS

PAIUTE LANE: SKYLINE TO GLENN DRIVE

SCALE: N.T.S

4
-

NOTES:

- ① ON STREET PARKING RESTRICTION SUBJECT TO CITY OF SUSANVILLE ACTION AND AMENDMENT(S) TO THE SUSANVILLE MUNICIPAL CODE
- ② DRAINAGE IMPROVEMENTS TO BE INSTALLED PER CITY OF SUSANVILLE STANDARDS

**CONCEPT LEVEL CROSS SECTIONS
PAIUTE LANE**

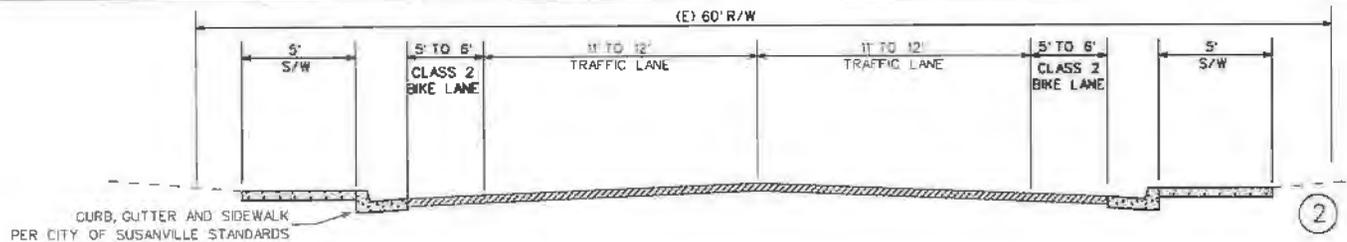
Numa Road, Skyline Road to Ridge Road and Spring Ridge Road, Numa Road to Wada Street (Refer to Exhibit Attachments D-1 and D-2)

Connectivity improvements on Numa Road and on Spring Ridge Road are important to provide improved bicycle and pedestrian access to/from the north end of the Paul Bunyan Connectivity Improvement Project area to the central Skyline Road corridor. With the suggested connectivity improvements, Numa Road, from Skyline Road to Spring Ridge Road, would include two traffic lanes 10 ½ to 12 feet wide, each, Class 2 Bike Lanes, each side, and, with the completion of curb, gutter and sidewalk improvements by others along the Community Evangelical Free Church property and Susanville Indian Rancheria properties, a continuous safe path for pedestrians to and from the Susanville Indian Rancheria Housing to employment centers at the Diamond Mountain Casino and to the Meadow View Elementary School and Skyline Park.

Connectivity improvements along Numa Road and Spring Ridge Road are supported by residents of northern Susanville and by the Susanville Indian Rancheria. Concept level cross sections of proposed roadway improvements follow this narrative. All Numa Road and Spring Ridge Road improvements would be subject to available design and construction funding and all improvements to the public roadways would be subject to review and approval by the Susanville Public works Department.

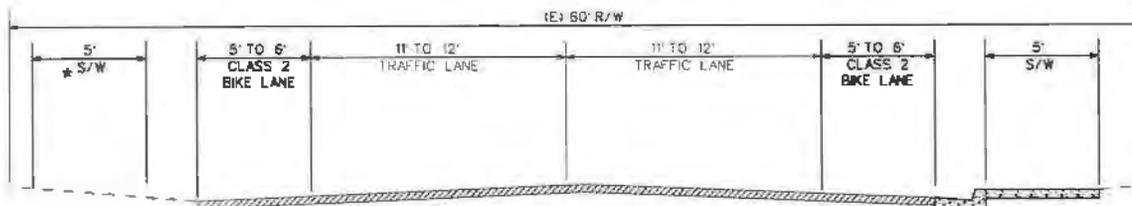
Numa Rd (Looking North)

Spring Ridge Dr (Looking East)



NUMA ROAD: SKYLINE ROAD TO CAMERON WAY

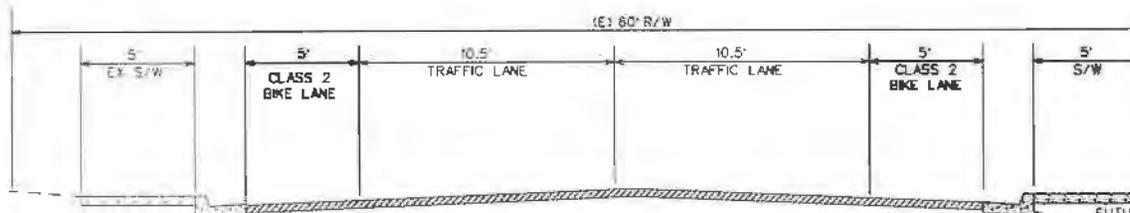
SCALE: NTS



NUMA ROAD: CAMERON WAY TO SPRING RIDGE ROAD

SCALE: NTS

* NEW CURB GUTTER & SIDEWALK BEGIN 315LF NORTH OF CAMERON WAY



SPRING RIDGE ROAD: NUMA ROAD TO WADA STREET

SCALE: NTS

NOTES:

- ① "FUTURE" IMPROVEMENTS BY ---- CHURCH AND SUSANVILLE INDIAN RANCHERIA
- ② DRAINAGE IMPROVEMENTS TO BE INSTALLED PER CITY OF SUSANVILLE STANDARDS

**NUMA ROAD &
SPRING RIDGE ROAD
SECTIONS**

**CONCEPT LEVEL CROSS SECTIONS
NUMA ROAD & SPRING RIDGE ROAD**

**State Route 139, Chestnut Street to Ridge Road
(Refer to Exhibit Attachments E-1, E-2, & E-3)**

State Route 139 (SR 139) forms the eastern boundary of the Paul Bunyan Connectivity Project Area. Connectivity improvements are important along this corridor because they would connect the Lassen Community College and the Banner Lassen Medical Center, both major destination and employment centers, with the rest of the Paul Bunyan Connectivity Project area. SR 139 also connects these destination and employment centers with Central Susanville commercial and residential areas to the south. As shown in the Concept Connectivity Exhibit Attachments E-1, E-2 and E-3, a Class 1 Bike Path / Multi-Use Path is proposed along the west side of State Route 139 south of Skyline Road and Chestnut Street and along the east side of State Route 139 from Skyline Road to Ridge Road. The Class 1 Bike Path / Multi-Use Path suggested along State Route 139 would connect with the City of Susanville's Class I Bike Path / Multi-Use Path at Skyline Road and would connect to the Lassen County Class I Bike Path which starts at State Route 139 and continues east and south to Johnsonville Road. These existing Class I Bike Paths are included in the Lassen County Bikeway Master Plan.

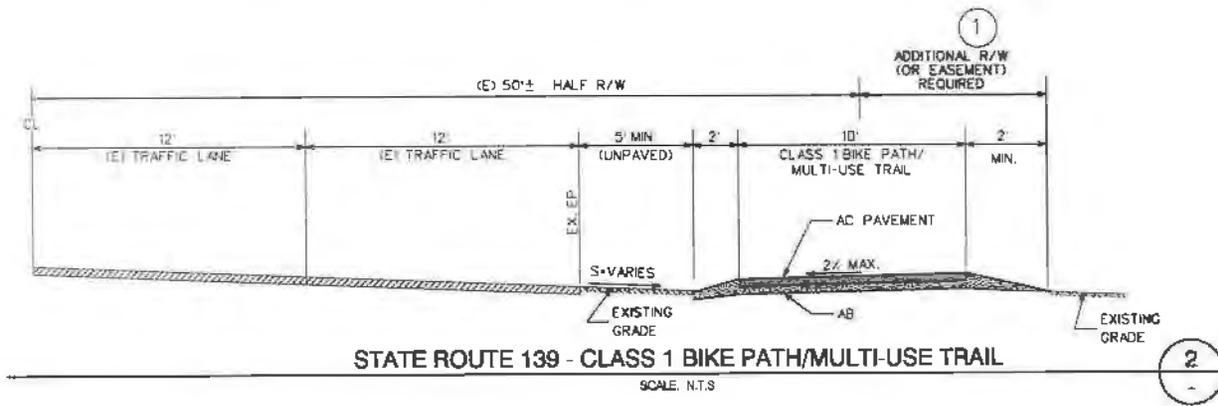
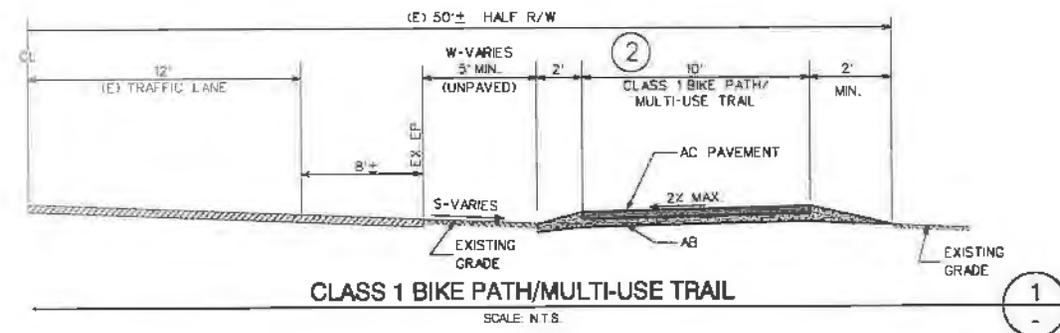
Per Caltrans Standards, Class I Bike Paths must be separated by not less than 5 feet from the edge of traveled highway roadway. Right of way is available to provide this separation except at the northerly limits of State Route 139 near Ridge Road. In this area, additional right of way will need to be obtained from the Banner Lassen Medical Center. If additional right of way cannot be obtained from the Medical Center a physical barrier would be needed between the edge of traveled way and the Class I Bike Path to prevent bicyclists and pedestrians from encroaching onto roadways.

Relatively level ground is available to construct the Class I Bike Path / Multi-Use Path along the east side of State Route 139 from Skyline Road to Ridge Road. This is not true for the section of SR 139 between Chestnut Street and Skyline Road. An alignment along the west side of SR 139 is proposed in this area but construction will require extensive grading to cut into the existing embankment which exists along the west side. Typically, the maximum grade recommended for Class I Bike Paths is 5%. This is also the maximum grade to maintain an ADA accessible path of travel.

Preliminary discussions have been conducted with Caltrans representatives regarding constructing a Class I Bike Path/ Multi-Use Path within the SR 139 right of way. Improvement plans would be subject to review and approval by Caltrans District 2 and a Caltrans Encroachment Permit will be required. Because of the opportunities for alternative transportation modes and reduced vehicle miles, Caltrans representatives suggested that either ATP (Active Transportation Program) STIP (Strategic Transportation Infrastructure Program) or CMAQ (Congestion Mitigation and Air Quality) funding may be available to the City for the State Route 139 Class I Bike Path / Multi-Use Path.

In addition to Caltrans approval, the SR 139 Class I Bike Path / Multi-Use Path improvements would be subject to review and approval by the Susanville Public Works Department. A typical section of the suggested SR 139 Class I Bike Path/ Multi-Use Path follows this narrative.

CA-139 (Looking North)



NOTES:

- ① R/W COORDINATION WITH BANNER LASSEN MEDICAL CENTER
- ② CALTRANS ENCROACHMENT PERMIT REQUIRED

**CONCEPT LEVEL CROSS SECTIONS
STATE ROUTE 139**

**STATE ROUTE 139
ROAD SECTIONS**

**Glenn Drive, Cherry Terrace to Parkdale Avenue
(Refer to Exhibit Attachment F-1)**

There are "connectivity" challenges in the southwest portion of the Paul Bunyan Connectivity Improvement Project area along Chery Terrace between Glenn Drive and Chestnut Street. Cherry Terrace in this area has limited rights of way available to provide Class 2 Bike Lanes and Susanville Standard sidewalks in addition to maintaining adequate traffic lanes. To provide an alternative route for west side connectivity, improvements are suggested along Glenn Drive from Cherry Terrace to Parkdale Avenue. Existing right of way is available along Glenn Drive to provide two, 10 to 10 ½ foot wide traffic lanes, Class 2 Bike Lanes, each side, an 8 to 9 foot wide parking aisle and the construction of infill curb, gutter and sidewalk improvements. Storm drainage, traffic striping and signage in accordance with City of Susanville Standards would also be included in the suggested improvements. To complete connectivity to the southerly limits of the Project area, improvements along Parkdale Avenue are also suggested.

Glenn Drive (Looking East)

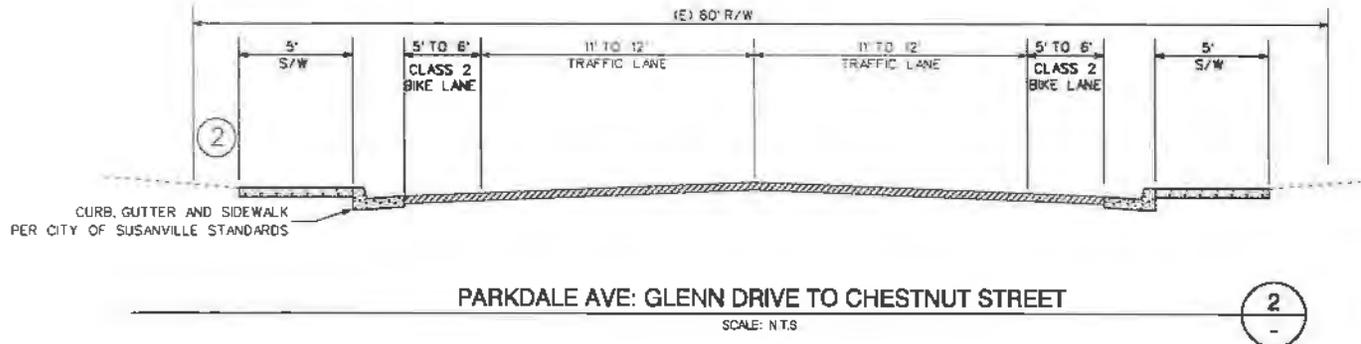
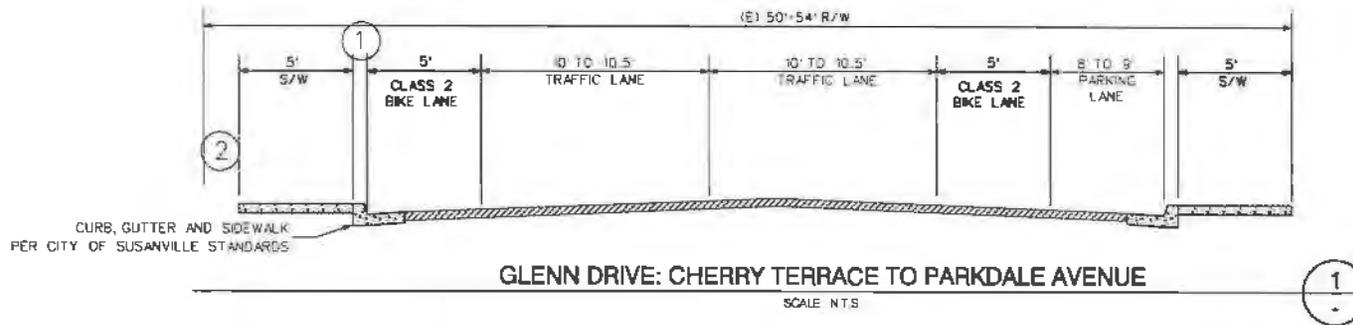
**Parkdale Avenue, Glenn Drive to Chestnut Street
(Refer to Exhibit Attachment F-2)**

Connection to the Glenn Drive connectivity improvements suggested above could occur along Parkdale Avenue from Glenn Drive to Chestnut Street. Similar to Glenn Drive, there is adequate right of way available on Parkdale Avenue to provide two, 11 to 12 foot wide traffic lanes, 5 to 6 foot wide Class 2 Bike Lanes, each side, and City of Susanville Standard infill curbs, gutter and sidewalk improvements. Storm drainage, striping and signing improvements are also suggested in conformance with City of Susanville Standards.

At one of the community workshops conducted for the Paul Bunyan Connectivity Improvements, Joaquin Street was suggested as a better bike route than Parkdale Avenue. There is only ± 23 feet of pavement width on Joaquin Street between existing curb, gutter and sidewalk improvements. Class 2 Bike Lanes, (two, each 5 foot wide minimum) could not be placed on Joaquin Street and still provide adequate width for two, 10 foot wide minimum traffic lanes. Class 3 Bike Route sharrows are suggested for Joaquin Street and for Glenn Drive from Joaquin Street to Parkdale Avenue as shown on attached Exhibit F-2.

Connectivity improvements suggested here for Glenn Drive, Parkdale Avenue and Joaquin Street are subject to available design and construction findings and are subject to review and approval by the Susanville Public Works Department

Concept level cross sections for Glenn Drive and Parkdale Avenue follow the narrative.



NOTES:

- ① ON STREET PARKING RESTRICTION SUBJECT TO CITY OF SUSANVILLE ACTION AND AMENDMENT(S) TO THE SUSANVILLE MUNICIPAL CODE
- ② DRAINAGE IMPROVEMENTS TO BE INSTALLED PER CITY OF SUSANVILLE STANDARDS

**CONCEPT LEVEL CROSS SECTIONS
GLENN DRIVE & PARKDALE AVE**

**GLENN DRIVE &
PARKDALE AVE
ROAD SECTIONS**

Chestnut Street
(Refer to Exhibit Attachments G-1, G-2 and G-3)

Chestnut Street is the southern boundary of the Paul Bunyan Connectivity Improvement Project area. Connectivity improvements along Chestnut Street would connect with proposed improvements on Paul Bunyan Road, State Route 139 and N. Roop Street / Cherry Terrace. West of Paul Bunyan Road, Chestnut Street mostly fronts single family residences. East of Paul Bunyan Road to State Route 139, Chestnut Street is a commercial roadway and serves Lassen County Human Resource Development and the County Sheriffs offices.

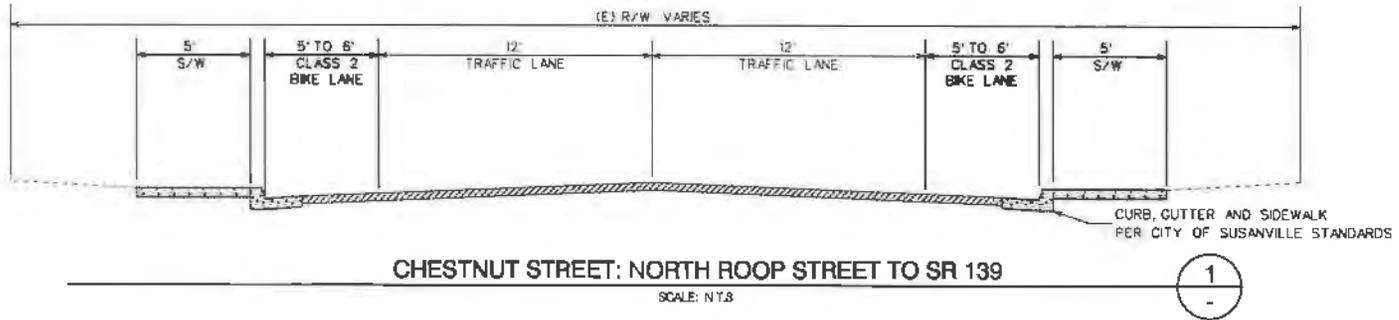
For the purpose of this concept level plan, suggested Chestnut Street improvements are limited to 2 traffic lanes varying in width from 10 to 12 feet, Class 2 Bike Lanes, each side and City of Susanville Standard infill curb, gutter and sidewalk improvements, each side, together with striping, signing and storm drainage facilities.

There are opportunities for additional connectivity improvements on Chestnut Street between Paul Bunyan Road and State Route 139. Available right of way mapping is, however, limited in this area and additional, in depth, map research and monument recovery is needed to evaluate other connectivity improvements that could be implemented. Concept level Exhibit Attachments prepared for Chestnut Street are presented in Exhibits G-1, G-2 and G-3. Concept level typical section follow this narrative.

The 5 way intersection at Chestnut Street, Paul Bunyan Road and Grand Avenue was evaluated for roundabout improvements. Preliminary roundabout plans were reviewed with participants at the community workshops held for the Paul Bunyan Connectivity Planning Project. There was, however, little interest generated by the public in improving this intersection with a roundabout.

Chestnut St (Looking East)

Chestnut St (Looking East)



NOTES: _____

- ① CONNECTIVITY IMPROVEMENTS SUBJECT TO FURTHER DETAILED DETERMINATION OF AVAILABLE R/W

**CONCEPT LEVEL CROSS SECTIONS
CHESTNUT STREET**

**CHESTNUT STREET
ROAD SECTIONS**

**Cherry Terrace, N. Roop Street
(Refer to Exhibit Attachments H-1 and H-2)**

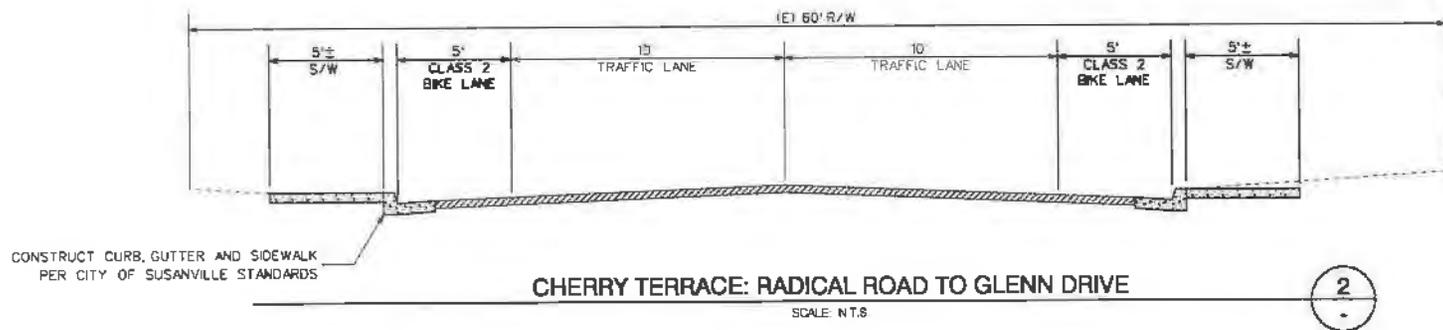
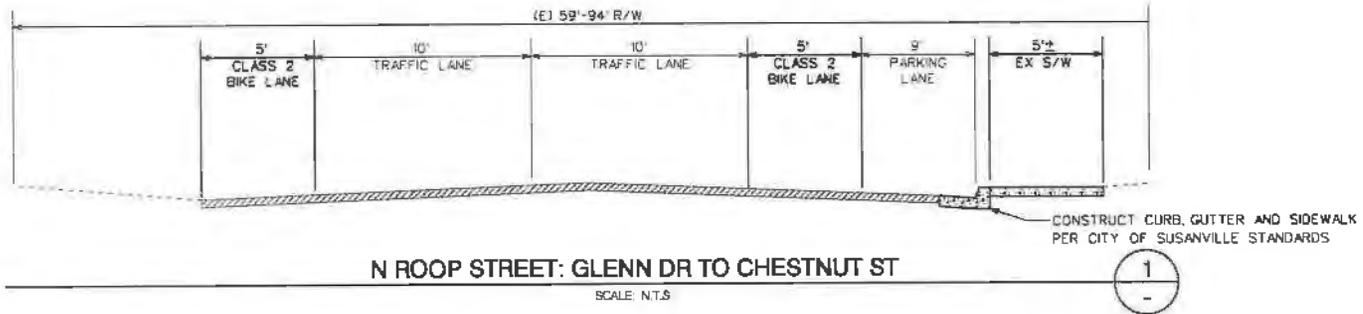
As previously noted Cherry Terrace between Chestnut Street and Glenn Drive has limited available right of way. This is a single family residential street and there is no suggestion that right of way be acquired from existing single family parcels to implement connectivity improvements. Suggested improvements on Cherry Terrace between Chestnut Street and Glenn Drive are limited to Class 3 Bike Route sharrows that can be marked along existing paved surfaces without impact to existing properties, improvements or rights of way. Additional right of way investigations are needed to implement more formal connectivity improvements for this portion of Cherry Terrace.

To provide pedestrian connectivity between Chestnut Street and Cherry Terrace north of Glenn Drive a continuous path of travel with City of Susanville Standard curb, gutter and sidewalk infill improvements is suggested along N. Roop Street and Glenn Drive. These suggested connectivity improvements are shown in Exhibit Attachments H-1 and F-1. Typical sections follow this narrative.

On Cherry Terrace Road between Radical Road and Glenn Drive there is adequate right of way to allow two, 10 foot wide traffic lanes, 5 foot wide Class 2 Bike Lanes, each side and City of Susanville Standard curb, gutter and sidewalk infill improvements. Improvements would also include striping, signage and City of Susanville Standard storm drainage features. With these improvements vehicle, bicycle and pedestrian connectivity would be available along the west perimeter of the Paul Bunyan connectivity improvement Project area. Cherry Terrace provides access to the Susanville Ranch Park and to park area parking lots.

Cherry Terrace (Looking Northwest)

N Roop St (Looking South)



NOTES:

- ① CONNECTIVITY IMPROVEMENTS SUBJECT TO FURTHER DETAILED DETERMINATION OF AVAILABLE R/W

**CONCEPT LEVEL CROSS SECTIONS
N ROOP STREET**

**N. ROOP &
CHERRY TERRACE
ROAD SECTIONS**

PRIORITIZATION OF SUGGESTED CONNECTIVITY IMPROVEMENTS

The “Top 9” connectivity improvements suggested for the Paul Bunyan Connectivity Sustainable Communities Grant Project have been identified in this Project Report. Implementation of any and all of these corridor improvements would respond to the City’s Connectivity Project objectives. Further prioritization of the Top 9 List can be conducted by evaluating each with respect to the following criteria:

Connectivity Benefits

Which of the corridor improvements provides the most connectivity benefits to the greatest number of Susanville residents, property owners and business interests?

Safety Benefits

Which of the corridor improvements provides the most significant safety benefits to the City of Susanville?

Constructability

Which of the corridor improvements can be constructed with the fewest right of way, jurisdictional and/or environmental (e.g. wetlands) constraints?

Funding

Which of the Paul Bunyan Project area improvements are most likely to receive grant funding?

Costs

Which of the corridor improvements can be conducted with the least cost and the best cost to benefit ratio?

Connectivity

All of the corridor improvements presented in this Project Report will improve Project area connectivity. From the findings of this Study and, in particular, the input from the community at the July 12, 2018 Community Workshop, the most significant connectivity improvements would likely be provided by suggested corridor improvements to:

- Paul Bunyan Road
- Skyline Road
- State Route 139

High levels of Project area connectivity would also be provided by suggested improvements to:

- Chestnut Street
- Glenn Drive / Parkdale Avenue / Joaquin Street
- N. Roop Street / Cherry Terrace

While the remaining three corridor improvements provide important connectivity features, implementation would benefit less City of Susanville residents than the other corridor improvements in the Top 9 list. The Paiute Lane improvements would principally benefit Meadow View School. The Numa Road / Spring Ridge Road improvements would principally provide connectivity for North Susanville and Susanville Indian Housing residents. The Radical Road improvements would principally provide improved connectivity to the west end of the Project area and to Susanville Ranch Park.

Safety Benefits

All of the corridor improvements suggested in this Study would improve bicycle and pedestrian safety. The level of safety provided is impacted by current and projected levels of traffic, traffic speeds and projected levels of bicyclists and pedestrians using the Project corridors. Based on the findings developed in this Study and traffic data available from the City, Lassen County and Caltrans, the greatest safety benefits would be provided by suggested corridor improvements on:

- Paiute Lane
- Paul Bunyan Road
- State Route 139
- Skyline Road

Significant safety benefits would also be provided with suggested improvements to:

- Chestnut Street
- Numa / Spring Ridge Road

Because the remaining Paul Bunyan corridor improvements principally serve residential streets with lower traffic volumes and fewer projected bicyclists and pedestrians the following could have a lesser, but still important, safety benefit:

- Glenn Drive / Parkdale Avenue / Joaquin Street
- N. Roop Street / Cherry Terrace
- Radical Road

Constructability

Available right of way, required coordination with other stakeholders and environmental issues were considered in evaluating the constructability of suggested corridor connectivity improvements. The following corridor improvements can be completed without the need for additional right of way acquisition and with few apparent environmental issues (to be verified). Coordination with other stakeholders (Meadow View School, Project residents with frontage along corridor, Susanville Indian Rancheria for example) will still be required:

- Paiute Lane
- Numa / Spring Ridge Road
- Glenn Drive / Parkdale Avenue / Joaquin Street

With respect to constructability, implementation of the following corridor improvements are encumbered, to some degree, by the need to acquire right of way, by potential or apparent environmental issues and / or with significant coordination required with other stakeholders:

- Skyline Road (R/W needed from Susanville Indian Rancheria and Lassen County)
- Paul Bunyan Road (Potential wetlands and drainage issues)
- Radical Road (drainage issues)

Construction of the Class I / Multi-Use Trail along State Route 139 will require right of way acquisition, a Caltrans Encroachment Permit and an approved Caltrans Project Study Report. Completion of improvements along Chestnut Street will require detailed right of way determination and coordination with Lassen County.

Funding

Available funding programs applicable to the various Paul Bunyan Connectivity Corridor Project improvements are presented in Section V of this Project Study. Based on findings to date, the corridor improvements that would most likely receive grant funding support include:

- Paiute Lane
- State Route 139
- Paul Bunyan Road
- Numa / Spring Ridge Road

Given the appropriate “nexus” between Project benefits, costs and available grant funds the next suggested funding tier is believed to be the improvements proposed on:

- Chestnut Street
- Skyline Road
- Radical Road

Costs

The lowest cost of the Paul Bunyan corridor improvements suggested in his Study are those proposed for:

- Cherry Terrace / N. Roop Street
- Numa / Spring Ridge Road

Moderate cost corridor improvements include:

- Radical Road
- Paiute Lane
- Glenn / Parkdale / Joaquin Street
- Chestnut Street

Highest cost of the corridor improvements presented in this Study are those suggested along:

- Skyline Road
- Paul Bunyan Road
- State Route 139

Benefit to Cost Ratio

Considering the connectivity and safety benefits of each corridor improvement and comparing these benefits with respect to projected costs, it is believed that despite their projected relatively high costs, the best Benefit to Cost Ratios would be provided by connectivity improvements at:

- Skyline Road
- Paul Bunyan Road
- SR 139

The next highest benefits to cost ratios are projected to be provided by connectivity improvements at:

- Chestnut Street
- N. Roop / Cherry Terrace
- Numa / Spring Ridge Road
- Paiute Lane

Summary – Prioritization

In **Table 1** is presented a summary of the prioritization criteria presented above and the priority ranking of each of the corridor improvements. Based on the previously present criteria and corridor rankings presented in **Table 1**, the following are suggested as the highest priority connectivity projects included in the Top 9 improvements of the Paul Bunyan Connectivity Improvement Project. All of the criteria presented herein have been weighted equally.

- Skyline Road
- Paul Bunyan Road
- Paiute Lane
- Numa / Spring Ridge Road
- State Route 139

TABLE 1
SUMMARY PRIORITIZATION OF TOP 9 PAUL BUNYAN CONNECTIVITY IMPROVEMENTS ⁽¹⁾

Corridor / Location	Connectivity Benefits	Safety Benefits	Constructability	Funding	Costs	Benefits to Costs	Summary Priority
Radical Road	Moderate	Moderate	Constructible	Fundable	Moderate Cost	Fair	3 rd Tier
Skyline Road	Very High	Very High	Constructible	Fundable	High Cost	Best	Top Priority
Paul Bunyan Road	Very High	Very High	Constructible	Very Fundable	High Cost	Best	Top Priority
Paiute Lane	Moderate	Very High	Very Constructible	Very Fundable	Moderate Cost	Good	Top Priority
Numa / Spring Ridge Road	Moderate	High	Very Constructible	Very Fundable	Low Cost	Good	Top Priority
State Route 139	Very High	Very High	Constructability Issues	Very Fundable	High Cost	Best	Top Priority
Glenn Drive / Parkdale Ave. & Joaquin Street	High	Moderate	Very Constructible	Fundability Issues	Moderate Costs	Fair	3 rd Tier
Chestnut Street	High	High	Constructability Issues	Fundable	Moderate Costs	Good	2 nd Tier
Cherry Terrace	High	Moderate	Very Constructible	Fundability Issues	Lot Cost	Good	2 nd Tier

(1) All criteria weighted equally.

V. FUNDING OPPORTUNITIES AND STRATEGIES

The City of Susanville was awarded funding for the Paul Bunyan Connectivity Project through the Caltrans Sustainable Communities Grant Program. With program ready connectivity plans prepared as part of this grant program, the City of Susanville can now proceed with applications for design and construction funding. For certain grant funding programs applications can be made sequentially. For example, a Project may be programmed for environmental work without being programmed for plans, specifications and estimates (PS&E). A Project may be programmed for design without being programmed for right of way acquisition or construction. A Project may be programmed for right of way acquisition without being programmed for construction.

State and Federal programs which best fit the benefits and conditions of various components of the Paul Bunyan Connectivity Project and the City's need for environmental, design, right of way and construction funding include:

- State Transportation Improvement Program (STIP)
- Active Transportation Program (ATP)
- Bicycle Transportation Account (BTA)
- Congestion Mitigation and Air Quality (CMAQ) Improvement Program
- Environmental Enhancement and Mitigation Program (EEMP)

STATE TRANSPORTATION IMPROVEMENT PROGRAM (STIP)

The STIP is a multi-year capital improvement program for transportation projects on and off the State Highway system. A STIP grant application could, therefore, be submitted by the City for the SR 139 Class 1 / Multi-Use Trail improvements as well as for corridor improvements described in this Project Report for the other 8 Paul Bunyan Connectivity Corridors. To apply for STIP funding the City would work through the Lassen County Transportation Commission. Guidelines adopted for the 2018 STIP and summarized herein and are more fully presented in:

www.catc.gov/programs/stip/2018-stip/2018-stip-guidelines-adopted-08161.pdf

Specific STIP guidelines include:

- Senate Bill 1 stabilized the funding in the State Highway Account that is directed to fund the STIP.
- Bicycle and pedestrian projects may be programmed in the STIP as long as they are eligible for State Highway Account or Federal Funds.
- For Projects located on a State Highway (SR 139, for example), the STIP requires that a Project Study Report (PSR) be prepared. For projects not on a State Highway, a PSR equivalent is required.
- A PSR equivalent must be adequate to define and justify the project scope, cost and schedule to the satisfaction of the regional agency (in this case, the Lassen County Transportation Commission).

- STIP program applications may be made sequentially; for environmental work only, for PS&E only, for right of way acquisition, for construction.
- STIP applications are evaluated with respect to how the Project's goals and objectives compare to, and help implement, the Region's Regional Transportation Plan. Roadway improvements on Chestnut Street and Paiute Lane and bikeway / pedestrian improvements on Paul Bunyan Road, Skyline Drive, Spring Ridge Road, Numa Road, Cherry Terrace and SR 139 are included in the 2017 Lassen Project Transportation Plan. Key corridor improvements along these roadways and included in this Paul Bunyan Connectivity Project Report are consistent with, and would help implement, the 2017 Lassen Regional Transportation Plan.

To best qualify for STIP funding benefits specific to the Paul Bunyan Connectivity Project should be quantified with respect to:

- Change in Vehicle Mile Traveled (VMT) per capita
- Change in commute mode sphere (travel to work or school)
- Change in fatalities and serious injuries per capita
- Change in fatalities and serious injuries per VMT
- Increased capacity benefits including new or upgraded bicycle lane / sidewalk miles on State Highways or on local streets and roads

STIP programming cycles begin with the release of proposed fund estimates in July of odd-numbered years. Once the fund estimate is adopted, regional planning agencies (in this case, the Lassen County Transportation Community) prepare transportation improvement plans for submittal by December 15 of odd numbered years. The City of Susanville should, therefore, work with the Lassen County Transportation Commission and apply for 2019 STIP funding. Paul Bunyan Community Project improvements suggested in this Study Report for Paul Bunyan Road, Skyline Drive, Numa / Spring Ridge Road, Cherry Terrace and SR 139 are the most likely candidates to be considered for STIP funding during the 2019 funding cycle.

ACTIVE TRANSPORTATION PROGRAM (ATP)

The purpose of the California Transportation Commissions Active Transportation Program (ATP) is to encourage increased use of active modes of transportation, such as biking and walking. The goals of the ATP are to:

- Increase the proportion of trips accomplished by biking and walking
- Increase the safety and mobility of non-motorized users
- Advance the active transportation efforts of regional agencies to achieve greenhouse gas reduction goals as established pursuant to Senate Bill 375 (Chapter 728, Statutes of 2008) and Senate Bill 391 (Chapter 585, Statutes of 2009)
- Enhance public health, including reduction of childhood obesity through the use of programs including, but not limited to, projects eligible for Safe Routes to School Program funding.
- Ensure that disadvantaged communities fully share in the benefits of the program
- Provide a broad spectrum of projects to benefit many types of active transportation users

All of the Paul Bunyan corridor improvements suggested in this Project Report would conform to the goals of the ATP.

Each ATP programming cycle includes four years of funding. New programming capacity for the 2019 ATP will be available for fiscal years 2019-2020, 2020-2021, 2021-2022 and 2022-2023. ATP funds are distributed to Metropolitan Planning Organizations (MPO) as well as to small urban and rural areas with populations of 200,000 or less. The City of Susanville is not located within an MPO. Ten percent of ATP funds are specifically designated for small urban and rural areas such as Susanville. Fifty percent of ATP funds are also competitively awarded statewide by the California Transportation Commission (CTC). A minimum of 25% of the funds in the statewide competitive program must benefit disadvantaged communities.

A summary of key ATP points applicable to the City of Susanville and the Paul Bunyan Connectivity Project are presented herein. The 2019 Active Transportation Program Guidelines can be downloaded at:

www.catc.ca.gov/programs/atp/docs/051618_2019_ATP_Guidelines_Final_Adopted.pdf

- The CTC does not require a matching fund for the ATP.
- Funding from ATP may be used to fund the development of community-wide active transportation plans encompassing disadvantaged communities and including bike, pedestrian, safe routes to schools, or comprehensive active transportation plans.
- The City of Susanville is eligible to apply for ATP Funds.
- The City may also partner with Caltrans to expand ATP funding opportunities. Partnering with Caltrans for ATP funding would be applicable for the Class 1 / Multi-Use Trail improvements suggested along SR 139.
- Eligible ATP Projects include infrastructure projects and includes environmental, design, right of way and construction phases of a capital (facilities) projects.
- To be considered for ATP funding a new infrastructure project will require a complete Project Study Report (PSR). A PSR would be specifically required for the SR 139 Class 1 / Multi-Use Trail Project. A PSR equivalent would be sufficient for the other Paul Bunyan Connectivity Corridor Projects.
- To qualify as a Disadvantage Community (DAC), Susanville must show that the Median Household Income is less than 80% of the statewide average or meet the requirements of the California Communities Health Screening Tool 3.0 or show that at least 75% of the public school students in the Project are eligible to receive free or reduced price meals under the National School Lunch Program.
- To qualify as a Safe Route to School Project, projects must directly increase safety and convenience for public school students to walk and / or to bike to school. Paul Bunyan connectivity project corridor improvements suggested along Paiute Lane, Skyland Road, SR 139, Paul Bunyan Road and Numa / Ridge Road would clearly qualify as Safe Routes to School projects.
- The City of Susanville would likely apply for ATP funds in the Medium Project Infrastructure only or Infrastructure / Non-Infrastructure Property Category with a total project cost between \$1.5 Million to \$7.0 Million or for ATP funds available in the Small Project, Infrastructure only or Infrastructure / Non-infrastructure Project Category with a total

project cost of less than \$1.5 Million. A sample Medium Infrastructure only ATP project application is included in the attachments to this Project Report.

BICYCLE TRANSPORTATION ACCOUNT (BTA)

The Bicycle Transportation Account (BTA) is an annual program that provides state funds administered by the Caltrans Local Assistance Programs for City and County projects that improve safety and convenience for bicycle commuters. To be eligible for BTA funds there must have been adopted a Bicycle Transportation Plan. In compliance with this requirement, the Lassen County Transportation Community has adopted the Lassen County Bikeway Master Plan. County Master Plan Bikeways specific to the City of Susanville are shown in **Figure 6**. With the Paul Bunyan Connectivity Project area the Lassen County Bikeway Master Plan (BMP) includes proposed Class 1 Bike Paths on:

- Skyline Road & Skyline Park
- Radical Road
- SR 139
- Paul Bunyan Logging Trail
- Spring Ridge Road

Class 2 Bike lanes are proposed in the Lassen County BMP on:

- Chestnut Street
- Parkdale Avenue
- Paiute Lane

Class 3 Bike Routes are shown on Cherry Terrace.

BTA projects are intended to improve safety and convenience for bicycle commuters and may include:

- New bikeways serving major transportation corridors
- New bikeways removing travel barriers to potential bicycle commuters
- Secure bicycle parking at employment centers park-and-ride lots, rail and transit terminals and ferry docks and landings
- Bicycle-carrying facilities on public transit vehicles
- Installation of traffic control devices to improve the safety and efficiency of bicycle travel
- Elimination of hazardous conditions on existing bikeways
- Planning
- Improvement and maintenance of bikeways

Eligible BTA project activities include:

- Project Planning
- Preliminary Engineering
- Final Design
- Right of way Acquisition
- Construction and/or Rehabilitation

At a minimum, Paul Bunyan Connectivity Corridor Projects included in this Project Report that are eligible for BTA funds include:

- Skyline Road
- SR 139
- Radical Road
- Numa Road / Spring Ridge Road
- Paiute Lane
- Parkdale Avenue / Glenn Drive
- Chestnut Street
- Cherry Terrace

A sample BTA Application Form is included in the attachments to this Project Report.

CONGESTION MITIGATION AND AIR QUALITY (CMAQ) IMPROVEMENT PROGRAM

The Congestion Mitigation and Air Quality (CMAQ) Improvement Program was established by the 1991 Federal Intermodal Surface Transportation Efficiency Act (ISTEA) and reauthorized with the passage of Transportation Equity Act for the 21st Century (TEA-21). A wide and diverse variety of programs are eligible for CMAQ funding. Specific project types include Bicycle and Pedestrian Facilities and Programs. To be eligible for CMAQ funding, projects must be located in a non-attainment area for ozone, carbon monoxide or particulate matter (PM-10). Lassen County is a nonattainment area for PM-10.

The CMAQ program is funded by the Federal Highway Administration and administered by the Caltrans Local Assistance Program. CMAQ funds may be used to establish new or expanded transportation projects or programs that reduce emissions, including capital investments in transportation infrastructure, congestion relief efforts, diesel engine retrofits or other capital projects.

Bicycle and pedestrian facilities and programs eligible for CMAC funding include:

- Constructing bicycle and pedestrian facilities (paths, bike racks, support facilities, etc.) that are not exclusively recreational and reduce vehicle trips
- Non-construction outreach related to safe bicycle use

Each of the Paul Bunyan Connectivity Project elements include the construction of bicycle and pedestrian facilities. Each of the Top 9 Corridor Projects identified in this Project Report would, therefore, be eligible for CMAQ funding.

ENVIRONMENTAL ENHANCEMENT AND MITIGATION PROGRAMS (EEMP)

The Environmental Enhancement and Mitigation Program (EEMP) grants to local, state and federal agencies funds to mitigate the environmental impacts caused by new or modified public transportation facilities. Grants for individual projects are generally limited to \$500,000 each. Matching funds are not required for EEMP grants, however, leveraging other sources of funds can make projects more competitive.

EEMP funding must be used to mitigate the environmental impacts of an existing transportation facility or the impacts associated with the construction of a new transportation facility. Mitigation of the environmental impacts of improvements to an existing transit facility (widening of Paul Bunyan Road or reconstruction of the Skyland Park Class I Bike / Multi-Use Trail, for example) or construction of a new facility (Radical Road improvements) would qualify for EEMP funding.

Improvements must be included in a Regional Transportation Program (2017 Lassen Regional Transportation Plan), Environmental Compliance (CEQA / NEPA) must be completed at the time of the EEMP grant application. Particularly successful EEMP grant projects include Best Management Practices that increase water conservation.

EEMP grants are selected based on:

- Review of application for completeness and initial eligibility
- Site visits by representatives of the EEMP
- Final evaluation and recommendations to the Resource Agency Secretary
- Recommendations to the California Transportation Commission

Contact information and program assistance can be obtained through the following:

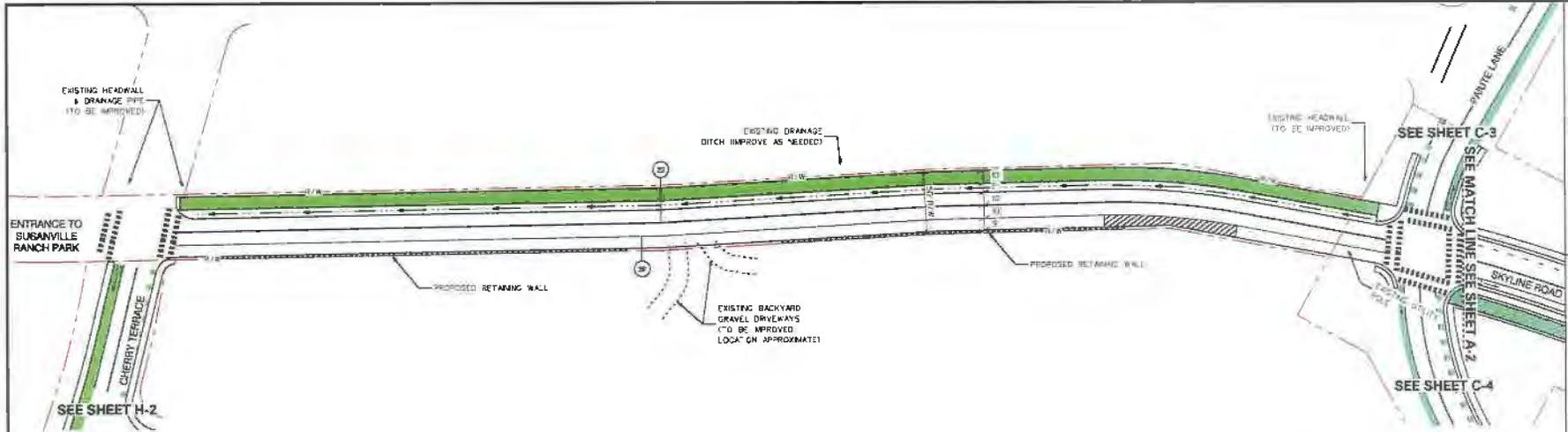
eemccordinator@resourcesca.gov or

<http://resources.ca.gov/grants/environmental-enhancement-and-mitigation-eem/>

Of the Top 9 Paul Bunyan connectivity corridor locations, the Paul Bunyan Road and the Radical Road corridor projects would likely have the best chance to qualify for an EEMP grant.

ATTACHMENTS

- **CONCEPT CONNECTIVITY EXHIBITS A-1 THROUGH H-2**
- **SAMPLE ACTIVE TRANSPORTATION PROGRAM (ATP) APPLICATION FORM**
- **SAMPLE BICYCLE TRANSPORTATION ACCOUNT (BTA) APPLICATION FORM**



Suggested, Concept Level, Radical Road Connectivity Improvements

- o Two, 10 Foot Wide Traffic Lanes
- o Parking Lot, South Side
- o Class 1 Multi-Use Trail, North Side
- o Striping and Signing
- o Drainage Improvements, North Side
- o Storm Drain Improvements @Cherry Terrace and Paiute Lane Intersections
- o Retaining Walls and Curb, South Side, with Provisions for Through Drainage
- o Pedestrian Crosswalk Improvements @Cherry Terrace and Paiute Lane Intersections
- o Retain Access to Brookwood Drive Parcels

LEGEND

- CA MUTCD STRIPING TYPE
- BME LANE MARKINGS PER CA MUTCD
- CONSTRUCT STREET LIGHTS, SPACED APPROX. 20' PER 1/2" OF SUSANVILLE STANDARDS SEE SHEET B-2
- INSTALL HIGH VISIBILITY CROSSWALK SEE DETAIL 2 SHEET C-3
- INSTALL 12" CROSSWALK STRIPING
- APPROX RIGHT OF WAY
- INSTALL CLASS 1 SHARROWS SEE DETAIL 2 SHEET B-2

NO	REVISIONS	DATE	BY

CONCEPT CONNECTIVITY EXHIBIT

SCALE: 1" = 40'

JOB NO. (BIBU-D)

RELEASE 1

DATE: AUG 31, 2016



EXHIBITS FOR CONCEPT DESIGN OF
PAUL BUNYAN CONNECTIVITY IMPROVEMENTS
 SUSANVILLE, CALIFORNIA

RADICAL ROAD
CHERRY TERRACE TO PAIUTE LN

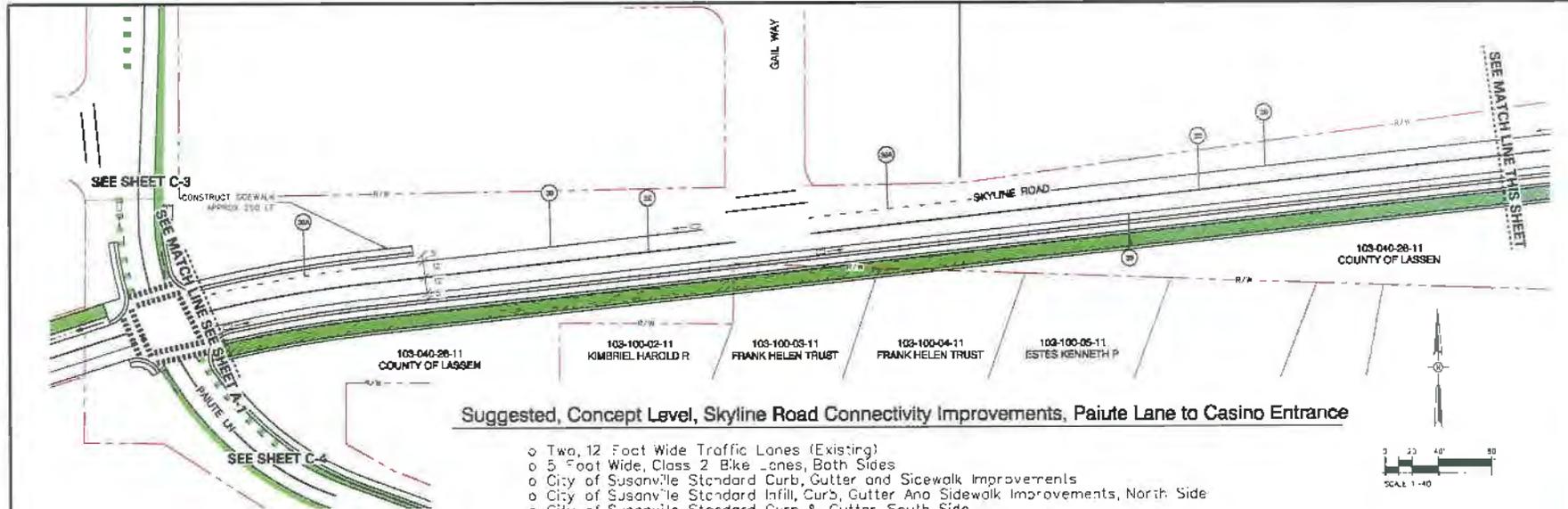


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NOTES

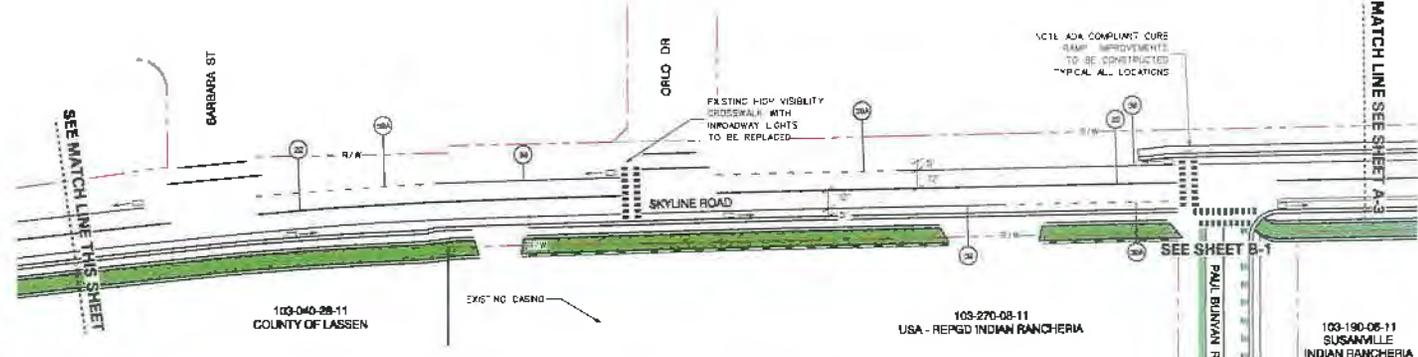
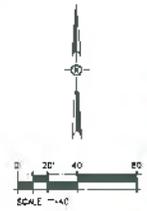
1. PARKING LOCATIONS ARE APPROXIMATE
2. PARKING COURT AND LOCATIONS MAY VARY DUE TO CONFLICT WITH EXISTING TREES, STEEP GRADES AND/OR EXISTING DRIVEWAYS



Suggested, Concept Level, Skyline Road Connectivity Improvements, Paiute Lane to Casino Entrance

- o Two, 12 Foot Wide Traffic Lanes (Existing)
- o 5 Foot Wide, Class 2 Bike Lanes, Both Sides
- o City of Susanville Standard Curb, Gutter and Sidewalk Improvements
- o City of Susanville Standard Infill, Curb, Gutter And Sidewalk Improvements, North Side
- o City of Susanville Standard Curb & Gutter, South Side
- o 10 foot Wide, Class 1 Bike Path / Multi-Use Trail, South Side

- LEGEND**
- CA MUTCD STRIPING TYPE
 - BKE LANE MARKINGS PER CA MUTCD
 - CONSTRUCT STREET LIGHTS, SPACED APPROX. 300 PER CITY OF SUSANVILLE STANDARDS SEE SHEET B-2
 - INSTALL HIGH VISIBILITY CROSSWALK SEE DETAIL 2 SHEET C-3
 - INSTALL CROSSWALK STRIPING
 - APPROX RIGHT OF WAY
 - INSTALL CLASS 3 SHARROWS SEE DETAIL 2 SHEET C-2



Suggested, Concept Level, Skyline Road Connectivity Improvements, Casino Entrance to Paul Bunyan Road

- o Two, 12 Foot Wide Traffic Lanes (Existing)
- o 5 foot Wide, Class 2 Bike Lanes, Both Sides
- o City of Susanville Standard Curb, Gutter and Sidewalk Improvements (Existing) North Side
- o City of Susanville Standard Curb & Gutter Improvements (Existing) South Side
- o 10 foot Wide, Class 1 Bike Path/Multi-Use Trail, South Side
- o Retaining Wall, Protective Railing, Casino Parking Lot Modifications, South Side
- o Replace Existing High Visibility Crosswalk at Orlo Road Intersection

NO.	REVISIONS AND DESCRIPTION	DATE	BY

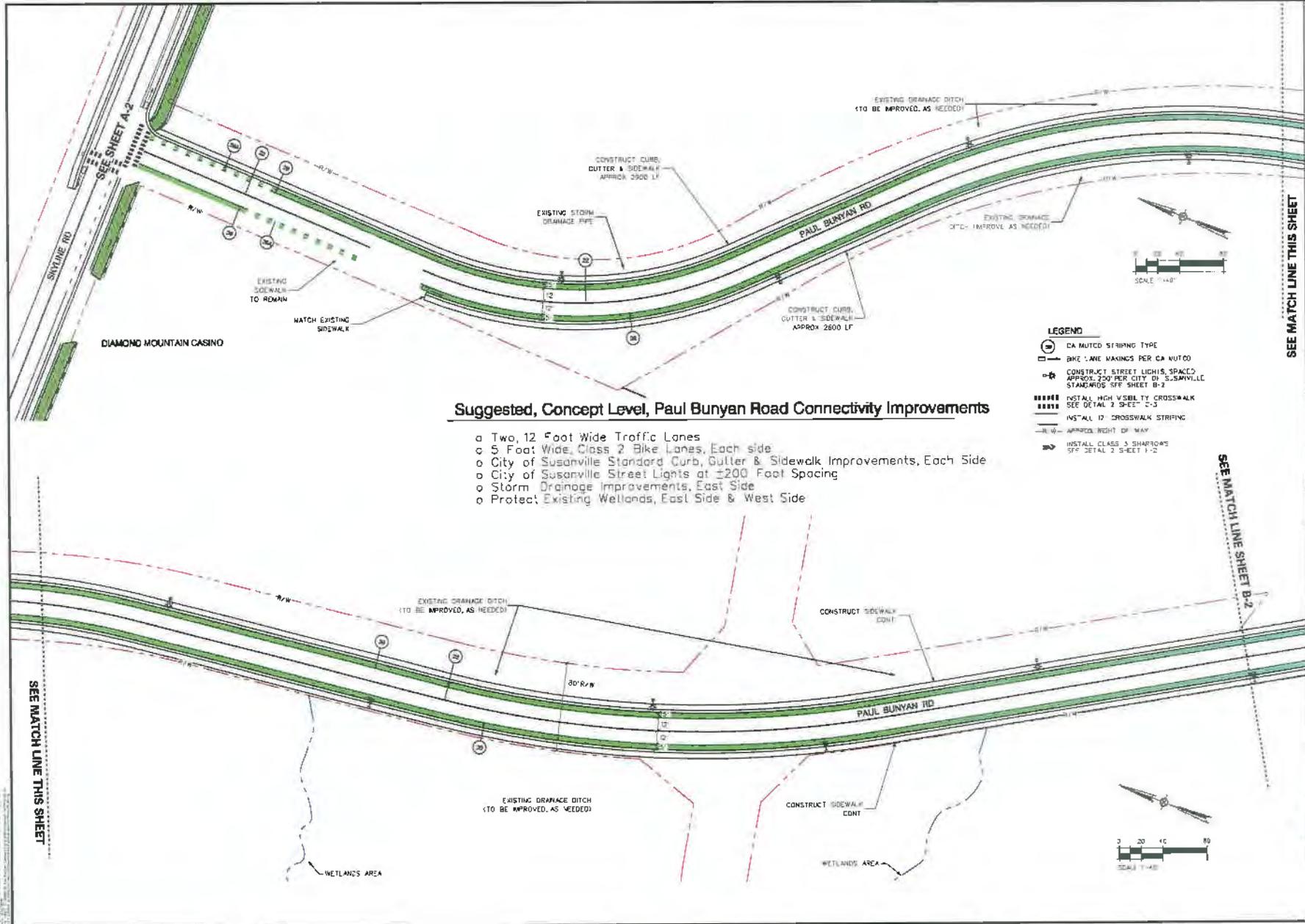
CONCEPT CONNECTIVITY EXHIBIT	RELEASE: 1
SCALE: _____	DATE: AUG. 9, 2018
JOB NO: _____	DATE: _____



EXHIBIT FOR CONCEPT DESIGN OF
PAUL BUNYAN CONNECTIVITY IMPROVEMENTS
 SUSANVILLE, CALIFORNIA
SKYLINE ROAD
PAIUTE LN TO PAUL BUNYAN RD

KASL
 KASL CONSULTANTS
 1000 N. MAIN ST. SUITE 100
 SUSANVILLE, CA 96150
 TEL: 530.885.1111 FAX: 530.885.1112

SHEET
A-2



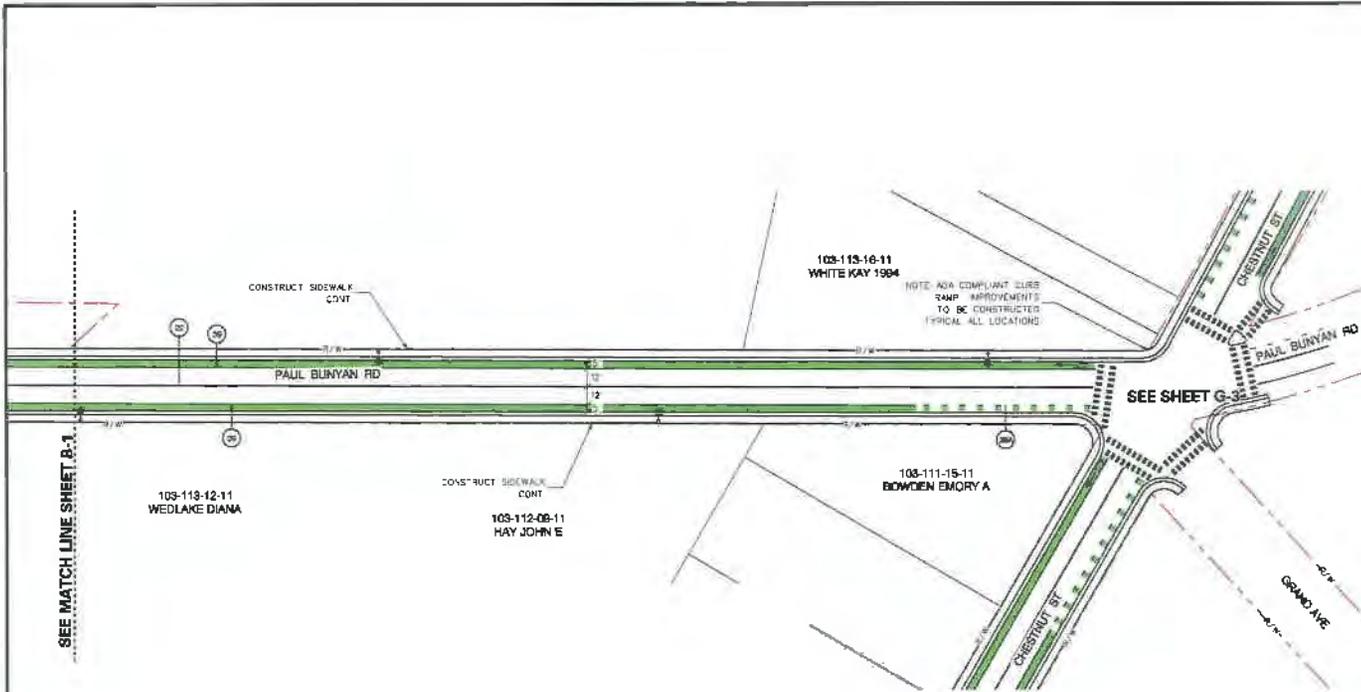
Suggested, Concept Level, Paul Bunyan Road Connectivity Improvements

- o Two, 12 Foot Wide Traffic Lanes
- o 5 Foot Wide, Class 2 Bike Lanes, Each side
- o City of Susanville Standard Curb, Gutter & Sidewalk Improvements, Each Side
- o City of Susanville Street Lights at ±200 Foot Spacing
- o Storm Drainage Improvements, East Side
- o Protect Existing Wetlands, East Side & West Side

LEGEND

- CA MUTCO STRIPING TYPE
- BIKE LANE MARKINGS PER CA MUTCO
- CONSTRUCT STREET LIGHTS, SPACED APPROX. 200 PER CITY OF SUSANVILLE STANDARDS SEE SHEET B-2
- INSTALL HIGH VISIBILITY CROSSWALK SEE DETAIL 2 SHEET 2-3
- INSTALL 12" CROSSWALK STRIPING
- R/W - APPROX. RIGHT OF WAY
- INSTALL CLASS 3 SHARPOW'S SEE DETAIL 2 SHEET 1-2

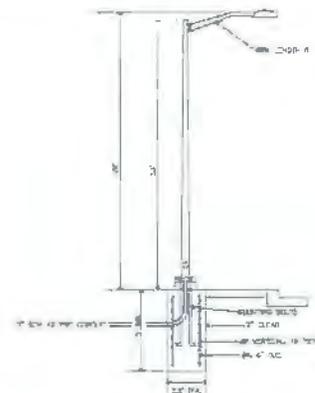
<p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DESCRIPTION</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	DESCRIPTION	DATE	BY																	<p>DATE: _____</p> <p>SCALE: _____</p> <p>JOB NO: 6680-G1</p> <p>RELEASE 1</p> <p>DATE: AUG 01, 2013</p>
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<p>REVIEW COPY</p>																					
<p>EXHIBITS FOR CONCEPT DESIGN OF PAUL BUNYAN CONNECTIVITY IMPROVEMENTS SUSANVILLE, CALIFORNIA</p>																					
<p>PAUL BUNYAN ROAD SKYLINE TO CHESTNUT ST 1 OF 2</p>																					
<p>KASL</p> <p>DATE: _____</p> <p>SCALE: _____</p> <p>JOB NO: _____</p>																					
<p>SHEET</p> <p>B-1</p>																					



- LEGEND**
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 - BIKE LANE MARKINGS PER CA MUTED
 - CONSTRUCT STREET LIGHTS, SPACED APPROX. 100' PER CITY OF SUSANVILLE STANDARD SET SPACING B-2
 - INSTALL HIGH VISIBILITY CROSSWALK SEE DETAIL 2 SHEET B-2
 - INSTALL 12" CROSSWALK STRIPING
 - APPROX RIGHT OF WAY
 - INSTALL CLOSE 3 SHARROWS SEE DETAIL 2 SHEET F 2

Suggested, Concept Level Paul Bunyan Road Connectivity Improvements; Existing 50' R/W, Chestnut Street To ±750' North Of Chestnut Street Intersection

- O Two, 12 Foot Wide Traffic Lanes (Existing)
- O 5 Foot Wide, Cross 2 Bike Lanes, Each Side
- C City Of Susanville Standard Curb, Gutter And Sidewalk Improvements
- C City Of Susanville Standard Street Lights
- O Storm Drainage Improvements Per City Standards



NOTES

1. POLE SHALL BE TAPERED GALVANIZED STEEL 4" X 7"
2. LUMINAIRE SHALL BE 100 WATT MULTIVOLT, WPS, FLI: CUTOFF.
3. MOUNTING BOLTS SHALL BE 1" X 3/8" X 4"
4. PHOTOCELL SHALL BE INSTALLED WITH SENSOR FACING NORTH.
5. SERVICE CONDUCTOR SHALL BE NO. 8 ALUMINUM 900 VOLT XLPE.
6. WIRE INSIDE POLE TO HAND HOLE SHALL BE NO. 10 OR NO. 12 COPPER WIRE.
7. CONCRETE SHALL CONFORM TO CITY STANDARD DETAIL 9-1
8. WHEN INSTALLED AT AN INTERSECTION, POLE SHALL BE SET SO THAT POLE ARM IS AT A 45° ANGLE TO INTERSECTION.

RECOMMENDED SPACING: PLACE AT STREET INTERSECTIONS, MAJOR COMMERCIAL/INDUSTRIAL ENTRANCES (UNLESS PRIVATE LIGHTING INSTALLED, PREFERRED), AND APPROXIMATELY 500' INTERVALS BETWEEN INTERSECTIONS. PLACEMENT AT CRITICAL PUBLIC FACILITIES SHOULD BE CONSIDERED AS SO.

NO.	REVISIONS	DATE	BY

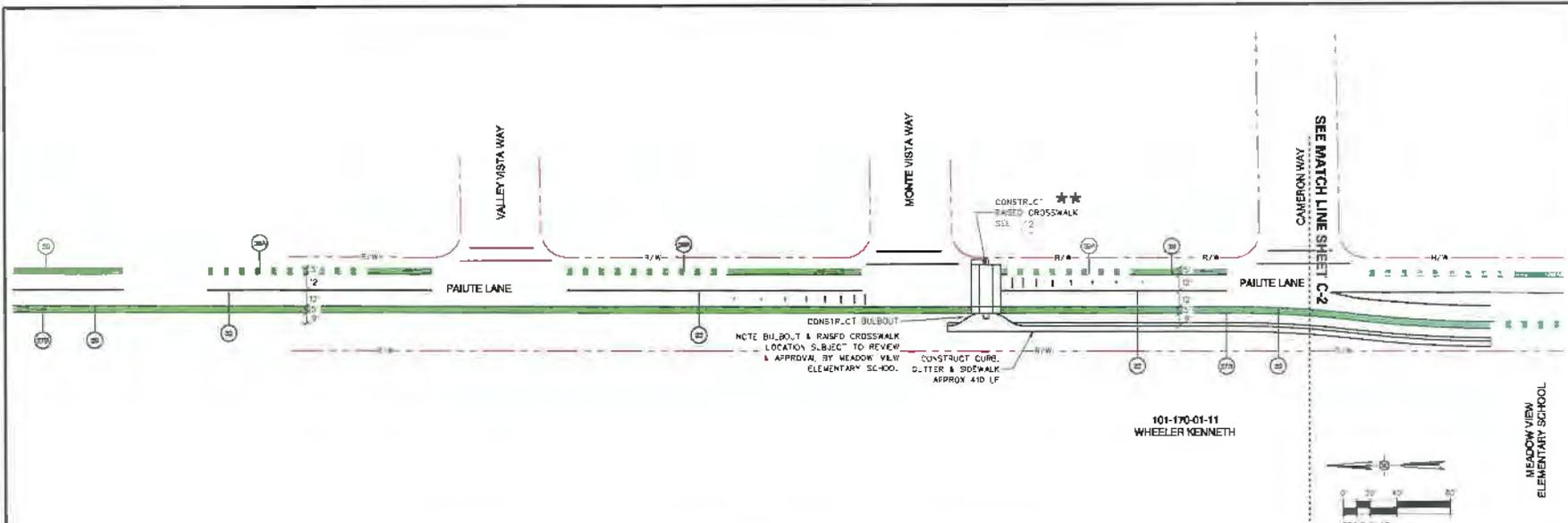
CONCEPT CONNECTIVITY EXHIBIT	RELEASE 1
	DATE: AUG 01, 2019
SCALE:	JOB NO. 18000101



EXHIBIT FOR CONCEPT DESIGN OF
PAUL BUNYAN ROAD CONNECTIVITY IMPROVEMENTS
 SUSANVILLE, CALIFORNIA
PAUL BUNYAN ROAD
SKYLINE TO CHESTNUT ST 2 OF 2



SHEET
B-2

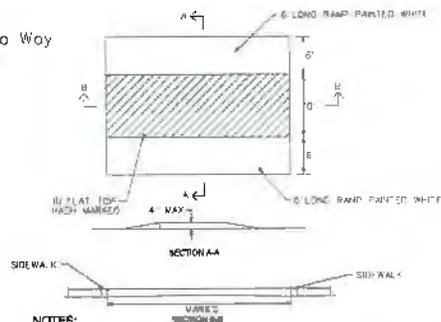
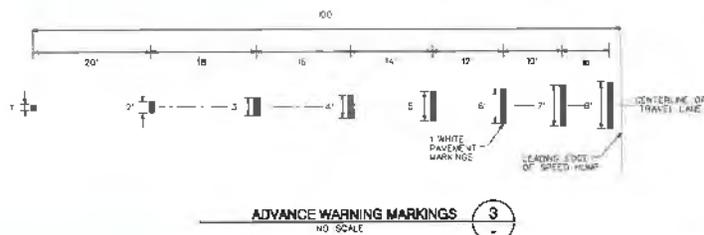


Suggested, Concept Level. Paulte Lane Connectivity Improvements; North End To Cameron Way

LEGEND

- CA MUTCO STRIPING TYPE
- BIKE LANE MARKINGS PER CA MUTCO
- CONSTRUCT STREET LIGHTS, SPACED APPROX. 300 PER CITY OF SUSANVILLE STANDARDS SEE SHEET B-2
- INSTALL HIGH-VISIBILITY CROSSWALK SEE DETAIL 2 SHEET C-3
- INSTALL 12" CROSSWALK STRIPING
- APPROX. RIGHT OF WAY
- INSTALL CLASS 3 SHARROWS SEE DETAIL 2 SHEET F-2

- Two, 12 Foot Wide Traffic Lanes (Existing)
- 5 Foot Wide, Class 2 Bike Lanes, Each Side
- City Of Susanville Standard Curb, Gutter And Sidewalk Improvements, East Side (Existing)
- City Of Susanville Standard Curb, Gutter And Sidewalk Improvements, West Side Beginning At Monte Vista Way
- Raised Crosswalk Arc Bulbouts; Location Subject To Review And Approval By Meadow View Elementary



- NOTES:**
1. THE FLAT-TOP SPEED BUMP IS ACCEPTABLE FOR USE AS A TRAFFIC CALMING DEVICE ON PUBLIC ROADS AND FIRE DISTRICT ACCESS ROADS WITH A POSTED SPEED LIMIT UP TO 25 MPH.
 2. SPEED BUMPS SHALL BE DESIGNED TO THE FOLLOWING SPECIFICATIONS: FLAT-TOP PORTION: WHICH IS 32 FEET LONG COMPRISING TWO (2) 16' LONG RAMP PORTIONS AND A 10' (10') LONG TOP TOTAL MARKING HEIGHT SHALL NOT EXCEED 4 INCHES.
 3. SPEED BUMPS SHALL EXTEND THE FULL LENGTH OF THE DRIVING SURFACE.
 4. SPACING OF SPEED BUMPS SHALL BE NO CLOSER THAN 750 FEET APART.
 5. SPEED BUMPS SHALL NOT BE PLACED ON GRADES EXCEEDING 8% AT POINTS WITHIN 100 FEET OF ROADWAY INTERSECTIONS OR WHERE THERE IS LIMITED SIGHT DISTANCE.
 6. RAMP PORTION SHALL BE PAINTED WHITE 10 FOOT FLAT-TOP PORTION SHALL BE HATCH-MARKED AND BE PAINTED WHITE. WARNING SIGNAGE SHALL BE REFLECTING. WARNING STRIPING SHALL BE WHITE REFLECTIVE. THROUGH-SIGHT TYPE PAINT OR STRIPING SHALL BE MAINTAINED.

RAISED CROSSWALK **

** SUBJECT TO CITY OF SUSANVILLE FIRE DEPARTMENT APPROVAL

REVISIONS	NO. DESCRIPTION	DATE	BY

CONCEPT CONNECTIVITY EXHIBIT

RELEASE 1
DATE AUG. 21, 2018

SCALE:
JOB NO. 6800 01

REVIEW COPY

EXHIBITS FOR CONCEPT DESIGN OF
PAUL BUNYAN CONNECTIVITY IMPROVEMENTS
SUSANVILLE, CALIFORNIA

PAULTE LANE

NORTHERLY LIMITS TO CAMERON WAY

KASL
KASL CONSULTING GROUP
1000 UNIVERSITY AVENUE, SUITE 100
SUSANVILLE, CA 96088
TEL: 530.938.2222
WWW.KASLCONSULTING.COM

SHEET
C-1

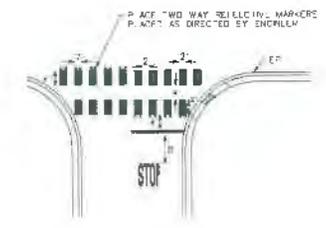


NOTE: ADA COMPLIANT CURB RAMP IMPROVEMENTS TO BE CONSTRUCTED TYPICAL ALL LOCATIONS

Suggested, Concept Level, Paiute Lane Connectivity Improvements, Shawn Lane To Skyline Road

- LEGEND**
- CA MUTCD STRIPING TYPE
 - BIKE LANE MARKING PER CA MUTCD
 - CONSTRUCT STREET LIGHTS, SPACED APPROX. 200' PER 1/4" OF SUSANVILLE STANDARD SEE SHEET B-2
 - INSTALL HIGH VISIBILITY CROSSWALK SEE DETAIL 2 SHEET C-2
 - INSTALL 12" CROSSWALK STRIPING
 - APPROX. RIGHT OF WAY
 - INSTALL CLASS 3 SHARROWS SEE DETAIL 2 SHEET F-2

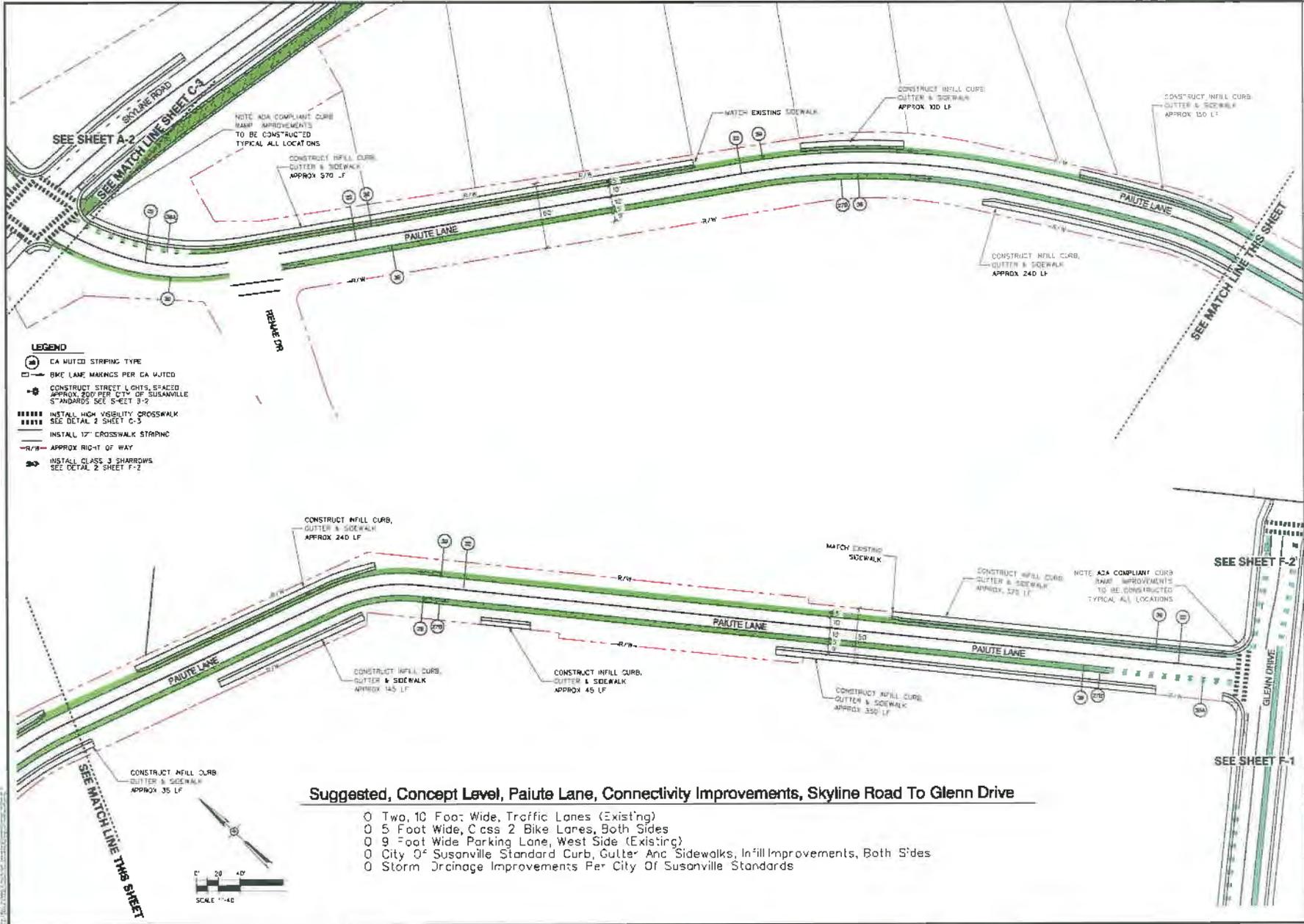
- 0 Two, 11 Foot Wide Traffic Lanes (Existing)
- C 5 Foot Wide, Class 2 Bike Lanes, Both Sides
- C 9 Foot Wide Parking Lane, West Side (Existing)
- 0 City Of Susanville Standard Curb, Gutter And Sidewalk Improvements, Both Sides (Existing)



HIGH VISIBILITY CROSSWALK STRIPING

2

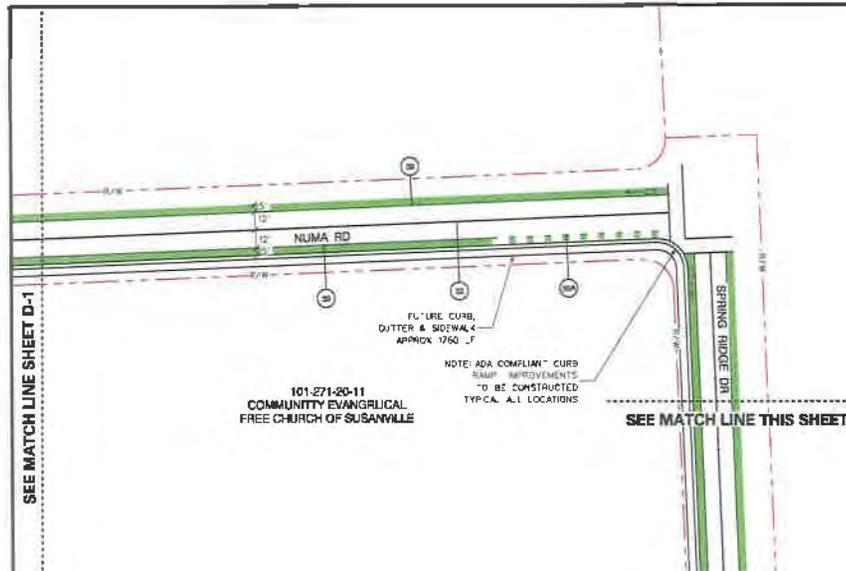
<p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 5%;">NO.</th> <th style="width: 45%;">DESCRIPTION</th> <th style="width: 10%;">DATE</th> <th style="width: 40%;">BY</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	DESCRIPTION	DATE	BY																																									<p style="text-align: center;">CONCEPT CONNECTIVITY EXHIBIT</p> <p>SCALE: _____ JOB NO. _____ DATE: AUC 01, 2018</p> <p>RELEASE 1</p>
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<p>EXHIBITS FOR CONCEPT DESIGN OF PAUL BUNYAN CONNECTIVITY IMPROVEMENTS SUSANVILLE, CALIFORNIA</p> <p style="font-size: 1.2em; font-weight: bold;">PAIUTE LANE</p> <p style="font-size: 1.2em; font-weight: bold;">SHAWAN WAY TO SKYLINE RD</p>																																													
<p>KASL</p> <p style="font-size: 0.8em;">KASL CONSULTING GROUP, INC. 1000 W. MAIN ST., SUITE 100 SUSANVILLE, CA 96131 TEL: 530.938.1111 FAX: 530.938.1112</p>																																													
<p>SHEET</p> <p style="font-size: 1.5em; font-weight: bold;">C-3</p>																																													



Suggested, Concept Level, Paiute Lane, Connectivity Improvements, Skyline Road To Glenn Drive

- Two, 10 Foot Wide, Traffic Lanes (Existing)
- 5 Foot Wide, Class 2 Bike Lanes, Both Sides
- 9 Foot Wide Parking Lane, West Side (Existing)
- City Of Susanville Standard Curb, Gutter, And Sidewalks, Infill Improvements, Both Sides
- Storm Drainage Improvements Per City Of Susanville Standards

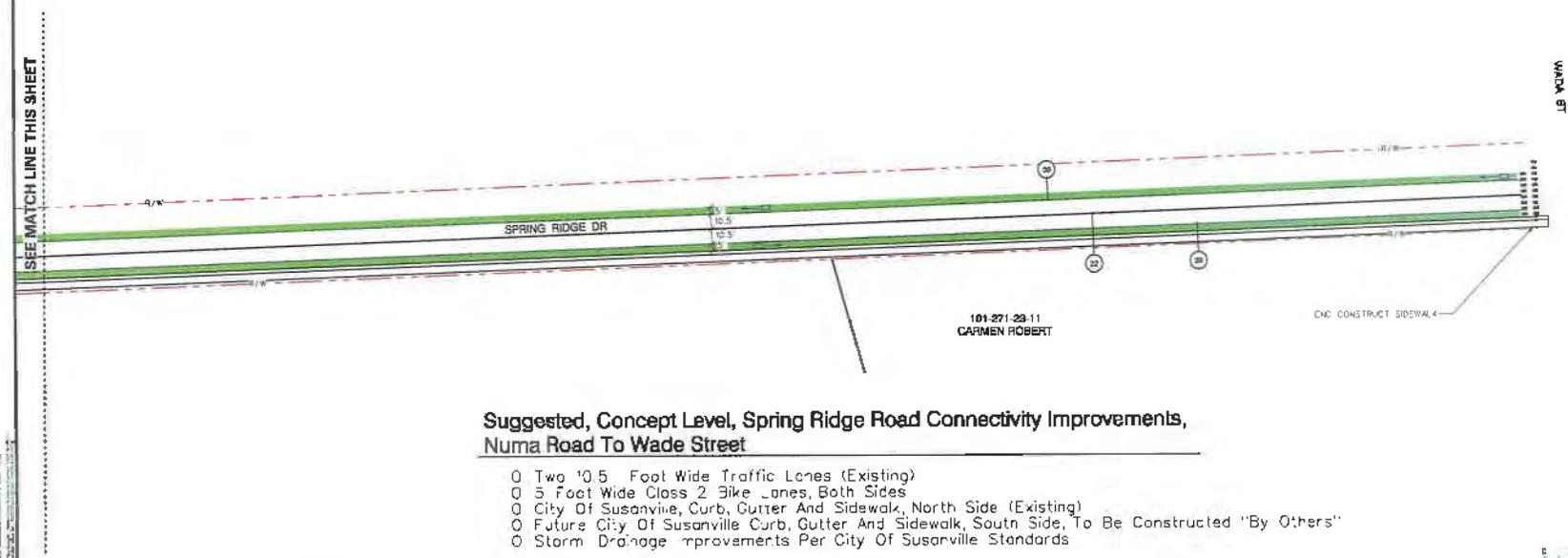
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<p>EXHIBIT FOR CONCEPT DESIGN OF PAUL BUNYAN CONNECTIVITY IMPROVEMENTS SUSANVILLE, CALIFORNIA</p> <p style="font-size: 1.2em; font-weight: bold;">PAIUTE LANE</p> <p style="font-size: 1.2em; font-weight: bold;">SKYLINE RD TO GLENN DR</p>									
<p>FAST</p> <p style="font-size: 1.5em; font-weight: bold;">SHEET C-4</p>									



Suggested, Concept Level, Numa Road Connectivity Improvements, Cameron Way To Spring Ridge Road

- Two 11 To 12 Foot Wide Traffic Lanes (Existing)
- 5 To 6 Foot Wide Class 2 Bike Lanes, Both Sides
- City Of Susanville Curb, Gutter And Sidewalk, West Side, To 3'5 Feet North Of Cameron Way
- Future City Of Susanville Curb, Gutter And Sidewalk, East Side, To Be Constructed 'By Others'
- Storm Drainage Improvements Per City Of Susanville Standards

- LEGEND**
- CA MUTED STRIPING TYPE
 - BIKE LANE MARKINGS PER CA MUTCD
 - ★ CONSTRUCT STREET LIGHTS, SPACED APPROX 300 FEET CITY OF SUSANVILLE STANDARDS SEE SHEET R-7
 - INSTALL HIGH VISIBILITY CROSSWALK SEE DETAIL 2 SHEET C-3
 - INSTALL 12" CROSSWALK STRIPING
 - R/W- APPROX RIGHT OF WAY
 - INSTALL CLASS 2 SK-ARROWS SEE DETAIL 2 SHEET F-2



Suggested, Concept Level, Spring Ridge Road Connectivity Improvements, Numa Road To Wade Street

- Two 10.5 Foot Wide Traffic Lanes (Existing)
- 5 Foot Wide Class 2 Bike Lanes, Both Sides
- City Of Susanville Curb, Gutter And Sidewalk, North Side (Existing)
- Future City Of Susanville Curb, Gutter And Sidewalk, South Side, To Be Constructed "By Others"
- Storm Drainage Improvements Per City Of Susanville Standards

<p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DESCRIPTION</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	DESCRIPTION	DATE	BY																	<p style="text-align: center;">CONCEPT CONNECTIVITY EXHIBIT</p> <p>SCALE: _____ JOB NO. _____ SHEET NO. _____</p> <p style="text-align: right;">RELEASE 1 DATE: AUG 01, 2016</p> <div style="text-align: center;"> </div> <p style="text-align: center;">EXHIBITS FOR CONCEPT DESIGN OF PAUL BUNYAN CONNECTIVITY IMPROVEMENTS SUSANVILLE, CALIFORNIA</p> <p style="text-align: center;">SPRING RIDGE ROAD NUMA RD TO WADA ST</p> <div style="text-align: center;"> </div> <p style="text-align: center;">SHEET D-2</p>
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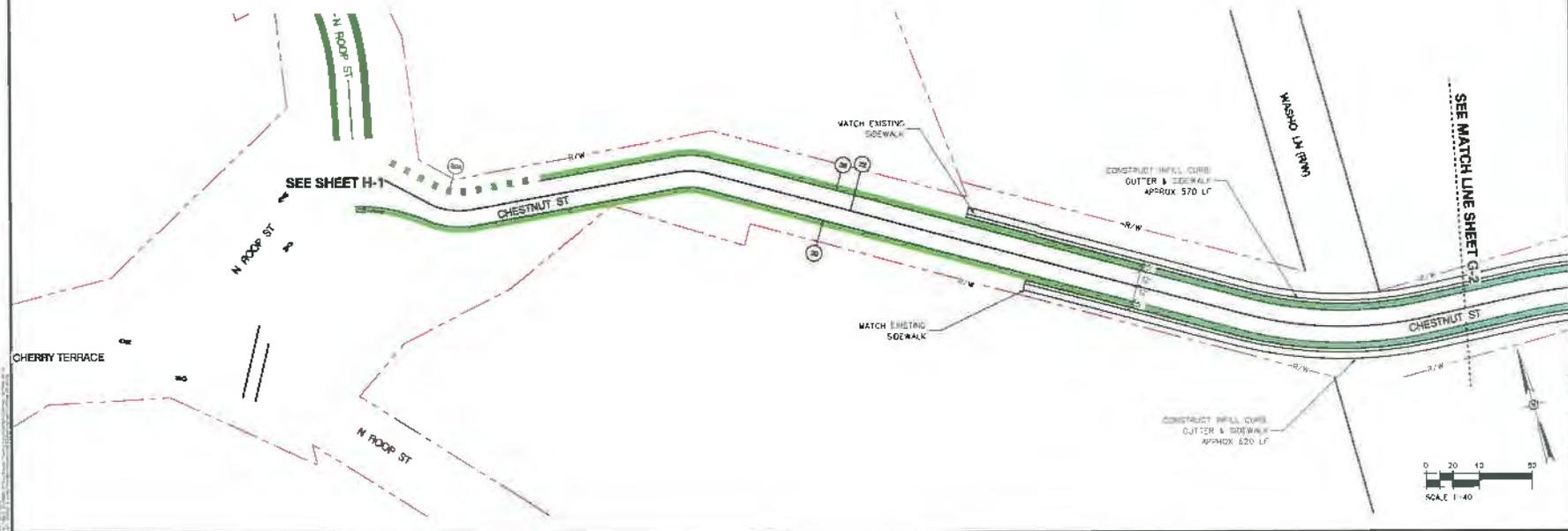
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 PLOTTER: HP DesignJet T1100e



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	STATE ROUTE 139 CHESTNUT ST TO SKYLINE RD DR 1 OF 3			SCALE: _____ RELEASE 1 JOB NO 6880 31 DATE: AUG 01, 2018		<table border="1"> <thead> <tr> <th>NO.</th> <th>DESCRIPTION</th> <th>BY</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td></tr> </tbody> </table>		NO.	DESCRIPTION	BY													
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SHEET E-1																							

**Suggested, Concept Level, Connectivity Improvements, Chestnut Street,
N. Roop Street/Cherry Terrace To State Route 139.**

- Two, 10 To 12 Foot Wide Traffic Lanes (Existing)
- 5 To 6 Foot Wide, Class 2 Bike Lanes, Both Sides
- City Of Susanville Standard Curb, Gutter And Sidewalk, nfi Improvements, North Side
- Storm Drainage Improvements Per City Of Susanville Standards



REVISIONS	
NO	DESCRIPTION

CONCEPT CONNECTIVITY EXHIBIT

SCALE: _____ JOB NO: _____

RELEASE 1 DATE: N.C. 07-23-16

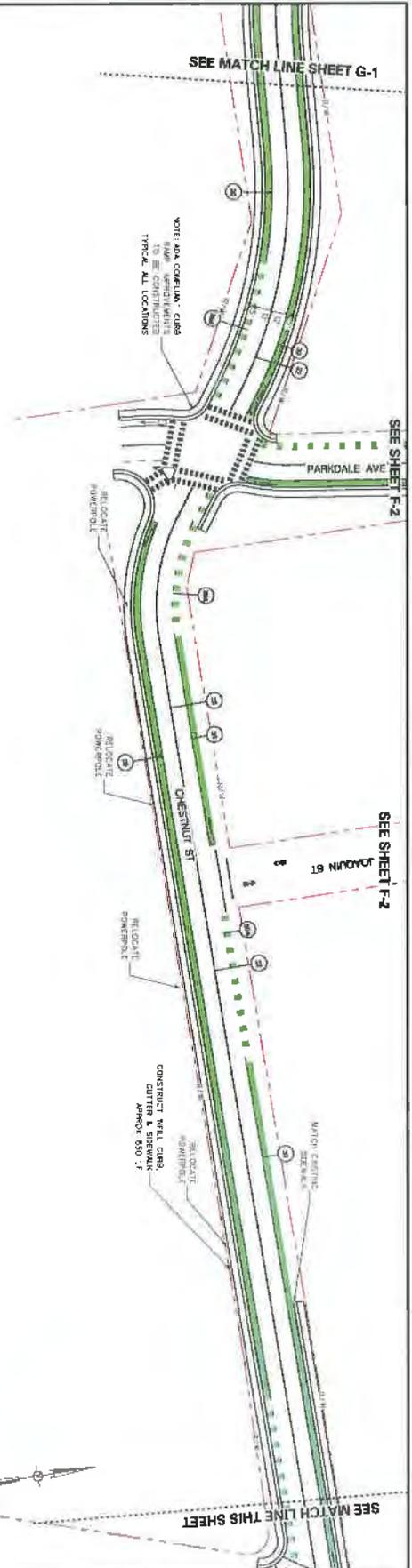
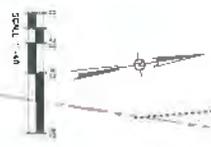
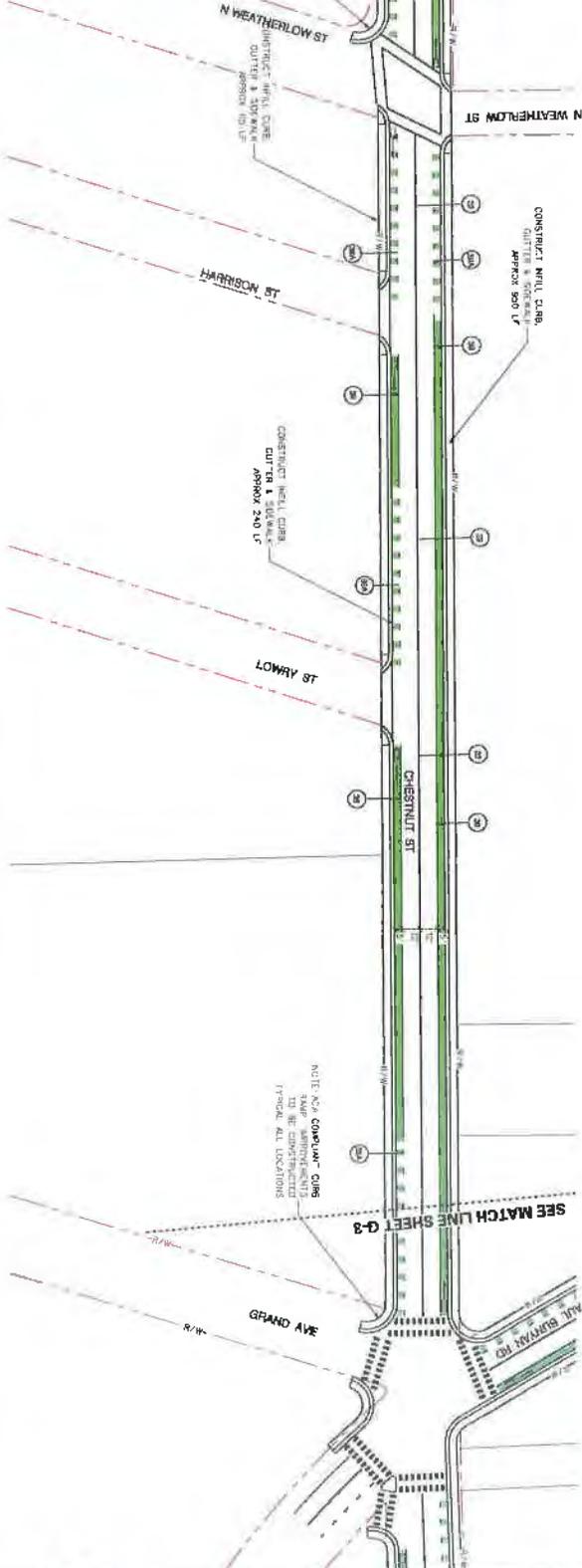
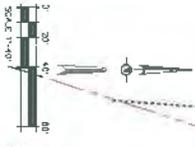


EXHIBITS FOR CONCEPT DESIGN OF
PAUL BUNYAN CONNECTIVITY IMPROVEMENTS
 SUSANVILLE, CALIFORNIA

**CHESTNUT ST
 N ROOP ST TO PARKDALE AVE**



SHEET
G-1



G-2
SHEET

EXHIBITS FOR CONCEPT DESIGN OF
PAUL BUNYAN CONNECTIVITY IMPROVEMENTS
 SUSANVILLE, CALIFORNIA

CHESTNUT ST
PARKDALE AVE TO PAUL BUNYAN RD



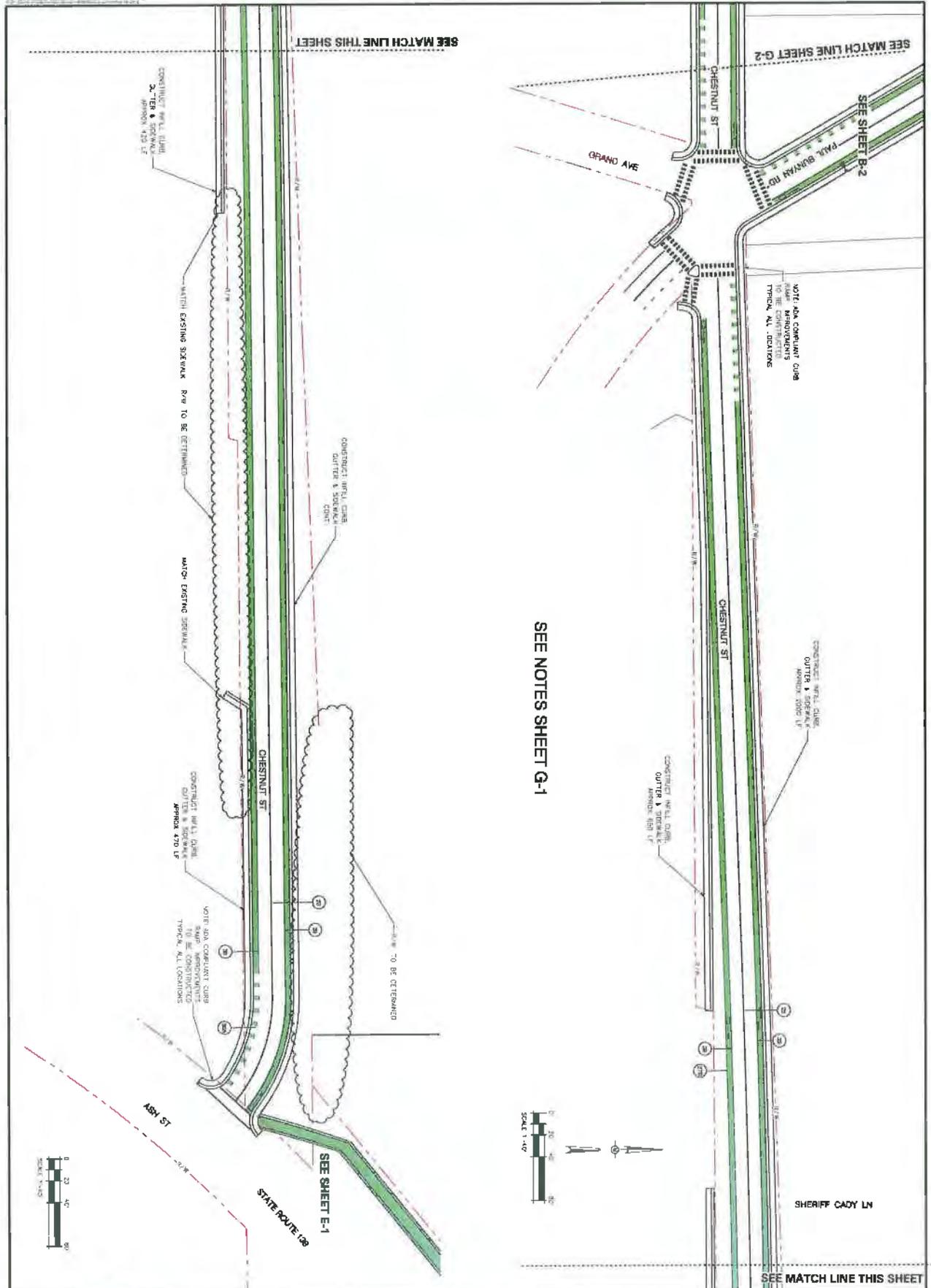
CONCEPT CONNECTIVITY EXHIBIT

SCALE: RELEASE 1

JOB NO: 11-00-000 DATE: 11/11/11

NO	DESCRIPTION	DATE	BY

11/15/2018 10:00 AM
 11/15/2018 10:00 AM
 11/15/2018 10:00 AM



G-3 SHEET		EXHIBITS FOR CONCEPT DESIGN OF PAUL BUNYAN CONNECTIVITY IMPROVEMENTS SUSANVILLE, CALIFORNIA		CONCEPT CONNECTIVITY EXHIBIT	REVISIONS NO. DESCRIPTION DATE BY																	
		CHESTNUT ST PAUL BUNYAN RD TO SR 139		SCALE: RELEASE 1 JOB NO: 5890-01 DATE: AUG 31, 2018	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 5%;">NO.</th> <th style="width: 75%;">DESCRIPTION</th> <th style="width: 10%;">DATE</th> <th style="width: 10%;">BY</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	DESCRIPTION	DATE	BY													
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		RELEASE 1	DATE AUG 01 2008
EXHIBITS FOR CONCEPT DESIGN OF PAUL BUNYAN CONNECTIVITY IMPROVEMENTS SUSANVILLE, CALIFORNIA			
N ROOP ST & CHERRY TERRACE GLENN DR TO CHESTNUT ST			
KASL		SHEET	
		H-1	

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

SUBMITTED BY: Mike Wilson, City Administrator 

MEETING DATE: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: Mission Statement, Vision Statement and Core Values

SUMMARY: Staff desires to explore and develop a Mission Statement, Vision Statement and Set of Core Values that will help identify the purpose of our City organization and will be a part of our branding and marketing plan, as well as, an overall strategic plan for the City of Susanville.

The City of Susanville currently lacks a clear and up-to-date Mission Statement, Vision Statement and Core Values Statement. Historic Records indicate the last mission statement developed was written by a former City Administrator who was only employed a short time and the Mission Statement was never officially adopted or approved by City Council.

Local governments are not required to adopt mission statements. However, a well-crafted mission statement can capture in a short, concise paragraph the purpose of the organization's existence and what actions it takes to fulfill this purpose.

Mission statements are directly connected to vision statements but, whereas a vision statement describes the desired future state of the organization, the mission statement describes how that vision will be achieved. Ideally, mission and vision statements are crafted through a collaborative process that involves a wide array of community stakeholders and elected officials, usually as part of an overall strategic plan.

Staff would like to open the development process to all City employees to solicit creative ideas and suggestions for our Mission, Vision and Core Values and bring those ideas back to Council at a future meeting for final selection, adoption and approval.

FISCAL IMPACT: None.

ACTION REQUESTED: Provide direction to staff on the desire to proceed or not proceed with this project.

ATTACHMENTS: None.

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: Kevin Jones, Police Chief

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: **Resolution No. 19-5642** approving the School Resource Officer Agreement with Lassen Union High School

PRESENTED BY: Kevin Jones, Police Chief

SUMMARY: The Lassen Union High School District and the City of Susanville have been in discussions regarding the agreement for services of the School Resource Officer. This agreement identifies specific details the prior agreement did not and articulates the exact amount of funds for services provided.

FISCAL IMPACT: Approximately \$45-55,000 per year after compensation

ACTION REQUESTED: Motion to approve Resolution No. 19-5642 approving the School Resource Officer agreement with Lassen Union High School and authorizing the City Administrator and Police Chief to sign agreement.

ATTACHMENTS: Resolution No. 19-5642
School Resource Officer Agreement

RESOLUTION NO. 19-5642
A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SUSANVILLE
APPROVING AGREEMENT WITH THE LASSEN HIGH SCHOOL DISTRICT
FOR SCHOOL RESOURCE OFFICER SERVICES, AUTHORIZING THE CITY
ADMINISTRATOR/POLICE CHIEF TO SIGN AGREEMENT

WHEREAS, the City of Susanville provides a School Resource Officer to Lassen Union High School District to provide campus security services; and

WHEREAS, the 2017 Agreement has been amended to reflect updated language in several sections of the agreement; and

WHEREAS, The Lassen High School District has approved the agreement at a regularly scheduled board meeting on April 9, 2019.

NOW THEREFORE, be it resolved that the City Council does hereby approve the Agreement between the Lassen Union High School District and the City of Susanville, a copy of which is attached hereto as Exhibit "A" and does further authorize the City Administrator and Police Chief to sign the same.

APPROVED: _____
Kevin Stafford, Mayor

ATTEST: _____
Gwenna MacDonald, City Clerk

The foregoing Resolution was adopted at a regular meeting of the City Council of the City of Susanville, held on the 17th day of April, 2019 by the following vote:

AYES:
NOES:
ABSENT:
ABSTAINING:

Gwenna MacDonald, City Clerk

APPROVED AS TO FORM: _____
Jessica Ryan, City Attorney

**AGREEMENT BETWEEN
THE CITY OF SUSANVILLE POLICE DEPARTMENT AND
THE LASSEN UNION HIGH SCHOOL DISTRICT**

This Agreement is made on this ____ day of April, 2019, by and between the City of Susanville, a municipal corporation and general law city of the State of California, hereinafter referred to as the "City," and Lassen Union High School District, a school district organized and existing under the laws of the State of California, hereinafter referred to as the "District"; together, the "Parties."

PURPOSE OF AGREEMENT:

The purpose of this Agreement is to state the terms and conditions under which the City will furnish supplemental law enforcement, consisting of a School Resource Officer, referred to hereafter as "SRO," to the high school campus located on 1110 Main Street in Susanville, hereinafter referred to as the "District Property"

FURNISHING OF LAW ENFORCEMENT SERVICES:

The City shall furnish a career law enforcement officer, as defined by Penal Code Section 830.1, of the City's own choice and designation to the LUHSD as an SRO every Monday through Friday during the school year, as follows: Work hours of the SRO at the District Property shall be from 07:30 AM – 3:30 PM hours, every regularly scheduled school day ("Work Hours"). If more than one police officer will split the role of SRO at the District Property, this shall be communicated to the District on a written and pre-determined schedule to ensure the role is covered each school day during Work Hours. SPD agrees to submit a staffing schedule to the DISTRICT every month.

The CITY and the DISTRICT agree the primary responsibility of the SRO is the safety of students and staff of the District. There may be times when said officer is unavailable to be present on District property during regular school hours and the parties hereto recognize, understand and agree that at those times it will be necessary for said police officer to be unavailable to District. The parties hereto recognize, understand and agree that the City, will attempt to limit the amount of time said police officer is unavailable to the District during normal school hours.

The police officer shall spend the majority of his/her time in and around the District property; provided, however, said police officer may respond as backup to major in-progress calls anywhere in the city, when necessary, in the discretion of said police officer and/or City. The police officer will maintain full law enforcement authority to investigate crimes on the District property and shall report to the Shift Supervisor at City's police department when initiating a crime investigation.

In consideration for times the SRO will be absent from the campus, City agrees to furnish two (2) Police Officers to the District for special events outside of the school day for four (4) events per year, up to four (4) hours.

Additional dedicated City police services may be obtained by a separate agreement.

FACILITIES:

The District shall provide the SRO with office space at the District Property equipped with a desk, telephone, computer, sufficient chairs and other technological equipment as agreed upon between the City and the District, for use during Work Hours.

The City shall provide all training, equipment, vehicle, and supplies to the SRO needed to perform duties according to this Agreement.

POLICE OFFICERS ARE EMPLOYEES OF CITY:

All police officers furnished by City shall be employees of the City of Susanville and shall be at all times subject to the supervision and control of City. District will not have any supervision responsibilities of said police officer. City shall have the sole responsibility of paying the salaries, taxes (including, but not limited to, retirement benefits, federal social security taxes and federal and California unemployment taxes) and all other expenses relating to each police officer of City serving as an SRO for the District.

POLICE OFFICERS SERVICES:

City police officer duties while acting as SROs shall encompass only law enforcement duties including, but not limited to, the following services: (a) physical presence on the District Property; (b) address crime and disorder problems on the District Property; (c) develop or expand crime prevention efforts on the District Property; (d) educate District staff in crime prevention and safety, as well as in the identification of crime and related issues; (e) conduct assessments of physical and procedural issues involving safety and security; (f) assist in the development of policy by the District that addresses crime, as well as recommend procedural changes in such policies; and (g) assist in the identification of physical changes in the environment that may reduce crime.

GENERAL DUTIES OF THE SRO:

The City and District agree the primary responsibility of an SRO is the safety of the students and staff on District grounds. The general duties of an SRO are to: (a) provide education, when appropriate, to District staff in the prevention, identification and control of crime and related issues; (b) establish an office of operations at the District Property; (c) establish a scheduled presence at the District Property during special events and athletic contests; (d) provide escorts upon request for staff, students and visitors to and from the District Property; (e) conduct assessments of physical and procedural issues involving safety and security; (f) conduct meetings with the District as needed; (g) assist administrative personnel in investigation of crimes committed on the District Property; (h) assist in the reduction of truancy for the District; (i) tobacco education and enforcement on District grounds; (j) work to prevent juvenile delinquency; and (k) when necessary, collaborate with District staff to conduct interrogations, searches and seizures of students

using the reasonable suspicion standard when initiated by District staff, and the probable cause standard when initiated by an SRO.

If either party wishes to amend the duties of an SRO pursuant to this Agreement, such an amendment must be in writing signed by both Parties to this Agreement.

STUDENT RECORDS:

The Parties agree that the SRO shall be allowed access to necessary school attendance and discipline records for which the SRO has a "legitimate educational interest." The SRO shall not inspect or copy confidential student records outside the scope of the SRO's service, including any record related to a student's immigration status, except as allowed by law. The District remains in direct control of the use, maintenance and disclosure of student records in accordance with Education Code section 49076 and other applicable provisions of law.

STUDENT DISCIPLINE:

District staff shall be responsible for student discipline and shall make all decisions regarding the imposition of discipline for students enrolled at their campus and to enforce the Education Code.

WORKERS' COMPENSATION COVERAGE:

The City shall provide and maintain during the course of this Agreement workers' compensation coverage for the protection of the City's employees engaged in work as SROs pursuant to this Agreement.

INDEMNITY:

The District shall defend, indemnify and hold harmless the City, its officers, directors and employees and each of them, from any and all claims, demands, causes of actions, damages, costs, expenses, actual attorneys' fees, losses or liabilities, in law or in equity, of every kind and nature whatsoever, arising out of or in connection with the District's negligent or intentional acts or omissions in the performance of this contract for: (a) bodily injury including, but not limited to bodily injury, sickness or disease, emotional injury or death to any person(s); and (b) damage to property of anyone including loss of use thereof.

The City shall defend, indemnify and hold harmless the District, its officers, directors and employees and each of them, from any and all claims, demands, causes of actions, damages, costs, expenses, actual attorneys' fees, losses or liabilities, in law or in equity, of every kind and nature whatsoever, arising out of or in connection with the City's negligent or intentional acts or omissions in the performance of this contract for: (a) bodily injury including but not limited to, bodily injury, sickness or disease, emotional injury or death to any person(s); and (b) damage to property of anyone including loss of use thereof.

It is the intention of the Parties that, where fault is determined to have been contributory, principles of comparative fault will be followed and each Party shall bear the proportionate cost of any damage attributable to the fault of that Party, its officers, directors, agents, employees, volunteers, subcontractors, and governing board.

Each Party shall immediately notify the other party of any claims or legal actions arising out of the performance of this Agreement.

SOLE AGREEMENT:

This Agreement constitutes the sole agreement of the Parties hereto relating to the matters set forth herein and correctly sets forth the rights, duties and obligations of each to the other as of its date. Any prior agreements, promises, negotiations, or representations not expressly set forth in this Agreement are of no force or effect.

WAIVER:

Waiver of a breach or default under this Agreement shall not constitute a continuing waiver of a subsequent breach of the same or any other provisions of this Agreement.

GOVERNING LAW:

This Agreement and all matters relating to it shall be governed by the laws of the State of California.

TERM/RENEWAL:

The term of this Agreement shall be for the remainder of the 2018-2019 school year and terminate on the last day of school of the year 2021. Parties may choose to extend this agreement prior to the expiration upon mutual agreement.

TERMINATION:

This Agreement may be terminated or suspended voluntarily by either party, by giving the other party written notice of the party's intent to terminate or suspend the Agreement by approval of each party's respective governing body.

INSURANCE:

Each of the Parties shall respectively furnish and keep in full force and effect at all times during the term of this Agreement the following insurance:

Public liability insurance in the minimum amounts of \$1,000,000 for one person and of \$2,000,000 for more than one person for loss from a tort resulting in bodily injury or death,

naming the other party as an additional insured and providing a Certificate of Insurance to that effect. Membership in a governmental self-insurance pooling arrangement is acceptable.

The District shall name the City, its Governing Board, officers, agents, and employees as an additional insured and provide the City with a copy of the Certificate of Insurance and the additional named insured endorsement prior to the commencement of this Agreement.

The City shall name the District, its Governing Board, officers, agents, and employees as an additional insured and provide the District with a copy of the Certificate of Insurance and the additional named insured endorsement prior to the commencement of this Agreement.

Either party may request an increase or decrease to insurance coverage limitations no later than December 31, to take effect six (6) months thereafter with such amendment being in writing and signed by both Parties to this Agreement.

COMPENSATION:

For the services agreed to be performed under this Agreement, the District shall pay the City the corresponding amount for each year, payable in equal quarterly payments:

2018/19	\$68,671.50
2019/20	\$70,000.00
2020/21	\$70,000.00

It is agreed that the City or the District may reopen this contract at any time for the purpose of negotiating fees for the services performed under this Agreement.

MODIFICATION:

This Agreement shall not be modified except in a writing signed by both Parties.

SEVERABILITY:

If any term or provision of this Agreement is determined to be unlawful or in conflict with any law of the State of California, the validity of the remaining portions or provisions shall not be affected. Each term or provision of the Agreement shall be valid and enforced as written to the fullest extent permitted by law.

NO THIRD-PARTY BENEFICIARIES:

Nothing contained in this Agreement shall be construed to create, and the Parties do not intend to create, any rights in third Parties.

HIRING CITY'S EMPLOYEES:

The District shall not, either during the term of this Agreement or for a period of one year following its termination, hire for its own employment any of the employees of the City who performed services pursuant to this Agreement.

NOTICES:

Any notice submitted or communicated required or permitted to be served on a Party hereto, may be served by personal delivery to the person or office of the person identified below. Service may also be made by mail, by placing first-class postage affixed thereto, and addressed as indicated below and deposited said envelope on the United States mail to:

CITY:
Mike Wilson
City Administrator
City of Susanville
66 N. Lassen Street
Susanville, CA 96130

DISTRICT:
Bill McCabe
Superintendent
Lassen High School District
1100 Main St.
Susanville, CA 96130

AUTHORITY OF PERSONS SIGNING AGREEMENT:

The person(s) signing this Agreement on behalf of the City and the District have been authorized to do so by their respective governing bodies and this Agreement has been duly executed and delivered in accordance with the authorization and constitutes a legal, valid and binding obligation of both Parties.

IN WITNESS WHEREOF, the Parties have caused this Agreement to be executed on the dates following their respective signatures below:

CITY OF SUSANVILLE

LASSEN UNION HIGH SCHOOL DISTRICT

Mike Wilson, City Administrator

Bill McCabe, Superintendent

Kevin L. Jones, Chief of Police

Attest:

Gwenna MacDonald, City Clerk

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: Quincy McCourt, Project Manager

Action Date: April 17, 2018

CITY COUNCIL AGENDA ITEM

SUBJECT: Final Design and Bid Documents for Fruit Growers Park Rehabilitation Project

PRESENTED BY: Quincy McCourt, Project Manager

SUMMARY: Staff is presenting the final plan for the future Fruit Growers Park for the Riverside Park Improvement Project. Staff has assessed the construction costs and weighed them with the priorities of the community outreach and has determined the attached drawing to be the best approach to invest the Program Income and Grant Funds. Staff is requesting City Council approval to proceed with put the project out to bid.

FISCAL IMPACT: Grant Funded including \$370K of Program Income.

ACTION REQUESTED: Motion to approve the bid documents for Fruit Growers Park for the Riverside Park Improvement Project.

ATTACHMENTS: To be provided.

Reviewed by:  City Administrator

- Motion only
- Public Hearing
- Resolution
- Ordinance
- Information

Submitted by: Quincy McCourt, Project Manager

Action Date: April 17, 2018

CITY COUNCIL AGENDA ITEM

SUBJECT: Dog Park Location Discussion.

PRESENTED BY: Quincy McCourt, Project Manager

SUMMARY: The City Council of the City of Susanville approved the concept of a Dog Park to be located at the Skyline Multi-Use Park, however the design at Skyline has not been approved. A second location proposed by volunteers and a private donor is property owned by Lassen County and located on North Mesa Street. Both locations would require shade, water and fencing. The design at Skyline Park is ready to begin construction if approved and funded. The North Mesa location will require discussion with Lassen County regarding lease options or acquisition, environmental assessment and door-to-door visits to surrounding neighbors. The public outreach to neighbors may be performed by volunteers. The North Mesa location may have upfront construction costs covered by a private donor, however cost estimates have not been received. At its April 3, 2019 meeting, the City Council discussed options to locate the Dog Park at Skyline, at North Mesa, or to have a Dog Park at both locations. Staff is requesting discussion and direction to staff as to how to proceed.

FISCAL IMPACT: \$1.00/year up to \$135,000.00 depending on direction to staff

ACTION REQUESTED: Provide direction to Staff.

ATTACHMENTS: None.

AGENDA ITEM NO. 13A

Reviewed by:  City Administrator

Motion only
 Public Hearing
 Resolution
 Ordinance
 Information

Submitted by: Gwenna MacDonald, City Clerk

Action Date: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: Administrative Services Departmental Report

PRESENTED BY: Mike Wilson, City Administrator

SUMMARY: Staff will provide an update of the operations and projects of the Administrative Services Department, including the Building/Planning, Finance and Parks Divisions.

FISCAL IMPACT: None

ACTION REQUESTED: Information Only

ATTACHMENTS: None

Reviewed by:  City Administrator

- Motion Only
- Public Hearing
- Resolution
- Ordinance
- Information

SUBMITTED BY: Michael Wilson, City Administrator

MEETING DATE: April 17, 2019

CITY COUNCIL AGENDA ITEM

SUBJECT: FOOD TRUCK UPDATE

SUMMARY: At a recent City Council Meeting, Council asked for an update on the subject of food trucks and how our ordinance prohibited Food Trucks. Having discussed this issue with our City Planner and other staff, we interpret the following related to the Zoning Interpretation for permitting of Food Trucks:

The only place where Food Trucks are somewhat listed in the Zoning Ordinance is in the Uptown Business District (UBD) wherein "Street Vendors Utilizing Movable Carts" is listed as a Use requiring a Use Permit. (Land Use perspective may be that Definition of "Street Vendor Utilizing Movable Cart" is distinct from "Food Truck".) Either way, our Planner, as the Community Development Director, could interpret this as Food Truck is not listed and thereby not allowed in any District and/or that such type of use is not allowed in any other District than UBD as such is not listed elsewhere.

Alternatively, he could interpret, per the language in most of the other Commercial and/or Industrial Districts: "Other Permitted Uses in the District are uses determined to be similar and Character and impact to those listed as determined by the Community Development Director." This could be a District by District interpretation, or a more general "blanket" determination.

Food Trucks are considered across a broad and varied spectrum by different communities. There are a lot of potential issues. It seems that Susanville/local Code would require that:

- 1.) All food trucks on private property require a Use Permit from the Planning Commission.
- 2.) All food trucks utilizing public right-of-way require an Encroachment Permit from the relevant jurisdiction; City, County or State.
- 3.) All food trucks will need approval from the Health Department.
- 4.) All food trucks need a City Business License.

It seems important that there is consistency between any Public Works permitting and any Land Use permitting. It seems that this can be whatever the City wants.

Ultimately, any determination the Planner makes will have to go to the Planning

Commission, either because a “not allowed” interpretation is appealed or whether a liberal interpretation accepts a Use Permit application in most any District. It then becomes important that the Planning Commission is not arbitrary and that approvals, conditions of approval or denials are based upon specific Zoning District regulations and/or other specified criteria.

There are minutes from a September 2011 Planning Commission meeting concerning an ice cream truck and the lengthy discussions that went on about the Code and what the City wants, both with clarifying Code and supporting businesses. They reflect much of our Planner’s thoughts on the subject.

Our Planner’s professional opinion would be to interpret broadly and allow mobile vendors in pretty much all Zoning Districts, other than Residential, with an approved Use Permit.

We spoke to the County Health Inspector and find that the State regulations in particular, in conjunction with the County regulations, are probably the more likely inhibitors of Mobile Food Vendors than the City Ordinances.

Trucks must first obtain an insignia approved by California State Department of Housing and Community Development (DHCD), which sounded difficult, or a non-starter for many parties, per our discussion with the County Health Inspector. Thereafter, Trucks are inspected by the County Health Department and they must have an approved Commissary, in addition whatever other requirements.

Existing Food Trucks who participate in other venues are likely compliant with County and State rules and could easily provide their service to the City of Susanville.

FISCAL IMPACT: None.

ACTION

REQUESTED: None. Report for information and discussion only.

ATTACHMENTS: None.